

Traffic Calming Study: Greenbelt Center City



Prepared by



for

City of Greenbelt

Department of Planning and Community Development



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INTRODUCTION

Greenman-Pedersen, Inc. (GPI) has prepared this Traffic Calming Study for key roadways within Central Greenbelt and provided recommended countermeasures to reduce vehicle speed, improve safety for all users, and improve pedestrian / cyclist connectivity.

STUDY AREA

The following local roadways under the City of Greenbelt's jurisdiction have been analyzed for evaluation of traffic calming:

- Ridge Road – from Lastner Lane to Westway
- Lakeside Drive – from Lakecrest Drive to Crescent Road
- Lastner Lane – from Ridge Road to Crescent Road
- Greenhill Road – from Crescent Road to Research Road
- Hillside Road – from Crescent Road to Research Road

EXISTING CONDITIONS

Greenman-Pedersen, Inc. (GPI) performed multiple site visits throughout November 2023 to evaluate the potential safety concerns and associated speeding concerns along the roadway limits stated above in Greenbelt, Maryland.

All five of the study area roadways are classified by Maryland Department of Transportation State Highway Administration (SHA) as local roadways under the City of Greenbelt jurisdiction. These roadways feed into major collectors (Crescent Road and Southway) that connect to major arterials MD 201 (Kenilworth Avenue) to the west or MD 193 (Greenbelt Road) to the south.

All roadways provide on-street parking along at least one side of the roadway except for Lakeside Drive, which limits parking to only one side of the roadway for its entire length. The posted speed limit of 25 miles per hour (MPH) is posted in multiple locations for both directions of travel on all study area roadways. All roadways provide a "closed" roadway section with curb and gutter for drainage.

There are no bicycle facilities provide although it is assumed that they can share the roadway with vehicles. Pedestrian facilities vary and are outlined for each road specifically below.

Ridge Road

Ridge Road is approximately 2 miles long and ranges from 28-42 feet wide, providing one travel lane in each direction, separated by a striped double-yellow centerline. On-street parking is provided along at least one side of the roadway for the majority of its length with painted spaces of seven feet wide and vehicle travel lane widths ranging from 10-14 feet wide. A speed hump is currently provided east of Research Road. Ridge Road runs around the outer perimeter of the historic Greenbelt community, providing access to the Beltsville Agricultural Research Center, Greenbelt Elementary School, two religious facilities, and 25 bus stops.

Sidewalks are provided along at least one side of the roadway for its entire length and provides multiple connections to the City's internal path network. A speed reduction zone is provided with 15 MPH school zone flashers in proximity to the Greenbelt Elementary School.

Lakeside Drive

Lakeside Drive is approximately 0.8 miles long and ranges from 25-35 feet wide, providing one travel lane in each direction with directional flow not separated. Yield-controlled intersections exist at Olivewood Court, Maplewood Court, and Lakeview Court. Vehicle travel lane widths range from 9-14 feet wide and on-street parking is assumed to be seven feet wide. Directional travel is separated by a striped double-yellow center line between Lakecrest

Drive and Westway. Lakeside Drive has a generally curved alignment with significant changes in vertical grades. Lakeside Drive provides a connection between Lakecrest Drive and Crescent Road and access to two bus stops.

There are no sidewalks along Lakeside Drive; however, there are multiple connections to the City's internal path network.

Lastner Lane

Lastner Lane is approximately 0.6 miles long and ranges from 34-36 feet wide, providing one travel lane in each direction, separated by a striped double-yellow centerline. Vehicle travel lane widths range from 17-18 feet wide. On-street parking is restricted on both sides of the roadway south of Rosewood Drive. Lastner Lane has a generally curved alignment with significant changes in vertical grades. Lastner Lane provides a connection between Ridge Road and Crescent Road, Boxwood Village Playground, and access to four bus stops.

Sidewalks are provided along both sides of the roadway except for the segment between Ivy Lane and Crescent Road where sidewalk is only provided along the easterly side.

Greenhill Road

Greenhill Road is approximately 0.4 miles long and is 26-feet wide, providing one travel lane in each direction with directional flow not separated. Greenhill Road has a generally curved alignment with significant changes in vertical grades at its northerly end. Greenhill Road provides a connection between Crescent Road and Research Road, the Greenbelt Baptist Church, and access to four bus stops.

Sidewalks are provided along both sides of the roadway except for the segment south of Greendale Place where sidewalk is only provided along the easterly side and the segment between Research Road and 128 Greenhill Road where sidewalk is only provided along the northerly side.

Hillside Road

Hillside Road is approximately 0.4 miles long and ranges from 22-40 feet wide, providing one travel lane in each direction with directional flow not separated. Vehicle travel lane widths range from 11-15 feet wide. Hillside Road has a generally curved alignment with gradual changes in vertical grades over its length. Hillside Road provides a connection between Research Road and Crescent Road, the Greenbelt Community Church, and access to 10 bus stops.

Sidewalks are provided along at least one side of the roadway for its entire length and provides multiple connections to the City's internal path network.

Traffic Volumes

Automatic traffic recorders (ATRs) were used to obtain daily traffic volumes and speeds along the study area roadways for weekday daily and Saturday periods from Thursday, November 16, 2023 to Saturday, November 18, 2023 when public school was in session. All traffic-count data is provided in detail in the Appendix A. Table 1 summarizes the existing daily and peak-hour traffic volumes.

The highest volume of traffic was recorded at Ridge Road between Eastway and Hamilton Place with peak hour directional distribution approximately 50/50 split at a rate less than five vehicles per minute. Other roadways showed higher directional distributions between weekday AM and PM periods which indicates a strong relationship to typical commuter hours. There is no indication that these study area roadways are being utilized for the purposes of cut-through travel although an origin-destination study would need to be performed in order to confirm.

Table 1
EXISTING TRAFFIC VOLUME SUMMARY

Location/Time Period	Daily Volume (vpd) ^a	Peak Hour Volume (vph) ^b	K Factor (%) ^c	Directional Distribution ^d	Percent Heavy Vehicles (%) ^e
Ridge Road between Laurel Hill Road and Plateau Place:					
<i>Weekday</i>	998	--	--	--	7
<i>Weekday AM Peak Hour</i>	--	230	23.0	55% NB	--
<i>Weekday PM Peak Hour</i>	--	148	14.8	61% SB	--
<i>Saturday</i>	478	--	--	--	6
<i>Saturday Mid Peak Hour</i>	--	73	15.3	52% SB	--
Ridge Road between Westway and Southway:					
<i>Weekday</i>	986	--	--	--	9
<i>Weekday AM Peak Hour</i>	--	124	12.6	79% EB	--
<i>Weekday PM Peak Hour</i>	--	128	13.0	55% EB	--
<i>Saturday</i>	916	--	--	--	7
<i>Saturday Mid Peak Hour</i>	--	123	13.4	55% EB	--
Ridge Road between Eastway and Hamilton Place:					
<i>Weekday</i>	1,427	--	--	--	5
<i>Weekday AM Peak Hour</i>	--	288	20.2	52% NB	--
<i>Weekday PM Peak Hour</i>	--	185	13.0	51% SB	--
<i>Saturday</i>	974	--	--	--	3
<i>Saturday Mid Peak Hour</i>	--	120	12.3	52% SB	--
Ridge Road between Lastner Lane and Research Road:					
<i>Weekday</i>	539	--	--	--	10
<i>Weekday AM Peak Hour</i>	--	117	21.7	57% EB	--
<i>Weekday PM Peak Hour</i>	--	80	14.8	53% WB	--
<i>Saturday</i>	364	--	--	--	7
<i>Saturday Mid Peak Hour</i>	--	54	14.8	59% EB	--
Lakeside Drive between Olivewood Court and Pinecrest Court:					
<i>Weekday</i>	287	--	--	--	3
<i>Weekday AM Peak Hour</i>	--	31	10.8	71% NB	--
<i>Weekday PM Peak Hour</i>	--	40	13.9	58% SB	--
<i>Saturday</i>	290	--	--	--	2
<i>Saturday Mid Peak Hour</i>	--	29	10.0	52% SB	--
Lakeside Drive north of Crescent Road:					
<i>Weekday</i>	245	--	--	--	2
<i>Weekday AM Peak Hour</i>	--	14	5.7	57% NB	--
<i>Weekday PM Peak Hour</i>	--	33	13.5	54% NB	--
<i>Saturday</i>	210	--	--	--	4
<i>Saturday Mid Peak Hour</i>	--	29	13.8	68% NB	--

^a In vehicles per day.

^b In vehicles per hour.

^c Percentage of daily traffic occurring during the peak hour.

^d EB = eastbound, WB = westbound, NB = northbound, SB = southbound.

^e Heavy Vehicles Percentage = Class 4 vehicle and higher of total vehicles (including busses).

Table 1 (Continued)
EXISTING TRAFFIC VOLUME SUMMARY

Location/Time Period	Daily Volume (vpd) ^a	Peak Hour Volume (vph) ^b	K Factor (%) ^c	Directional Distribution ^d	Percent Heavy Vehicles (%) ^e
Lastner Lane south of Ridge Road:					
Weekday	536	--	--	--	12
Weekday AM Peak Hour	--	120	22.3	56% NB	--
Weekday PM Peak Hour	--	79	14.7	61% SB	--
Saturday	379	--	--	--	7
Saturday Mid Peak Hour	--	56	14.8	63% NB	--
Lastner Lane between Rosewood Drive and Ivy Lane:					
Weekday	726	--	--	--	9
Weekday AM Peak Hour	--	136	18.7	56% SB	--
Weekday PM Peak Hour	--	99	13.6	64% NB	--
Saturday	646	--	--	--	4
Saturday Mid Peak Hour	--	86	13.3	59% SB	--
Greenhill Road west of Research Road:					
Weekday	608	--	--	--	9
Weekday AM Peak Hour	--	117	19.2	56% EB	--
Weekday PM Peak Hour	--	101	16.6	51% WB	--
Saturday	465	--	--	--	2
Saturday Mid Peak Hour	--	62	13.3	52% EB	--
Greenhill Road north of Greenway Place:					
Weekday	809	--	--	--	7
Weekday AM Peak Hour	--	137	16.9	55% SB	--
Weekday PM Peak Hour	--	119	15.7	51% NB	--
Saturday	663	--	--	--	1
Saturday Mid Peak Hour	--	67	10.1	51% SB	--
Hillside Road north of Northway Road:					
Weekday	710	--	--	--	8
Weekday AM Peak Hour	--	92	13.0	55% SB	--
Weekday PM Peak Hour	--	99	13.9	54% SB	--
Saturday	468	--	--	--	2
Saturday Mid Peak Hour	--	53	11.3	62% SB	--
Hillside Road south of Woodland Way:					
Weekday	860	--	--	--	7
Weekday AM Peak Hour	--	108	12.6	60% SB	--
Weekday PM Peak Hour	--	122	14.2	57% NB	--
Saturday	650	--	--	--	2
Saturday Mid Peak Hour	--	92	14.2	50% SB	--

^a In vehicles per day.

^b In vehicles per hour.

^c Percentage of daily traffic occurring during the peak hour.

^d EB = eastbound, WB = westbound, NB = northbound, SB = southbound.

^e Heavy Vehicles Percentage = Class 4 vehicle and higher of total vehicles.

As shown in Table 1, all study roadway segments were found to provide adequate capacity for the weekday and Saturday daily volumes except for the segment of Ridge Road between Eastway and Hamilton Place. Capacity of a two-lane local street is 1,000 vehicles per day based on the Highway Capacity Manual (HCM) level-of-service D/E threshold.

Speeds

Vehicle speed measurements were conducted along the study area roadways by measuring the elapsed time for vehicles traveling a short, pre-measured distance between two checkpoints. The travel times were recorded using ATRs and the speeds were derived by dividing the elapsed time into the measured distance between checkpoints. The results of the speed measurements are summarized in Table 2.

Table 2
OBSERVED TRAVEL SPEED SUMMARY ^a

Location	Posted Speed ^a	Average Speed ^b	85 th Percentile Speed ^c	Peak Direction
Ridge Road between Laurel Hill Road and Plateau Place	25	27	33	53% SB
Ridge Road between Westway and Southway	25	24	30	64% EB
Ridge Road between Eastway and Hamilton Place	25	26	31	52% NB
Ridge Road between Lastner Lane and Research Road	25	26	31	52% WB
Lakeside Drive between Olivewood Court and Pinecrest Court	25	22	28	53% SB
Lakeside Drive north of Crescent Road	25	20	25	54% SB
Lastner Lane south of Ridge Road	25	24	29	52% SB
Lastner Lane between Rosewood Drive and Ivy Lane	25	25	30	52% SB
Greenhill Road west of Research Road	25	22	27	52% WB
Greenhill Road north of Greenway Place	25	20	24	52% SB
Hillside Road north of Northway Road	25	22	27	50% SB
Hillside Road south of Woodland Way	25	26	32	50% SB

^a In miles per hour (MPH).

^b Average speed of all observed vehicles.

^c Speed at, or below which 85 percent of all observed vehicles travel.

As shown in Table 2, the 85th percentile speeds (the speed at or below which 85 percent of all observed vehicles travel under free-flow conditions) were found to be consistent (within +/- 3 MPH) with the posted speed limit along Lakeside Drive, Greenhill Road, and the northerly half of Hillside Road. The 85th percentile travel speeds along Lastner Lane were found to be within 5 MPH of the posted speed limit, which is generally considered acceptable by industry standards, but can be improved to meet goals set by the City of Greenbelt. All segments of Ridge Road and the southerly half of Hillside Road were found to have 85th percentile travel speeds 5 MPH or greater than the posted speed limit which is particularly concerning due to the frequency of bus travel. The combination of higher speeds from heavier vehicles could result in more dangerous collisions. Coordination may be needed with the service providers to evaluate bus headways and vehicle telemetry to make sure drivers aren't operating in an unsafe manner.

Speeding along the study area roadways (speeds five mph or greater than posted limit) was generally observed between the weekday morning hours of 6-11 AM and weekday evening hours of 2-4 PM. The lack of speeding between the hours of 6 PM to 6 AM suggests that the existing roadways create an environment that is generally consistent with the posted speed limit.

A summary of the weekday average daily traffic (ADT) and the 85th percentile speeds is illustrated in Figure 1.

Public Transportation

Washington Metropolitan Area Transit Authority (WMATA) bus route G12 provides service along Lastner Lane. Bus routes G12 and G14 provide service along Ridge Road and Lakeside Drive. These routes provide services during weekdays and weekends. Prince George's County Department of Public Works and Transportation (PG DPW&T) bus route 11 provides service along Greenhill Road and Hillside Road on weekdays only.

Both WMATA and PG DPW&T bus routes provide connections to Metro Rail at Greenbelt Station or New Carrollton Station. The conditions of bus stops vary by what amenities they provide (i.e., benches, shelters), if any. All stops provide sign designation; however not all stops provide bus pads or are placed consistently on the arrival or departure side of intersections and some pads do not connect to sidewalks. The detailed bus schedules are provided in the Appendix B.

Prince George's County provides public school bus service for Greenbelt Elementary School which utilize study area roadways. These stops are not signed or provide dedicated bus pads. The detailed bus schedules are provided in the Appendix B. Morning pickups are scheduled for the following routes:

- Bus Route 459 – 9:05 AM located at Lakeside Drive / Westway intersection.
- Bus Route 460 – 9:04 AM located at Lackcrest Drive / Lakeside Drive intersection.

Afternoon drop-offs are scheduled for the following routes:

- Bus Route 276 – 3:55 PM located at Lakeside Drive / Westway intersection.
- Bus Route 277 – 3:46 PM located at Lackcrest Drive / Lakeside Drive intersection.

Collisions

Collision data for the study area intersections was obtained from the City of Greenbelt for the most recent available four-year period (January 2019 through November 2023). A summary of the crashes along the study area roadways is provided in Table 3. The collision data does not provide information regarding how the crash occurred or contributing factors that may have influenced driver behavior. The detailed crash summaries are provided in the Appendix C. A visual summary of the collision data is provided in Figure 2.

Based on the data provided by the City of Greenbelt, there is a high frequency of hit & run collisions which does not have a clear rationale or pattern of collision type (i.e., angle, sideswipe, rear-end, etc.). It's also not clear whether these hit & run collisions occurred for vehicles parked along the roadway or parked within off-street lots. These collisions may be minor fender-bender incidents which are more common at slower vehicle speeds. In order to provide a conservative assessment, it has been assumed that collisions occurred along the roadway most closely located with address listed. The roadway segments with annual crash rate greater than three collisions per year over the four-year study period are:

- Ridge Road from Research Road to Northway (10)
- Ridge Road from Hamilton Place to Southway (15)
- Lakeside Drive from Westway to Lakecrest Drive (15)

Lakeside Drive, Lastner Lane, and Hillside Road have overall lower average and 85th percentile travel speeds which indicates that safety issues along this roadway can also be attributed to factors other than speed, though it's unclear from the crash data what those factors are. Crashes that resulted in personal injury are less than five percent likely to occur per study roadway which is consistent with lower travel speed roadways.

There were no clear collision pattern types based on the data provided and no intersection had annual crash rates higher than three per year. The intersections with the highest concentration of collisions over the four-year study period are:

- Ridge Road / Southway (7)
- Greenhill Road / Hillside Road / Research Road (5)
- Lakeside Drive / Westway (4)
- Lastner Lane / Crescent Road (4)

Table 3
COLLISION SUMMARY

Location	Number of Collisions		Collision Severity ^a		Collision Type ^b			Percent During Commuter Peak ^c
	Total	Average per Year	PD	PI	Hit & Run	Vehicle	Ped / Bike	
Ridge Road from Lastner Lane to Research Road	3	0.75	2	1	1	2	--	33%
Ridge Road from Research Road to Northway	10	2.50	10	--	5	5	--	0%
Ridge Road from Northway to Hamilton Place	8	1.00	8	--	1	7	--	38%
Ridge Road from Hamilton Place to Southway	15	3.75	14	1	4	10	1	7%
Ridge Road / Southway	7	1.75	7	--	1	6	--	0%
Ridge Road from Southway to Westway	5	1.25	5	--	2	3	--	40%
Lakeside Drive / Crescent Road	3	0.75	3	--	1	2	--	33%
Lakeside Drive from Crescent Road to Olivewood Court	0	0.00	--	--	--	--	--	0%
Lakeside Drive from Olivewood Court to Westway	7	1.75	7	1	1	6	--	14%
Lakeside Drive / Westway	4	1.00	4	--	--	4	--	0%
Lakeside Drive from Westway to Lakecrest Drive	15	3.75	15	--	11	4	--	27%

Source: City of Greenbelt (2019-November 2023).

^a PD = assumed property damage only; PI = Personal Injury.

^b Ped / Bike = Pedestrian / Bicycle.

^c Percent of vehicle incidents that occurred during the weekday AM (7:00 AM-9:00 AM) and weekday PM (4:00 PM-6:00 PM) commuter peak periods.

Table 3
COLLISION SUMMARY (Continued)

Location	Number of Collisions		Collision Severity ^a		Collision Type ^b			Percent During Commuter Peak ^c
	Total	Average per Year	PD	PI	Hit & Run	Vehicle	Ped / Bike	
Lastner Lane from Ridge Road to Hedgewood Drive	6	1.50	6	--	4	2	--	50%
Lastner Lane from Hedgewood Drive to Crescent Road	3	0.75	3	--	1	2	--	0%
Lastner Lane / Crescent Road	4	1.00	4	--	--	4	--	0%
Greenhill Road from Crescent Road to Research Road	2	0.50	2	--	--	2	--	0%
Greenhill Road / Research Road	5	1.25	4	1	1	3	1	0%
Hillside Road from Research Road to Crescent Road	4	1.00	4	--	--	4	--	25%

Source: City of Greenbelt (2019-November 2023).

^a PD = assumed property damage only; PI = Personal Injury.

^b Ped / Bike = Pedestrian / Bicycle.

^c Percent of vehicle incidents that occurred during the weekday AM (7:00 AM-9:00 AM) and weekday PM (4:00 PM-6:00 PM) commuter peak periods.

STUDIES BY OTHERS

GPI has reviewed previous studies for the study area roadways which were performed on behalf of The City of Greenbelt^{1,2,3,4,5,6}. These studies noted the following themes related to Traffic Calming:

- Multiple requests for speed studies due to complaints from residents about excessive vehicle speeds along study area roadways.
- The City adopted a Complete and Green Streets policy which prioritizes pedestrian and bicycle travel in 2019 and updated in 2022.
- Roadway design needs to address safety for all modes and abilities.
 - Sidewalks, paths, ramps, etc. need to meet Public Rights-of-Way Accessibility Guidelines (PROWAG).
 - Signage and pavement markings need to meet the Maryland Manual for Uniform Traffic Control Devices (MdMUTCD) standards.
- Parking prohibitions to provide adequate sight distances at street intersections and mid-block crossings.
- Improve connections between the interior pathway system and roadways.
- Improve bicycle safety, connectivity, and comfort for both on-road and pathway facilities.
 - Installing share-lane markings (sharrows).

TRAFFIC CALMING

GPI has reviewed the Maryland State Highway Administration (SHA) Context Driven Toolkit⁷, the National Association of City Transportation Officials (NACTO) Urban Street Design Guide⁸, and Global Street Design Guide⁹ for “speed reduction mechanisms” for potential traffic calming countermeasures which could be implemented along the study area roadways.

Design strategies / calming treatments not considered

Barrier Separated Bike Lane and Protected Intersection

These treatments typically include the presence of a vertical curbing element (made of rubber, concrete, flexible posts, etc.) in addition to a painted buffer. Flexible posts or plastic bollards within curbing elements are set to a height less than 3.5 feet high to minimize conflicts with sight lines. While these are applicable in urban environments similar to those in Greenbelt, the existing study area roadway widths do not provide adequate room for bicycle lanes and therefore providing a buffer for this type of bicycle accommodation has not been recommended as part of this study.

In-Lane Floating Bus Stop

In-lane floating bus stop consists of a raised platform that allows buses to pick up passengers without pulling out of traffic lanes. In order to accomplish this without negatively impacting cyclists, bike lanes are diverted behind the bus stop amenities. While this configuration allows transit vehicles to stay in their own lane without crossing the bike paths, there are concerns with current design standards by the disabled community. This treatment may

¹ Speed and Traffic Reports Greenhill Rd; City of Greenbelt; April 2015.

² Approved Pedestrian & Bicycle Master Plan Update; City of Greenbelt; January 2022.

³ Speed and Traffic Reports Ridge Road between Southway and Westway; City of Greenbelt; December 2015.

⁴ Stop Sign Compliance on Lastner Lane; City of Greenbelt; February 2022.

⁵ Ridge Road Speed Concern; City of Greenbelt; March 2022.

⁶ Sight Distance Analysis, 33 Ridge Road; Greenman-Pedersen, Inc.; December 2022.

⁷ Context Driven Toolkit; MD SHA - <https://experience.arcgis.com/experience/769bd85416ff4e46bf3cb78a67ed4640/>

⁸ Urban Street Design Guide; NACTO; <https://nacto.org/publication/urban-street-design-guide/design-controls/design-speed/speed-reduction-mechanisms/>

⁹ Traffic Calming Strategies; Global Street Design Guide; <https://globaldesigningcities.org/publication/global-street-design-guide/designing-streets-people/designing-for-motorists/traffic-calming-strategies/>

not be necessary for bus routes with long headways and low volume of passengers as the disruption of the stops could be minimal. No bus ridership data was collected as part of this study and therefore has not been recommended at this time.

Pedestrian Hybrid Beacon, Leading Pedestrian Intervals, No Turn on Red

Strategies related to traffic signals have not been included in this study as a single traffic signal is provided within the study area. This signal is located east of 22 Court Ridge Road is assumed to be operating two-phases (east-west travel along Ridge Road and a dedicated pedestrian phase) with adequate time to cross the two-lane roadway. Turning movement counts (TMCs) were not performed as part of this study to determine if traffic signal warrants are met and all other intersections are unsignalized.

Lane Width Reduction

Reducing the width of vehicle travel lanes where they are either undefined or painted at offset widths greater than 11-feet can create an environment where drivers feel comfortable traveling at higher speeds than what is posted. SHA recommends that minimum 11-foot lanes be provided on roadways which regularly accommodate truck and bus traffic. However, this treatment is typically used on collector and arterial roadways where roadway shoulders are also provided and has therefore, not been considered as part of this study.

Lane Shift, Chicane and On-Street Parking

Shifting the centerline alignment of a roadway and thus providing horizontal deflection which forces drivers to deviate from a straight-line can help to reduce speed. This treatment is frequently reinforced by placing on-street parking on different sides of the roadway. If the centerline of the roadway is shifted multiple times throughout a roadway segment due to placement of on-street parking or curb bump-outs, then the treatment is considered to be a chicane.

The limits of on-street parking are already established by signage and/or pavement markings on at least one side of the study area roadways. This traffic calming treatment was not considered because it would require more significant corridor-wide modifications increasing impacts and cost.

Posted Speed Limit Reduction

The posted speed limit of 25 MPH has been determined and is enforced by the City of Greenbelt. This speed is generally consistent with local roadways and does not require further reduction. A speed limit reduction for the Greenbelt Elementary School has been established as 15 MPH.

Building Lines and Street Trees

It is not recommended that future homes be constructed closer to the roadway nor the placement of street trees within the sidewalks where it would impact the minimum width requirements. These strategies can help to narrow a driver's field of vision but may affect the character of the neighborhood.

Diverter and One-Way Restriction

Diverter typically include the placement of a barrier which breaks vehicle access of a roadway but maintains permeability for pedestrians and bicyclists. Restricting traffic flow through regulation signage and/or turning barriers can also re-route vehicles. There is no evidence from the ATR data collected that the study area roadways are being utilized as cut-through routes which needs correcting. In addition, it is unclear from the collision data if these treatments would be needed to address a specific pattern of collisions.

Raised Intersection / Speed Table

Raised intersections exist where the approaching roadways are vertically deflected to meet sidewalk level. This strategy is more desirable for urban areas where pedestrian volumes vastly exceed vehicles and are given right-of-way.

There are no “Snow Emergency Route” signs posted along the study area roadways. However, this strategy is less desirable for emergency responders as they will be forced to reduce their speed or risk bottoming out their vehicles. The study area roadways do not provide direct access to a dense commercial area where it would be preferred that pedestrian crossings be prioritized.

Design strategies / calming treatments considered but not utilized

Roundabout, Mini Roundabout, and Traffic Circle

A mini roundabout or traffic circle is defined by the Federal Highway Administration (FHWA) as “a single lane with an inscribed circular diameter between 50 and 90 feet. Its defining feature is traversable central and splitter islands to accommodate large vehicles.” These treatments are typically applied at unsignalized intersections.

Construction of a roundabout would require reconstruction of existing curb lines and evaluation of drainage impacts similar to curb bump-outs which alter intersection radii. FHWA defines a roundabout as featuring “channelized, curved approaches that reduce vehicle speed, entry yield control that gives right-of-way to circulating traffic, and counterclockwise flow around a central island that minimizes conflict points.” The central and splitter islands are not traversable for passenger vehicles and can sometimes utilize semi-mountable curbs to accommodate large vehicles. These designs can require additional right-of-way impacts and require the update of regulatory and warning signage from STOP signed control. It is unclear from the collision data if the design of these intersection treatments would be preferred to address specific collision patterns and therefore have not been included as part of this study. No TMCs were collected as part of this study so the impact to intersection operations is unknown at this time and would need to be confirmed. The following intersections could be evaluated further for these treatments:

- Ridge Road / Northway
- Ridge Road / Gardenway
- Ridge Road / Southway
- Greenhill Road / Orange Court / Greenway Place
- Hillside Road / Northway

Pros:

- Reduces number of collision points within a typical four-way intersection.
- Geometry lends to reduction in collision severity impacts.
- Operates without the need for electricity.
- Improves intersection and stopping sight distances where existing sight lines are limited.

Cons:

- Can require more space than typical STOP controlled intersections.
- Require additional design considerations for cyclists.
- Traffic circles and mini roundabouts can require additional education to inform residents about the functionality of the treatment. Not all motorists intuitively understand who has the right-of-way.

Speed Hump

Speed humps are parabolic vertical traffic calming devices larger than speed bumps which span the roadway width so vehicles are encouraged to pass over with both wheels at the same time. Placement of speed humps needs to be evaluated to make sure they're not spaced too far apart to be effective or too close to intersections. Additional assessment is required to evaluate drainage areas and confirm that flow is not interrupted.

Pros:

- Design and construction materials vary depending on the roadway needs and speed reduction desired.
- Design has proved to successfully reduce vehicle speeds for low volume, low speed roadways.
- Vehicles approaching at higher speeds experience an uncomfortable jarring.

Cons:

- Less desirable for emergency responders as they will be forced to reduce their speed or risk bottoming out their vehicles.
- Placement in areas with a high density of driveways can be difficult.

Speed Cushion

Speed cushions are a variation of speed humps or speed tables that include wheel cutouts to allow large vehicles to pass unaffected, while reducing passenger car speeds by extending enough to affect the wheel in one direction. Placement of speed cushions needs to be evaluated to make sure they're not spaced too far apart to be effective or too close to intersections.

Pros:

- Design and construction materials vary depending on the roadway needs and speed reduction desired.
- More desirable for emergency responders and buses as the wheelbases of their vehicles are wider than passenger vehicles and therefore can pass over without the need to reduce their speed. It is recommended that approval from emergency response officials be obtained prior to installation.

Cons:

- Cannot be combined with mid-block crosswalks.
- Placement in areas with a high density of driveways can be difficult.
- Sufficient visibility and lighting are recommended to function safely.

Rectangular Rapid Flashing Beacon (RRFB)

RRFBs consist of two, rectangular- shaped yellow indications, each with a light-emitting diode (LED)-array-based light source¹⁰. The LED array flashes with an alternating high frequency when activated by a pedestrian push button to attract driver's attention of a pedestrian's presence.

Pros:

- Reduce pedestrian crashes.
- Flashers activate only when button is depressed so message is likely to be more effective than LED lights which flash continuously.

Cons:

- More expensive than regular signage.
- Requires energy source (i.e., solar powered or hard-wired).

Design strategies / calming treatments considered**Continental Crosswalk**

Continental crosswalks, also known as Longitudinal Bar crosswalks, are a type of crosswalk design with high visibility to drivers due to their thick striping which is applied parallel to approaching vehicles. This style of marking is consistent with the MdMUTCD and is recommended for all crosswalks at intersecting roadways but is not required for private driveways.

Pros:

- Increased visibility.

¹⁰ Rectangular Rapid Flashing Beacons (RRFB); U.S. Department of Transportation Federal Highway Administration; Office Of Safety; https://highways.dot.gov/sites/fhwa.dot.gov/files/RRFB_508_0.pdf

- Low-cost strategy which is generally easy to maintain.
- Material application can vary.

Cons:

- Additional maintenance may be required depending on material applied.

Green Pavement for Bike Lanes

Application of green pavement markings placed along a bicycle lane can help improve driver awareness of a conflicting bicycle facility. Additional benefits of this treatment include increased yielding compliance from drivers and discourages illegal stopping in bike lanes. Green-colored pavement has Interim Approval (IA-14) through Federal Highway Administration (FHWA) for use pending revision to the Manual on Uniform Traffic Control Devices (MUTCD) conditions. SHA recommends that green pavement “can be considered at intersections, particularly through wide or complex intersection where the bike path may be unclear, and conflicts with medium or high permissive turning volumes.”

Pros:

- Increased visibility of cyclists and potential conflicts.
- Improved yielding behavior by drivers to cyclists.
- Low-cost strategy which is generally easy to maintain.

Cons:

- Additional maintenance may be required depending on material applied.

Corner Radii and Curb Extensions

Corner radii reductions and curb extensions (also referred to as bump-outs) visually and physically narrow the roadway. The reduction of a corner radius can have a significant impact to the turning speed of vehicles and can create a more compact intersection.

Pros:

- Reduced corner radii can force vehicles to reduce speed in order to execute a turning movement safely.
- More compact intersections can improve sight lines for vehicles and pedestrians by placing crosswalks and STOP lines closer to the center of the intersection.
- More compact intersections reduce crossing distance for pedestrians.
- Can provide opportunities for pavement removal. It is recommended that the sections of pavement removal, not part of proposed sidewalks, be replaced with turfgrass to decrease stormwater impacts.
- Semi-mountable curbing and pavement options are possible for reducing the turning speeds of passenger vehicles while remaining accessible for vehicles with larger turning radii such as busses or emergency vehicles.
- Increases available space for street furniture, benches, plantings, and street trees.

Cons:

- Reconstruction of curb may require additional consideration for drainage and stormwater management depending on the grade of the roadway.
- Reduction of curb radii can limit access to larger vehicles such emergency responders or busses.

Departure-side Bus Stops

Placement of bus stops on the departure side of intersections is not generally considered to be a traffic calming strategy; however, there are noted safety benefits compared to stops located on the arrival side of an intersection.

Pros:

- Buses clear the intersection to allow passengers to board or alight without holding up traffic behind them.
- The intersection crosswalk is not obstructed by a stopped bus for vehicles traveling in the same direction which may otherwise block a pedestrian seeking to cross the roadway.
- Discourages passengers from crossing the street in-front of the bus.

Cons:

- Trailing vehicles may enter an unsignalized intersection behind a bus and block other movements.

Midblock Crosswalk

This treatment allows pedestrians to cross a roadway at non-intersection locations. It is recommended that midblock crossings be provided along with pedestrian warning signage in advance and at the crossing location.

Pros:

- Where possible, a median can be provided to allow pedestrians the ability to cross one direction of travel at a time and reduce crossing distance.
- Dedicated crossing location where drivers can better anticipate that a pedestrian will cross.

Cons:

- Placement can be limited by a variety of factors including driveway density, sight lines, and traffic speed.
- Crossing location can require additional measures for signage and pavement markings to reinforce compliance.

Hardened Centerline and Median

The practice of hardening centerlines can be accomplished by the creation of a physical barrier between two opposing flows of traffic in addition to pavement striping and is similar to barrier separated bike lanes. This can also be accomplished more permanently by constructing a median (typically includes some combination of asphalt/concrete, loam/seed, and vertical curbing).

Pros:

- Reduces the speed of turning vehicles as they're forced to navigate around an obstruction.
- Reduction of head-on, angled, and pedestrian collisions.
- Potential to provide pedestrian refuge allowing for crossing one direction of travel at a time.
- Helps reduce travel lane widths, which in turn, reduces vehicle speeds.
- Mountable/sloped treatment allows larger vehicles to complete turning maneuvers successfully.

Cons:

- Centerline hardening treatments cannot be used in lieu of required KEEP RIGHT (R4-7 or R4-7(1)) and OBJECT MARKER (OM1-3) signs where median noses are present.
- Medians may require additional consideration for drainage and stormwater management depending on the location of the median and the grade of the roadway.
- Does not improve sight line visibility of pedestrians looking to enter the roadway from the sidewalk.
- May require high visibility paint or additional delineators to alert approaching motorists.

Pinchpoint / Choker

Curb extensions applied at midblock locations where both roadway edges are narrowed as mirrored opposites.

Pros:

- Focuses the driver's field of vision.
- Facilitates mid-block crossings and improves pedestrian visibility.
- Formalizes limits of on-street parking.

Cons:

- Reconstruction of curb may require additional consideration for drainage and stormwater management depending on the grade of the roadway.

Raised Crosswalk

Raised crosswalks are similar to speed humps design with the exception that the vertical deflection levels off and provides a flat surface for crosswalk¹¹. Further investigation into pedestrian crossing volumes could be explored in future studies to determine if increased safety precautions at heavily utilized crossings are needed based on usage and collision history.

Pros:

- Design varies depending on the roadway needs and speed reduction desired.
- May be combined with mid-block crosswalks.

Cons:

- This strategy is less desirable for emergency responders as they will similarly be forced to reduce their speed or risk bottoming out their vehicles.
- Placement in areas with a high density of driveways can be difficult.

CONCEPTUAL COST ESTIMATES

The recommended improvements are anticipated to be implemented by the City of Greenbelt's Public Works Department. In order to provide an estimate cost of implementation and materials, GPI utilized the Maryland SHA Price Index – January 2024¹². The list is not fully inclusive of drainage structures, erosion and sediment controls, mobilization, impacts to utilities etc. The quantities of all materials were based on measurements taken from Google Earth and have not been confirmed with to-scale measurements. The cost to implement these improvements will fluctuate based on the City's procurement process, availability of materials, and current unit costs. The itemized conceptual cost estimate details are provided in Appendix D.

CONCEPTUAL IMPROVEMENTS

This section provides context of the multiple traffic calming design strategies and treatments at key locations. The study area roadway segments have been assigned a priority ranking based on the available data to better guide the City on which locations should be addressed first. A summary of locations for traffic calming improvements to be considered by the City of Greenbelt are summarized in Figure 3. The locations are focused on intersections and roadway segments where the needs for traffic calming were rated medium to high. The detailed ranking criteria are provided in Appendix E.

The recommended improvements for the study area roadways are intended to accomplish the overall goal of improving safety by addressing the concern of vehicle travel speed. Unless otherwise noted, all recommended improvements are made within the City of Greenbelt's right-of-way. These recommendations are made as concepts and do not account for impacts to utilities.

Similar to previous studies prepared by others, GPI recommends that a combination of traffic calming measures be implemented on the study area roadways. These improvements will have the intentional effect of reducing vehicle speeds along the study area roadways when implemented together while helping promote alternative modes of travel within Historic Greenbelt. A typical corridor design was not feasible due to the densely spaced intersections of side-street and driveways are densely spaced (approximately 12 to 25 intersections per mile), changes to limits of on-street parking, and presence of sidewalk. All recommended sidewalk and ramp improvements are intended to be constructed in a manner consistent with standards set by the Americans with Disabilities Act (ADA).

¹¹ Raised Crosswalk; U.S. Department of Transportation Federal Highway Administration; https://safety.fhwa.dot.gov/ped_bike/step/docs/techSheet_RaisedCW2018.pdf

¹² Price Index January 2024; Maryland Department of Transportation State Highway Administration; https://roads.maryland.gov/ohd2/MDSHA_Pirce%20Index_Jan2024.pdf

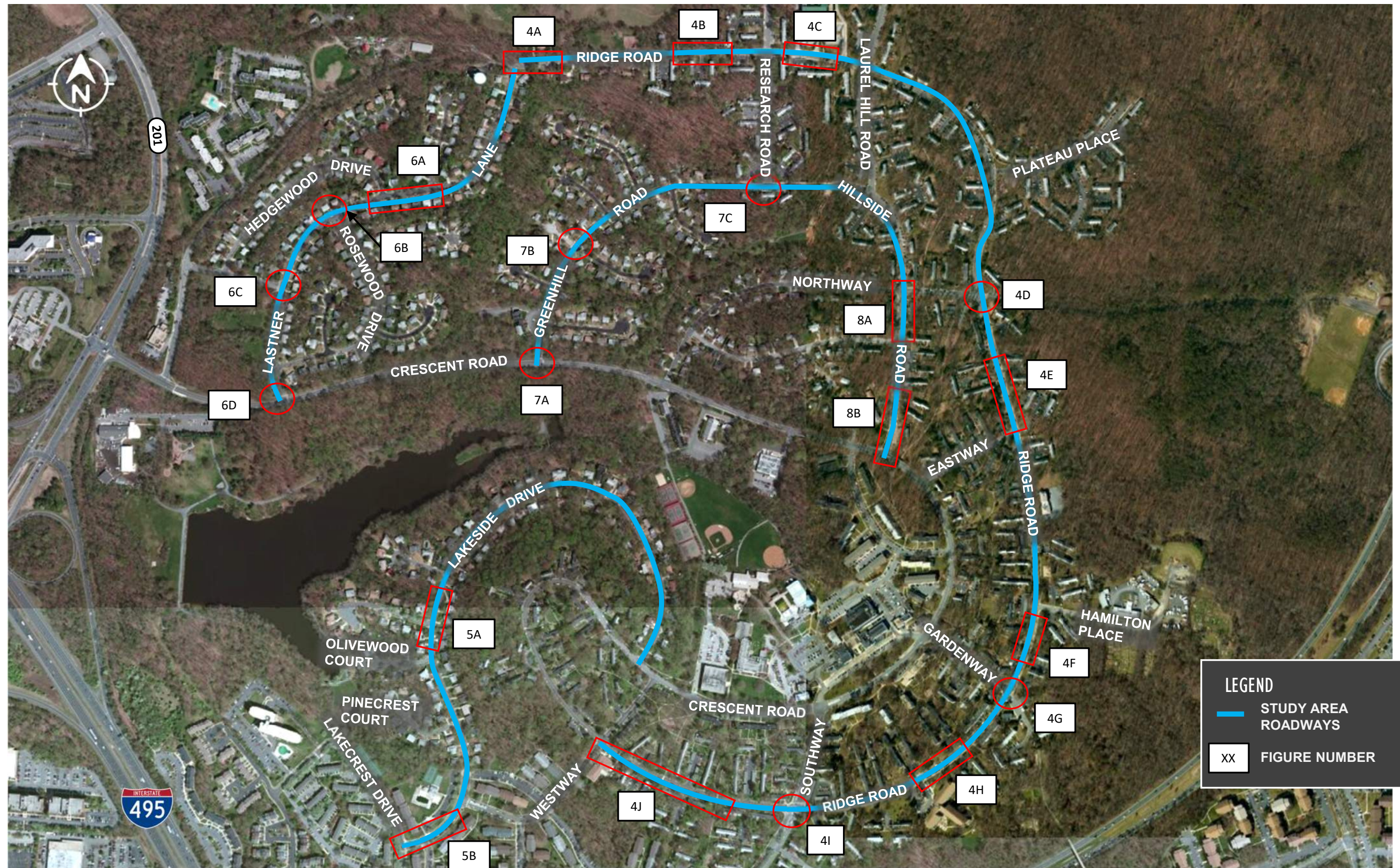


FIGURE 3
OVERALL CONCEPTUAL IMPROVEMENT MAP

Ridge Road / Lastner Lane – Priority Medium

The recommended long-term improvements are illustrated in Figure 4A.

Short-term: Estimated cost = \$17,240.

- Install temporary curb with flex posts to create a pinchpoint at the existing mid-block crossing.
- Install temporary curb with flex posts extending west from the Ridge Road mid-block crossing to narrow the westbound roadway width to 11-feet wide.
- Restripe the STOP lines on the Lastner Lane northbound and Ridge Road westbound approaches closer to the intersection.
- Stripe the double yellow centerlines on the Lastner Lane northbound and Ridge Road westbound approaches extending to the relocated STOP lines.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$47,400 not including costs for drainage improvements.

- Construct approximately 40 feet of sidewalk along the northerly side of Ridge Road to realign the Ridge Road mid-block crossing perpendicular. This crossing is currently oriented at an angle, and it is recommended that this be realigned to a perpendicular crossing to further reduce crossing distance and improve pedestrian visibility.
- Reconstruct access ramps at the Ridge Road mid-block crossing to meet ADA standards. The existing northerly ramp partially exists within a residential driveway, is obstructed by a utility pole, and does not meet ADA standards.
- Remove sections of Ridge Road to construct a pinchpoint at the Ridge Road mid-block crossing and reduce the Ridge Road westbound approach to Lastner Lane to 11-feet wide.
 - The recommended roadway width reduction will require the reconstruction of one driveway apron but is not anticipated to affect any existing drainage structures.
- Apply short-term striping recommendations.

GPI recommends the following be performed at the intersection of Ridge Road / Lastner Lane and the existing mid-block crossing located approximately 230 feet to the east. Traffic calming improvements are recommended in this location based on the 85th percentile speed of 31 MPH recorded. No reduction of marked on-street parking spaces is anticipated as a result of these improvements.



Realign mid-block crossing and reconstruct ramp out of driveway



LEGEND

- PERVIOUS AREA
- SIDEWALK / RAMP
- DRIVEWAY RECONSTRUCTION

FIGURE 4A
RIDGE ROAD / LASTNER LANE IMPROVEMENTS

Ridge Road at 7 Court Research Road – Priority Medium

The recommended long-term improvements are illustrated in Figure 4B.

Short-term: Estimated cost = \$15,780.

- Install temporary curb to supplement existing with flex posts to increase the separation of walkway and roadway.
- Perform vegetation clearing and necessary adjustments of flex posts to provide a minimum 5-foot wide walkway.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$34,350 not including costs for drainage improvements.

- Construct approximately 135 feet of 5-foot wide sidewalk along the northerly side of Ridge Road to connect existing sidewalk east of 7 Court Research Road to existing sidewalk west of the 92 Ridge Road driveway.
- Remove sections of Ridge Road to provide grass buffer for sidewalk and reduce the Ridge Road westbound approach to Lastner Lane to 11-feet wide.
 - Adjustment of existing curb will reinforce parking restriction between 92 Ridge Road and 7 Court Research Road and better define limits of permitted on-street parking.
 - The recommended roadway width reduction will require the reconstruction of two driveway aprons but is not anticipated to affect any existing drainage structures.

GPI recommends the following be performed between the intersection of Ridge Road / 7 Court Research Road and approximately 135 feet to the west at 92 Ridge Road. Traffic calming improvements are recommended in this location based on the 85th percentile speed of 31 MPH recorded. No reduction of on-street parking spaces is anticipated as a result of these improvements.



FIGURE 4B
 RIDGE ROAD AT 7 COURT RESEARCH ROAD IMPROVEMENTS

Ridge Road at Greenbelt Elementary School / 65 Ridge Road – Priority High

The recommended long-term improvements are illustrated in Figure 4C.

Short-term: Estimated cost = \$9,100.

- Install temporary curb with flex posts to create a pinchpoint at the existing mid-block crossing.
- Install temporary curb with flex posts on either side of the 65 Ridge Road driveways to re-enforce the limits of on-street parking and narrow the appearance of the roadway.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$19,200 not including costs for drainage improvements.

- Construct curb bump-outs to create a pinchpoint at the existing mid-block crossing. Construct curb bump-outs on either side of the 65 Ridge Road driveways to reinforce the limits of on-street parking and narrow the appearance of the roadway.
- Reconstruct the existing speed hump to create a formal raised crosswalk which will allow pedestrians to cross the roadway at sidewalk elevation.
 - Reconstruction will require the reconstruction of ramps on either side of the crossing and drainage channels.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 33 MPH and high volume of collisions along this segment of roadway recorded. No reduction of on-street parking spaces is anticipated as a result of these improvements.



Ridge Road / Plateau Place - Priority High

The recommended long-term improvements are illustrated in Figure 4D.

Short-term: Estimated cost = \$12,850.

- Install temporary curb with flex posts to reduce corner radii at the intersection.
- Install temporary curb with flex posts along entrance of Plateau Place to re-enforce the limits of on-street parking and narrow the appearance of the roadway.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.
- Stripe continental crosswalks at ramp crossings.

Long-Term: Estimated cost = \$50,520 not including costs for drainage improvements.

- Construct curb bump-outs to reduce corner radii at the intersection.
- Extend existing sidewalk to the new curb locations.
- Construct new ramps crossing at Ridge Road towards the existing bus stop and new sidewalk.
- Coordinate with WMATA to relocate the bus stop located on the southwest corner to the northwest corner and construct a new bus pad.
- Apply short-term striping recommendations.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 33 MPH and high volume of collisions along this segment of roadway recorded. No reduction of on-street parking spaces is anticipated as a result of these improvements.



LEGEND	
■	PERVIOUS AREA
■	SIDEWALK / RAMP
■	DRIVEWAY RECONSTRUCTION

FIGURE 4D
RIDGE ROAD / PLATEAU PLACE IMPROVEMENTS

Ridge Road / Northway – Priority High

The recommended long-term improvements are illustrated in Figure 4E.

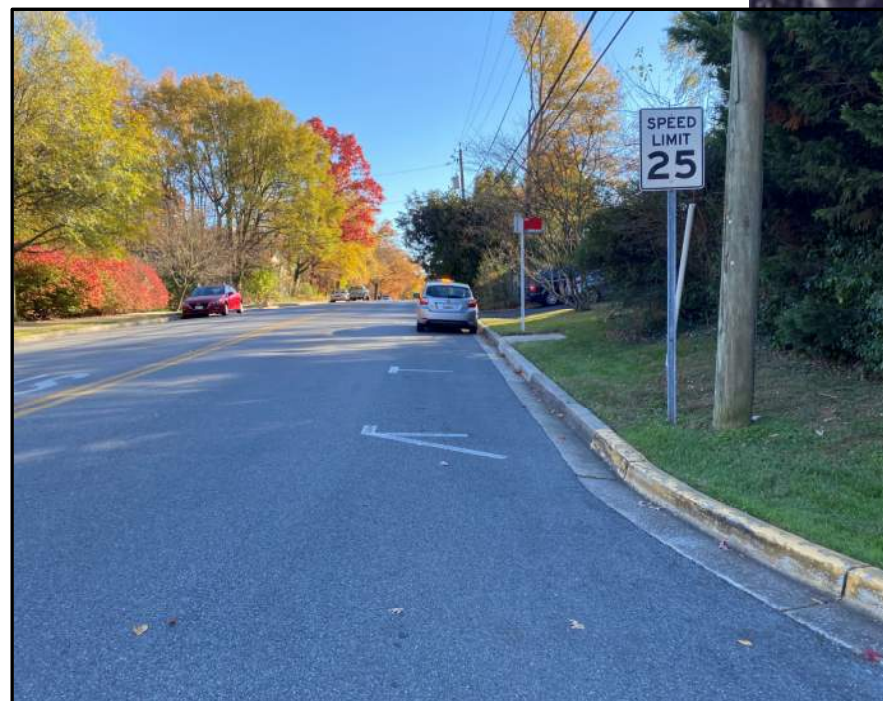
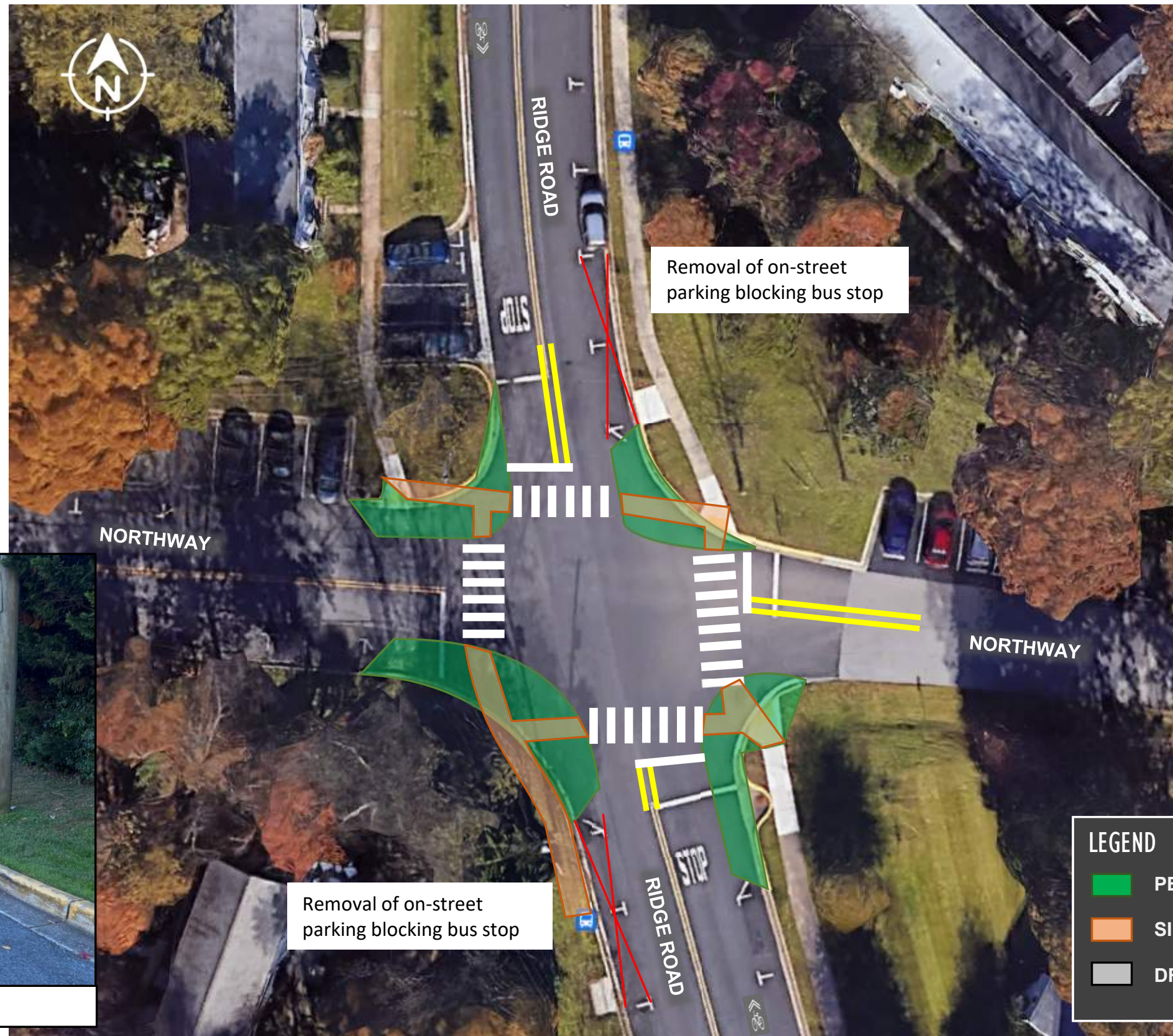
Short-term: Estimated cost = \$23,160.

- Install temporary curb with flex posts to reduce corner radii on all four corners of the intersection.
- Stripe a continental crosswalk across the Northway westbound approach where access ramps are currently provided.
- Restripe the STOP lines on the Ridge Road northbound, Ridge Road southbound, and Northway westbound approaches closer to the intersection.
- Stripe the double yellow centerlines on the Ridge Road northbound, Ridge Road southbound, and Northway westbound approaches extending to the relocated STOP lines.
- Remove two parking spaces on the southwest corner and two parking spaces on the northeast corner of the intersection which obstruct the WMATA bus stop.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$54,330 not including costs for drainage improvements.

- Construct curb bump-outs to reduce corner radii on all four corners of the intersection.
 - Adjustment of curb lines will require updates to drainage inlets on the northwest, southwest, and southeast corners.
- Construct sidewalk connections on all four corners of the intersection to create crossings for the Northway eastbound and Ridge Road southbound approaches.
 - Sidewalk construction on the southwest corner should extend from the intersection to the existing bus pad.
- Apply short-term striping recommendations.
- Stripe continental crosswalks across all four approaches.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 33 MPH recorded. The reduction of two on-street parking spaces is anticipated as a result of these improvements to provide a dedicated space for WMATA buses.



Existing bus pad within on-street parking space

Removal of on-street parking blocking bus stop

Removal of on-street parking blocking bus stop

LEGEND

- PERVIOUS AREA
- SIDEWALK / RAMP
- DRIVEWAY RECONSTRUCTION

FIGURE 4E
RIDGE ROAD / NORTHWAY IMPROVEMENTS

Ridge Road from 46 Court Ridge Road to Eastway / 44 Court Ridge Road – Priority High

The recommended long-term improvements are illustrated in Figure 4F.

Short-term: Estimated cost = \$19,740.

- Install temporary curb with flex posts to create a pinchpoint for a mid-block crosswalk which would connect the easterly sidewalk with the internal path network.
- Install temporary curb with flex posts to reduce corner radii on all four corners of the Eastway / 44 Court Ridge Road intersection.
- Stripe a continental crosswalk across Ridge Road at the proposed mid-block crossing.
- Install advanced warning and pedestrian warning signage at new crossing.
- Restripe the STOP line on Ridge Road northbound, Ridge Road southbound, Eastway eastbound approaches closer to the intersection.
- Stripe the double yellow centerline on the Ridge Road northbound, Ridge Road southbound, Eastway eastbound approaches extending to the relocated STOP lines.
- Stripe share the road "sharrow" pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$55,220 not including costs for drainage improvements.

- Construct curb bump-out to create a pinchpoint for a raised, mid-block crosswalk which would connect the easterly sidewalk with the internal path network.
 - The recommendation for a raised crosswalk at this mid-block crossing is based on the overall straight alignment of Ridge Road which is more comfortable for drivers to travel at higher speeds and is a more appropriate speed control measure than STOP sign control.
- Construct curb bump-outs to reduce corner radii on all four corners of the Eastway / 44 Court Ridge Road intersection.
- Construct sidewalk connections on the southeast and southwest corners of the intersection and ADA-compliant ramps.
- Coordinate with WMATA to relocate the bus stop located on the northwest corner to the southwest corner and construct a new bus pad.
- Apply short-term striping recommendations excluding Ridge Road STOP lines.
- Stripe a continental crosswalk across Ridge Road northbound and southbound approaches to Eastway and across Eastway approach to improve access to relocated bus stops.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 33 MPH and high volume of collisions along this segment of roadway recorded. The reduction of three on-street parking spaces is anticipated as a result of these improvements.



Raised crosswalk with updated signage

Removal of on-street parking blocking bus stop

Relocate bus stop to departure side of intersection



Access to internal path network is hidden and provides no connection to greater sidewalk network

LEGEND

- PERVIOUS AREA
- SIDEWALK / RAMP
- DRIVEWAY RECONSTRUCTION

FIGURE 4F
RIDGE ROAD FROM 46 COURT RIDGE ROAD TO EASTWAY / 44 COURT RIDGE ROAD IMPROVEMENTS

33 Ridge Road – Priority High

A sight distance analysis study was performed by GPI December 2022 which confirmed the limited sight distances at the driveways at 33 Ridge Road¹³. The recommended long-term improvements are illustrated on the following page. The recommended long-term improvements are illustrated in Figure 4G.

Short-term: Estimated cost = \$143.

- Remove four on-street parking spaces north of the southerly driveway opposite Hamilton Place.
- Remove three on-street parking spaces south of the southerly driveway.
- Install flex posts to create to shadow out the 33 Ridge Road driveway opening further from the edge of curb.

¹³ Sight Distance Analysis, 33 Ridge Road; Greenman-Pedersen, Inc.; December 9, 2022.

Ridge Road / Gardenway – Priority High

The recommended long-term improvements are illustrated in Figure 4H.

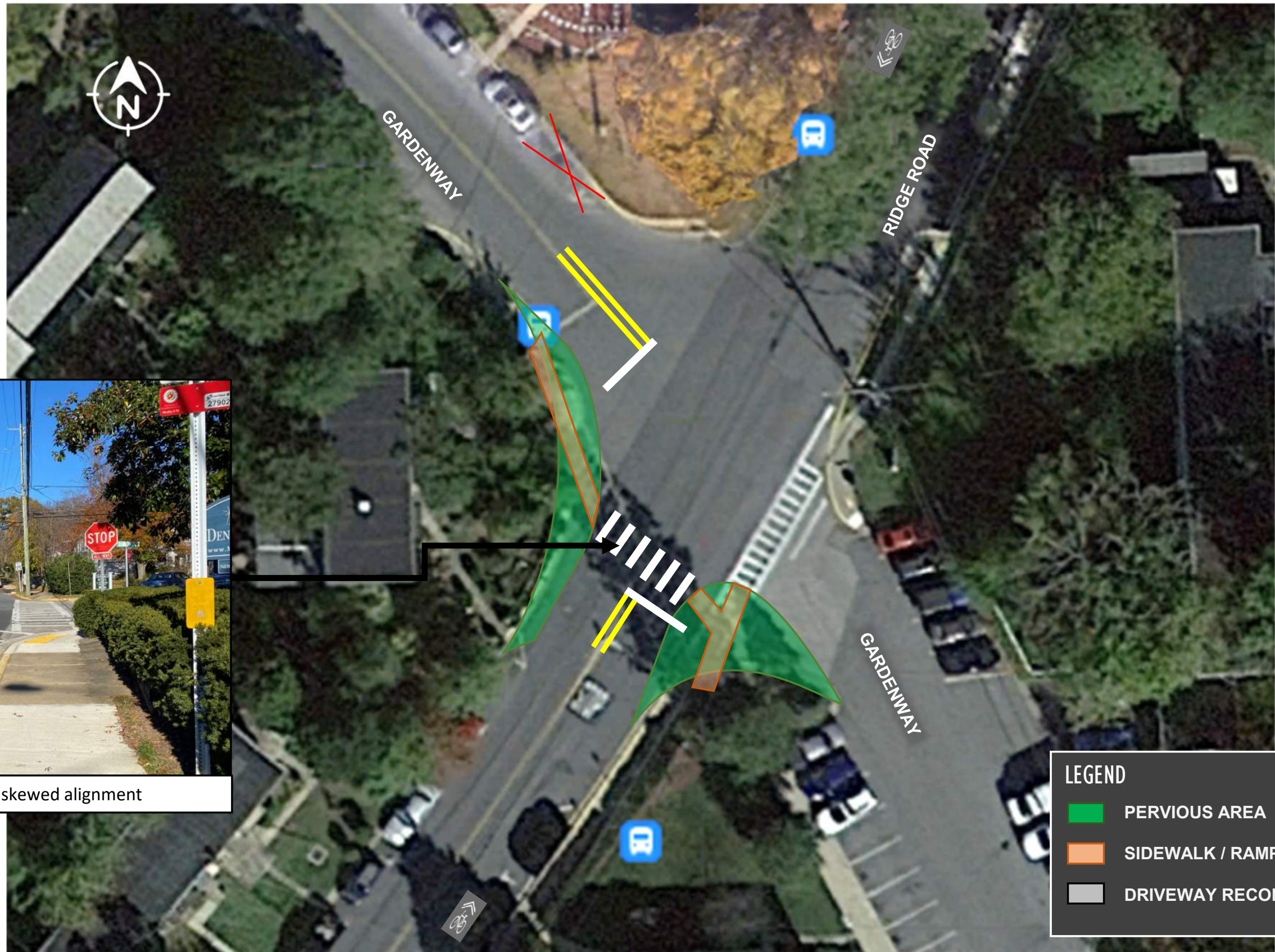
Short-term: Estimated cost = \$21,470.

- Install temporary curb with flex posts to reduce corner radii on the southeast and southwest corners of the intersection.
- Restripe a continental crosswalk across the Ridge Road approach.
- Restripe the STOP line on the Ridge Road northbound and Gardenway eastbound approaches closer to the intersection.
- Stripe the double yellow centerline on the Ridge Road northbound and Gardenway eastbound approaches extending to the relocated STOP lines.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$28,440 not including costs for drainage improvements.

- Construct semi-mountable curb bump-outs to reduce corner radii on the southeast and southwest corners of the intersection. Lack of crosswalk across Gardenway eastbound approach allows for use of semi-mountable treatment without affecting location of pedestrian ramps.
 - Adjustment of curb lines will require updates to the drainage inlet on the southeast corner.
 - Adjustment to curb radii will need to be evaluated to WMATA bus turning paths to ensure that all turning movements can be executed.
- Construct sidewalk connections on the southeast and southwest corners of the intersection and ADA-compliant ramps.
- Apply short-term striping recommendations.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 31 MPH recorded.



Existing crosswalk offers poor visibility and skewed alignment

FIGURE 4H
RIDGE ROAD / GARDENWAY IMPROVEMENTS

21 – 24 Court Ridge Road – Priority High

The recommended long-term improvements are illustrated in Figure 4I.

Short-term: Estimated cost = \$11,050.

- Install temporary curb with flex posts to create a pinchpoint at the Ridge Road northbound and southbound approach to the 21 Court Ridge Road / 22 Court Ridge Road driveway entrances.
- Install temporary curb with flex posts to create a pinchpoint at the Ridge Road northbound and southbound approaches to the mid-block crosswalk north of the 23 Court Ridge Road / 24 Court Ridge Road driveway entrances.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$44,480 not including costs for drainage improvements.

- Construct curb bump-outs to create a pinchpoint at the Ridge Road northbound and southbound approach to the 21 Court Ridge Road / 22 Court Ridge Road driveway entrances.
- Construct curb bump-outs to create a pinchpoint at the Ridge Road northbound and southbound approaches to the mid-block crosswalk north of the 23 Court Ridge Road / 24 Court Ridge Road driveway entrances.
- Remove existing traffic signal at the Ridge Road southbound approach to the 21 Court Ridge Road / 22 Court Ridge Road driveway entrances and replace with MdmUTCD-compliant rectangular rapid flashing beacons (RRFBs).
 - Existing equipment does not meet current MdmUTCD or ADA standards.
 - Additional analysis would need to be performed to confirm warranting conditions for a traffic signal to remain and be reconstructed.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 31 MPH and high volume of collisions along this segment of roadway recorded. No reduction of on-street parking spaces is anticipated as a result of these improvements.



FIGURE 41
21 – 24 COURT RIDGE ROAD IMPROVEMENTS

Ridge Road / Southway – Priority High

The recommended long-term improvements are illustrated in Figure 4J.

Short-term: Estimated cost = \$32,180.

- Install temporary curb with flex posts to reduce corner radii on all four corners of the intersection and provide 11-foot wide lanes approaching and departing.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$91,660 not including costs for drainage improvements.

- Construct curb bump-outs to reduce corner radii on all four corners of the intersection and provide 11-foot wide lanes approaching and departing.
 - Adjustment of curb lines will require updates to the drainage inlet on the northeast corner.
- Reconstruct sidewalk connections at the intersection and ADA-compliant ramps except for the southeast corner.
- Reconstruct Southway northbound and southbound medians to provide pedestrian refuge areas.
- Restripe continental crosswalks on all four approaches.
- Restripe the STOP line on all four approaches closer to the intersection.
- Stripe the double yellow centerline on the Ridge Road eastbound and westbound approaches extending to the relocated STOP lines.
- Restripe the double yellow centerline on the Ridge Road westbound approach to accommodate proposed bump-outs and provide 11-foot wide travel lanes east of the intersection.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 30-31 MPH recorded and the concentration of intersection collisions. No reduction of on-street parking spaces is anticipated as a result of these improvements.

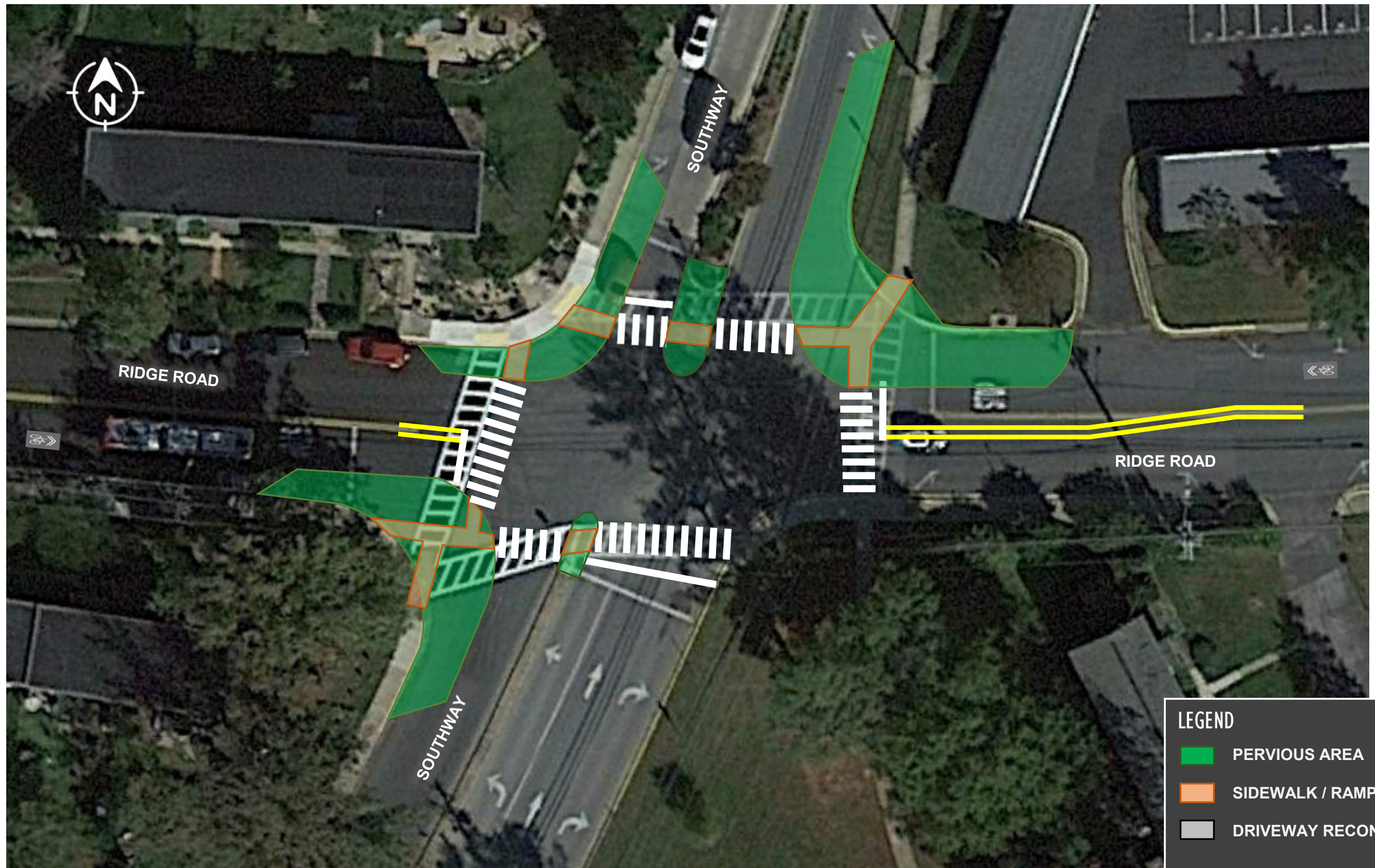


FIGURE 4J
RIDGE ROAD / SOUTHWAY IMPROVEMENTS

Ridge Road between Southway and Westway – Priority Medium

The recommended long-term improvements are illustrated in Figure 4K.

Short-term: Estimated cost = \$16,480.

- Install temporary curb with flex posts to create pinchpoints at three pathway entrances.
- Install temporary curb with flex posts to reduce radii at the southwest and southeast corners of the Ridge Road / Westway intersection.
- Restripe continental crosswalks for the three mid-block crosswalks.
- Stripe share the road “sharrow” pavement markings in both directions along Ridge Road.

Long-Term: Estimated cost = \$58,860 not including costs for drainage improvements.

- Construct curb bump-outs to create pinchpoints at three pathway entrances.
- Construct curb bump-outs to reduce radii at the southwest and southeast corners of the Ridge Road / Westway intersection.
- Construct a raised, mid-block crosswalk at the existing crossing west of 11 Court Ridge Road and at the mid-block crosswalk between 14 Court Ridge Road and Southway.
- Install advanced warning and pedestrian warning signage at new crossing.
- Apply short-term striping recommendations.
- Stripe continental crosswalks for Westway northbound and Ridge Road westbound approaches.
- Restripe STOP line for Ridge Road westbound approach behind proposed crosswalk.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 30 MPH recorded and connections to the internal path network. Previous studies noted that there is overall speed compliance in this area; however, more recent data suggests that additional steps may be required for compliance with the posted speed limit. No reduction of on-street parking spaces is anticipated as a result of these improvements.



Entrances to crosswalk obstructed by parked vehicles



Raised crosswalk with updated signage

Reconstruct ramp at 16 Court Ridge Road to avoid conflict with utility pole

LEGEND

- PERVIOUS AREA
- SIDEWALK / RAMP
- DRIVEWAY RECONSTRUCTION

FIGURE 4K
RIDGE ROAD BETWEEN SOUTHWAY AND WESTWAY IMPROVEMENTS

Lakeside Drive from Lakeview Circle to Olivewood Court – Priority Medium

The recommended long-term improvements are illustrated in Figure 5A.

Short-term: Estimated cost = \$10,810.

- Install STOP control signage and pavement markings to replace current YIELD conditions at the intersections with Olivewood Court, Maplewood Court, and Lakeview Circle.
- Install temporary curb with flex posts to remove on-street parking within the intersections with Olivewood Court and Lakeview Court.

Long-Term: Estimated cost = \$96,080 not including costs for drainage improvements or impacts to utilities.

- Construct curb bump-outs along east side of Lakeside Drive across from Lakeview Circle and Olivewood Court to condense the intersections and formalize limits of on-street parking.
 - This will provide a secondary benefit of providing pedestrian access from Lakeview Circle and Olivewood Court to the internal path network.
- Construct a five-foot wide sidewalk along the westerly side of Lakeside Drive from Lakeview Court to Olivewood Court. The City right-of-way extends approximately nine feet beyond the current edge of roadway where sidewalk is being proposed based on measurements made using Prince George's County GIS database <https://www.pgatlas.com/>.
- Reconstruct ramps at the existing pedestrian crossing at Maplewood Court to provide a more perpendicular crossing and condense the intersection.
- Reapply continental crosswalk pavement markings at realigned crossing.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 28 MPH recorded and connections to the internal path network. It is estimated that these recommended improvements will result in the reduction of approximately three on-street parking spaces.



FIGURE 5A
LAKESIDE DRIVE FROM LAKEVIEW CIRCLE TO OLIVEWOOD COURT IMPROVEMENTS

Lakeside Drive from Lakecrest Drive to Westway – Priority Medium

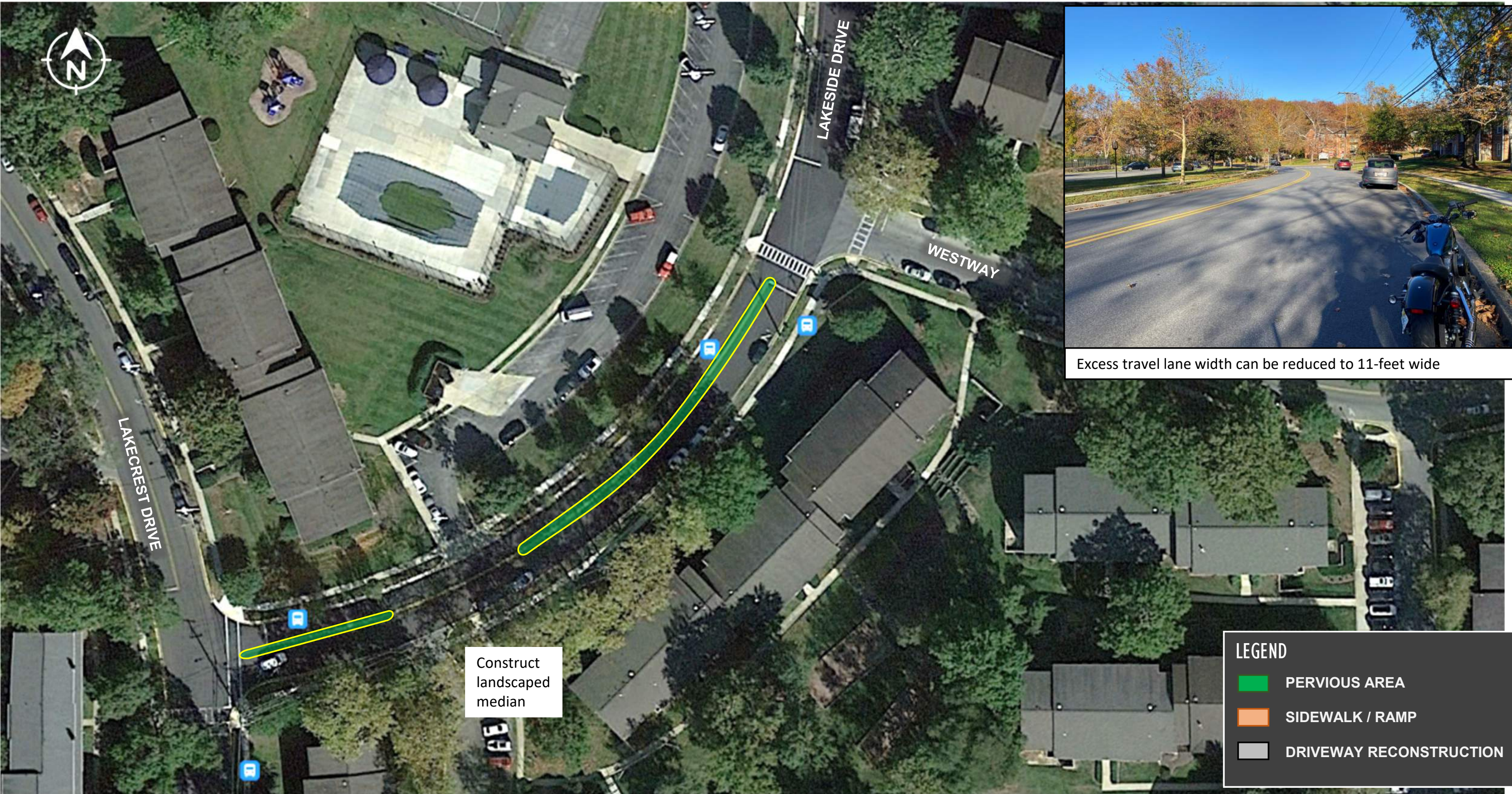
The recommended long-term improvements are illustrated in Figure 5B.

Short-term: Estimated cost = \$41,770.

- Stripe hatched median with flex posts along Lakeside Drive to reduce travel lane width to 11 feet wide in each direction.
- Install temporary curb with flex posts to create a median which narrows the travel lane width to 11-feet wide in each direction.

Long-Term: Estimated cost = \$76,980 not including costs for drainage improvements or impacts to utilities.

- Construct raised landscaped median along Lakeside Drive to reduce travel lane width to 11 feet wide in each direction.



Construct landscaped median

Excess travel lane width can be reduced to 11-foot wide

LEGEND

- PERVIOUS AREA
- SIDEWALK / RAMP
- DRIVEWAY RECONSTRUCTION

FIGURE 5B
 LAKESIDE DRIVE FROM WESTWAY TO LAKECREST DRIVE IMPROVEMENTS

Lastner Lane from Julian Court to Hedgewood Drive – Priority High

The recommended long-term improvements are illustrated in Figure 6A.

Short-term: Estimated cost = \$23,800.

- Stripe continental crosswalks across the Julian Court and Hedgewood Drive approaches to Lastner Lane.
- Install advanced warning and pedestrian warning signage at new crossing.
- Install temporary curb with flex posts to remove on-street parking within the intersections with Julian Court and Hedgewood Drive.
- Install temporary curb with flex posts to reduce corner radii on the Julian Court and Hedgewood Drive approaches to Lastner Lane.

Long-Term: Estimated cost = \$50,430 not including costs for drainage improvements.

- Construct curb bump-outs to reduce corner radii on Julian Court and Hedgewood Drive and extend curb on opposite side of intersection to remove on-street parking within the intersections.
 - Adjustment of curb lines will require updates to drainage inlets at Hedgewood Drive.
- Construct sidewalk connections and ramps for crosswalks across Lastner Lane at the southbound approach to Julian Court and the northbound approach to Hedgewood Drive.
- Install advanced warning and pedestrian warning signage at new crossing.
- Apply short-term striping recommendations.
- Coordinate with WMATA to relocate the bus stop located on the northeast corner of the Lastner Lane / Julian Court intersection to the northwest corner and construct a new bus pad.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 29 MPH recorded. It is estimated that these recommended improvements will result in the reduction of approximately three on-street parking spaces.



FIGURE 6A
 LASTNER LANE FROM JULIAN COURT TO HEDGEWOOD DRIVE IMPROVEMENTS

Lastner Lane / Rosewood Drive – Priority Medium

The recommended long-term improvements are illustrated in Figure 6B.

Short-term: Estimated cost = \$15,790.

- Stripe a continental crosswalk across the Rosewood Drive approach to Lastner Lane.
- Install temporary curb with flex posts to remove on-street parking within the intersection.

Long-Term: Estimated cost = \$30,820 not including costs for drainage improvements.

- Construct curb bump-outs to reduce corner radii and extend curb on opposite side of intersection to remove on-street parking within the intersection and improve sight lines.
 - Adjustment of curb lines will require update to drainage inlet on southeast corner of the intersection.
- Construct sidewalk connections and ramps for crosswalks across both Lastner Lane approaches.
- Install advanced warning and pedestrian warning signage at new crossings.
- Apply short-term striping recommendations.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 30 MPH recorded. It is estimated that these recommended improvements will result in the reduction of approximately two on-street parking spaces.



FIGURE 6B
 LASTNER LANE / ROSEWOOD DRIVE IMPROVEMENTS

Lastner Lane / Ivy Lane – Priority Medium

The recommended long-term improvements are illustrated in Figure 6C.

Short-term: Estimated cost = \$21,810.

- Stripe a continental crosswalk across the Ivy Lane approach to Lastner Lane.
- Install temporary curb with flex posts to remove on-street parking within the intersection.

Long-Term: Estimated cost = \$41,780 not including costs for drainage improvements.

- Construct curb bump-outs to reduce corner radii and extend curb on opposite side of intersection to remove on-street parking within the intersection and improve sight lines.
 - Adjustment of curb lines will require update to drainage inlet on northeast side of the intersection in front of 116 Lastner Lane.
- Construct sidewalk connections and ramps for crosswalks across the Lastner Lane southbound approach.
- Apply short-term striping recommendations.
- Coordinate with WMATA to construct a new bus pad.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 30 MPH recorded. It is estimated that these recommended improvements will result in the reduction of approximately two on-street parking spaces.



LEGEND

- PERVIOUS AREA
- SIDEWALK / RAMP
- DRIVEWAY RECONSTRUCTION

FIGURE 6C
LASTNER LANE / IVY LANE IMPROVEMENTS

Lastner Lane / Crescent Road – Priority Medium

The recommended long-term improvements are illustrated in Figure 6D.

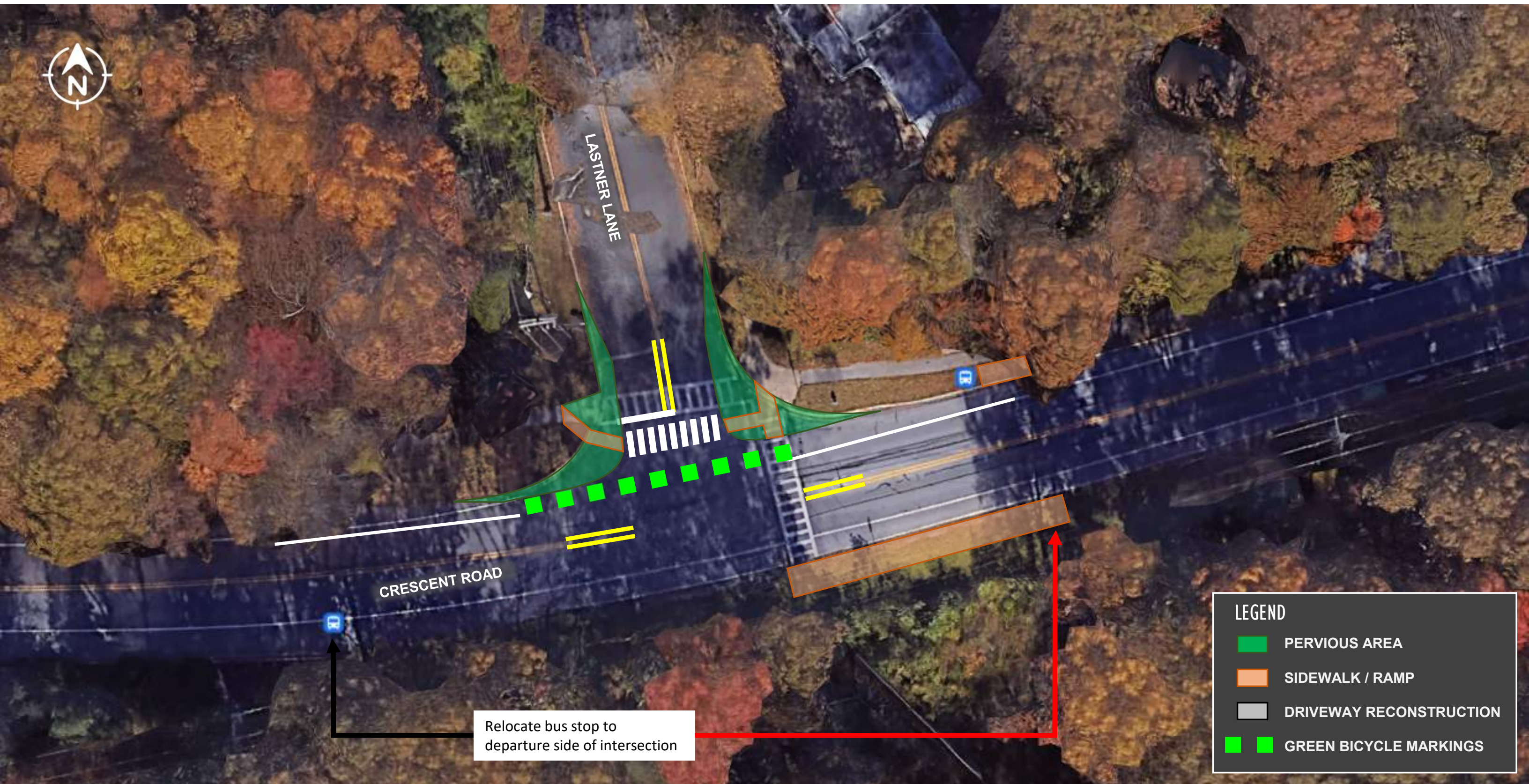
Short-term: Estimated cost = \$23,930.

- Stripe green bicycle pavement markings across the Lastner Lane southbound approach.
- Restripe existing roadway shoulder along the northerly side of Crescent Road, west of the intersection, to provide a continuous bicycle lane.
- Install temporary curb with flex posts to reduce turning radii.

Long-Term: Estimated cost = \$44,250.

- Construct curb bump-outs to reduce corner radii and improve sight lines.
- Reconstruct sidewalk connections and ramps for crosswalks across the Lastner Lane southbound and Crescent Road westbound approaches.
- Apply short-term striping recommendations.
- Restripe continental crosswalk and extend STOP line across the Lastner Lane southbound approach closer to the intersection.
- Stripe the double yellow centerline on the Lastner Lane extending to the relocated STOP line.
- Coordinate with PG DPW&T to relocate the bus stop located on the southwest corner of the intersection to the southeast corner and construct a new bus pad.
- Coordinate with PG DPW&T to construct a new bus pad at existing stop on northeast corner of intersection.
 - This stop is not recommended for relocation due to the existing shelter.

Traffic calming improvements are recommended in this location based on the 85th percentile speed of 30 MPH recorded. No reduction of on-street parking spaces is anticipated as a result of these improvements.



Relocate bus stop to departure side of intersection

LEGEND	
	PERVIOUS AREA
	SIDEWALK / RAMP
	DRIVEWAY RECONSTRUCTION
	GREEN BICYCLE MARKINGS

Greenhill Road / Crescent Road – Priority Medium

The recommended long-term improvements are illustrated in Figure 7A.

Short-term: Estimated cost = \$17,330.

- Stripe green bicycle pavement markings across the Lastner Lane southbound approach.
- Install temporary curb with flex posts to reduce turning radii.

Long-Term: Estimated cost = \$32,820.

- Construct curb bump-outs to reduce corner radii and improve sight lines.
- Reconstruct sidewalk connections and ramps for crosswalks across the Greenhill Road southbound and Crescent Road westbound approaches.
- Apply short-term striping recommendations.
- Coordinate with PG DPW&T to construct new bus pads and provide connection to internal path network.

No reduction of on-street parking spaces is anticipated as a result of these improvements.

Previous studies and the most recent data collected show that 85th percentile travel speeds are +/- 5 MPH within the posted speed limit. In addition, the average annual collision crash rate is less than one per year indicating that a critical safety issue does not exist. There were six recorded vehicles traveling at ≥ 35 MPH over the most recent 72-hour data collection. However, GPI notes that improvements could be made to address other concerns regarding sight lines and pedestrian connectivity which could also have the benefit of traffic calming.



FIGURE 7A
GREENHILL ROAD / CRESCENT ROAD IMPROVEMENTS

Greenhill Road / Orange Court / Greenway Place – Priority Medium

The recommended long-term improvements are illustrated in Figure 7B.

Short-term: Estimated cost = \$13,040.

- Install temporary curb with flex posts to reduce corner radii on all four corners of the intersection.
- Stripe a continental crosswalk across the Orange Court and Greenway Place approaches where access ramps are currently provided.

Long-Term: Estimated cost = \$29,790 not including costs for drainage improvements.

- Construct curb bump-outs to reduce corner radii on all four corners of the intersection.
 - Adjustment of curb lines will require updates to drainage inlet on southeast corner.
- Construct sidewalk connections on all four corners of the intersection to create crossings for the Greenhill Road northbound and southbound approaches.
- Apply short-term striping recommendations.
- Stripe continental crosswalks and restripe STOP lines for the Greenhill Road northbound and southbound approaches.
- Coordinate with PG DPW&T to relocate the bus stop located on the northwest corner to the southwest corner and construct a new bus pad.

No reduction of on-street parking spaces is anticipated as a result of these improvements.

Previous studies and the most recent data collected show that 85th percentile travel speeds are +/- 5 MPH within the posted speed limit. In addition, the average annual collision crash rate is less than one per year indicating that a critical safety issue does not exist. There were six recorded vehicles traveling at ≥ 35 MPH over the most recent 72-hour data collection. However, GPI notes that improvements could be made to address other concerns regarding sight lines and pedestrian connectivity which could also have the benefit of traffic calming.

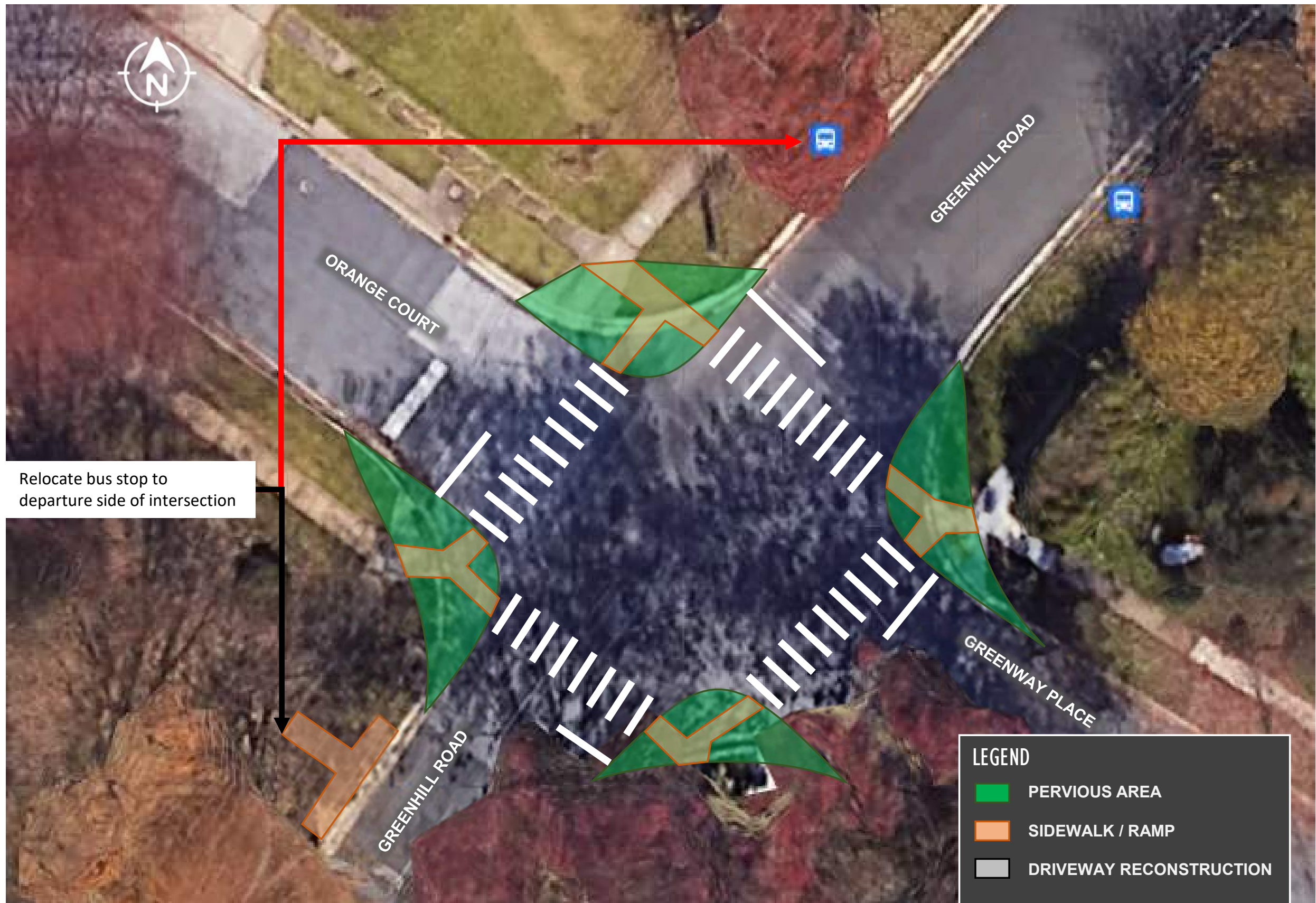


FIGURE 7B
GREENHILL ROAD / ORANGE COURT / GREENWAY PLACE IMPROVEMENTS

Greenhill Road / Hillside Road / Research Road – Priority Medium

The recommended long-term improvements are illustrated in Figure 7C.

Short-term: Estimated cost = \$13,330.

- Install temporary curb with flex posts to reduce corner radii on the Research Road southbound approach.

Long-Term: Estimated cost = \$28,820 not including costs for drainage improvements.

- Construct curb bump-outs to reduce corner radii on the Research Road southbound approach.
 - Adjustment of curb lines will require update to drainage inlet on northwest corner.
- Reconstruct ramps for Research Road crosswalk.
- Construct ramps and sidewalk connections for the Hillside Road westbound approach.
- Stripe continental crosswalks and restripe STOP line for the Hillside Road westbound approach.
- Install advanced warning and pedestrian warning signage at new crossing.

No reduction of on-street parking spaces is anticipated as a result of these improvements.

Previous studies and the most recent data collected show that 85th percentile travel speeds are +/- 5 MPH within the posted speed limit. In addition, the average annual collision crash rate is less than one per year indicating that a critical safety issue does not exist. There were six recorded vehicles traveling at ≥ 35 MPH over the most recent 72-hour data collection. However, GPI notes that improvements could be made to address other concerns regarding sight lines and pedestrian connectivity which could also have the benefit of traffic calming.



FIGURE 7C
GREENHILL ROAD / HILLSIDE ROAD / RESEARCH ROAD IMPROVEMENTS

Hillside Road from Northway to Woodland Way – Priority Medium

The recommended long-term improvements are illustrated in Figure 8A.

Short-term: Estimated cost = \$27,050.

- Install temporary curb with flex posts to reduce corner radii on all four corners of the Hillside Road / Northway intersection.
- Install temporary curb with flex posts to reduce corner radii on the Woodland Way eastbound approach to Hillside Road.
- Install advanced pedestrian warning signage at existing crosswalk located at 12 Hillside Road and between Northway and Woodland Way.

Long-Term: Estimated cost = \$83,850 not including costs for drainage improvements.

- Construct raised crosswalk at the existing mid-block crosswalk located at 12 Hillside Road to improve pedestrian visibility and access to the internal path network.
 - The recommendation for a raised crosswalk at this mid-block crossing is based on the desire to reduce vehicle speed northbound prior to the roadway curve and provide a consistent pedestrian facility where visibility is improved beyond parked vehicles in the existing head-in spaces.
 - This treatment will further enhance recent striping and sidewalk improvements.
 - Install advanced warning and pedestrian warning signage at new crossing.
- Construct curb bump-outs to reduce corner radii all four corners of the Hillside Road / Northway intersection and on the Woodland Way eastbound approach to Hillside Road.
 - Adjustment of curb lines will require update to drainage inlets on the northeast and northwest corner of Hillside Road / Northway.
- Reconstruct existing ramps and construct new connections for crosswalks across Hillside Road at Northway.
- Coordinate with PG DPW&T to construct new pinchpoint between Northway and Woodland Way for bus pads and provide connection to internal path network.
 - This mid-block crossing location was recently restriped with on-street parking spaces directly adjacent to crosswalk markings.
- Stripe continental crosswalks and restripe STOP line for the Hillside Road westbound approach to Northway, Northway westbound approach, Woodland Way eastbound approach.
- Maintain striped continental crosswalk at the proposed pinchpoint crossing between bus pads.

It is estimated that these recommended improvements will result in the reduction of approximately four on-street parking spaces. The 85th percentile speeds along Hillside Road north of Northway reflect similar speed characteristics as Greenhill Road.



FIGURE 8A
HILLSIDE ROAD FROM NORTHWAY TO WOODLAND WAY IMPROVEMENTS

Hillside Road / Crescent Road – Priority Medium

The recommended long-term improvements are illustrated in Figure 8B.

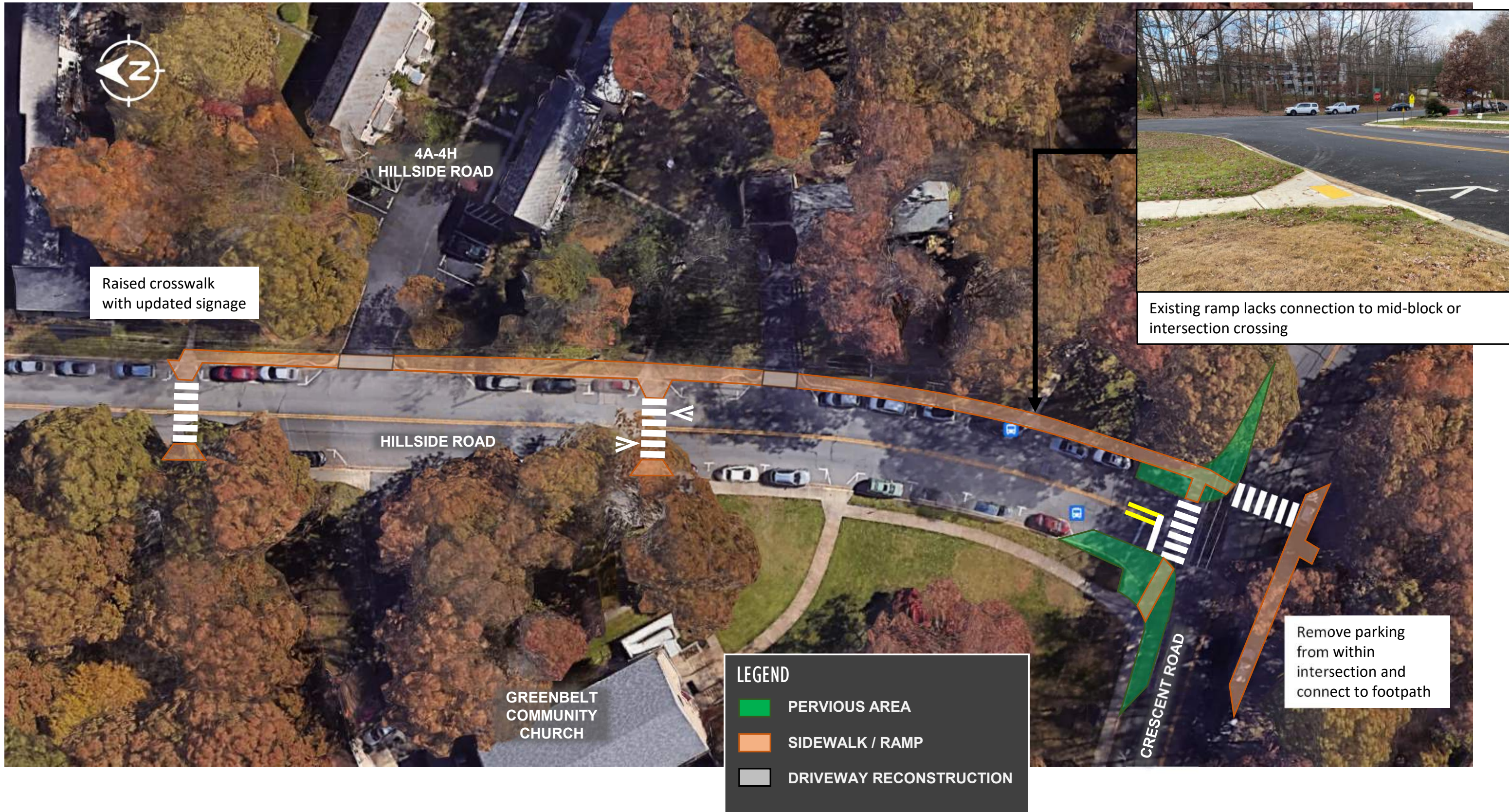
Short-term: Estimated cost = \$20,270.

- Install temporary curb with flex posts to reduce corner radii on the Hillside Road southbound approach to Crescent Road.
- Install temporary curb with flex posts north of 4A-4H Hillside Road driveway to create a pinchpoint and mid-block crossing.
- Install advanced warning and pedestrian crossing signage at proposed mid-block crossing.
- Stripe a continental crosswalk at the proposed pinchpoint crossing.

Long-Term: Estimated cost = \$77,450.

- Construct curb bump-outs to reduce corner radii on the Hillside Road southbound approach to Crescent Road.
 - Adjustment of curb lines will require update to drainage inlets on the northeast and northwest corner of Hillside Road / Crescent Road.
- Construct a raised crosswalk north of 4A-4H Hillside Road driveway to create a pinchpoint and mid-block crossing.
- Install advanced warning and pedestrian warning signage at new crossing.
- Reconstruct existing ramps and construct new connections for crosswalks across Hillside Road southbound approach and Crescent Road westbound approach.
- Construct five-foot wide sidewalk along the easterly side of Hillside Road from Crescent Road to the proposed mid-block crossing and remove existing ramps which terminate at Hillside Road without crosswalk connections.
 - The recommended sidewalk construction will require the reconstruction of two driveway aprons but is not anticipated to affect any existing drainage structures.
- Construct five-foot wide sidewalk along the southerly side of Crescent Road to remove parking within the intersection and provide connection to existing footpath.
- Stripe continental crosswalks for proposed mid-block crossing, Hillside Road southbound approach and Crescent Road westbound approach.
- Restripe STOP line for the Hillside Road southbound approach.
- Restripe on-street parking spaces on easterly side of Hillside Road as needed based on the relocation of ramp access.
- Install advanced warning and pedestrian warning signage at new crossings.

It is estimated that these recommended improvements will result in the net reduction of approximately four on-street parking spaces. The 85th percentile speed along Hillside south of Woodland Way are +/- 7 MPH above the speed limit. In addition, there were 115 recorded vehicles traveling at ≥35 MPH over the most recent 72-hour data collection.



Existing ramp lacks connection to mid-block or intersection crossing

FIGURE 8B
HILLSIDE ROAD / CRESCENT ROAD IMPROVEMENTS

CONCLUSIONS AND RECOMMENDATIONS

The study area roadways, except for Lakeside Drive, provide sidewalk along at least one side for its entire length and while the construction of continuous sidewalks along both sides of the roadway would improve pedestrian accessibility, it would not necessarily address pedestrian safety or reduce vehicle travel speeds. Connections to the existing sidewalk and internal path network have been recommended in conjunction with traffic calming measures to improve overall walkability. GPI notes additional access points to the trail network exist and may also be considered for these treatment options in future pedestrian connectivity efforts. The existing roadway widths are insufficient for providing dedicated bicycle lanes; however, the anticipated reduction of vehicle speeds with the proposed improvements will create a safer environment for cyclists to operate within the roadway.

The recommended long-term improvements include reconstruction of intersections to create bump-outs or pinch-points by adjusting curb radii which has the benefit of creating passive control with permanent infrastructure. The removal of pavement and added grass areas will reduce demand on drainage systems and have an overall positive environmental impact. The long-term recommendations are generally more expensive because they require additional resources and materials to implement; however, they generally require less maintenance.

The proposed locations for raised crosswalks can be downgraded to short-term improvements by striping crosswalks, installing/relocating pedestrian warning signage, and installing flexible bollards to shadow out further from the edge of curb.

The recommended short-term improvements would seek to create similar conditions as the long-term improvements by providing flexible bollards (less than 3.5 feet high) and updated signage and pavement markings. It is still recommended that ramps be reconstructed, as needed, to provide ADA compliance with the latest standards. No pavement would be removed under short-term conditions and more easily applied pavement marking paint can be considered. Placement of signage, bollards, and paint can still yield lower travel speeds by forcing drivers to alter their driving behavior through narrower travel lanes, tighter turning radii, and improving visibility for pedestrians and cyclists.

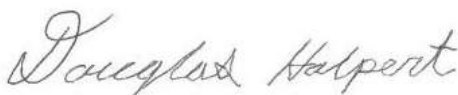
All recommended improvements seek to address the City of Greenbelt's Pedestrian / Bike Master Plan goals of improving pedestrian/bicycle safety and mobility. Physical manipulation of the study area roadways, including key intersections, though horizontal and vertical deflection create an environment where it will be uncomfortable for drivers to travel a higher speed and thus creating roadways which are more forgiving in terms of the severity of incidents. With the above recommended improvements, the goal of reducing vehicle travel speeds can be accomplished without active enforcement. Both long-term and short-term recommendations have been made with the conscious effort of improving access to public transit. Some of these recommendations have been made at the cost of removing on-street parking spaces. No parking occupancy study has been performed to confirm if number of parking spaces is adequately meeting demand nor the potential for additional spaces in other locations.

It is recommended that the City of Greenbelt continue their outreach efforts to educate its residents about any roadway improvements that are implemented and explain the reasoning for design. This will help reinforce cultural norms of acceptable driving behaviors in a residential area.

Should you have any questions or require additional information, please feel free to contact me at (443) 753-5546.

Sincerely,

Greenman-Pedersen, Inc.



Douglas S. Halpert, P.E., PTOE
Project Manager

APPENDIX

- A) TRAFFIC COUNT DATA
- B) PUBLIC TRANSPORTATION ROUTES
- C) CITY OF GREENBELT COLLISION DATA
- D) CONCEPTUAL COST ESTIMATES
- E) TRAFFIC CALMING RANKING SYSTEM

APPENDIX A – TRAFFIC COUNT DATA

CLASSIFICATION
Ridge Rd Bet. Laurel Hill Rd & Plateau Pl

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_001

Time	NORTHBOUND													SOUTHBOUND													TOTALS																		
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total			
12:00	1	3	2	0	0	0	0	0	0	0	0	0	0	6	1	8	3	1	0	0	0	0	0	0	0	0	13	2	11	5	1	0	0	0	0	0	0	0	0	0	0	0	19		
12:15	1	6	2	0	0	0	0	0	0	0	0	0	0	9	0	9	2	1	0	0	0	0	0	0	0	0	12	1	15	4	1	0	0	0	0	0	0	0	0	0	0	0	21		
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19:30	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	7	0	0	0	0	0	0	0	0	0	0	7	0	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
19:45	0	6	0	2	0	0	0	0	0	0	0	0	0	8	0	3	0	2	0	0	0	0	0	0	0	0	5	0	9	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	13
20:00	0	7	0	0	0	0	0	0	0	0	0	0	0	7	0	6	0	0	0	0	0	0	0	0	0	0	6	0	13	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13
20:15	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	3	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
20:30	0	3	1	0	0	0	0	0	0	0	0	0	0	4	0	0	1	0	0	0	0	0	0	0	0	0	1	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5
20:45	0	5	0	3	1	0	0	0	0	0	0	0	0	9	0	5	0	1	0	0	0	0	0	0	0	0	6	0	10	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	15
21:00	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	3	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6
21:15	0	3	0	1	0	0	0	0	0	0	0	0	0	4	0	1	0	1	0	0	0	0	0	0	0	0	2	0	4	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	6
21:30	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	2	0	0	0	0	0	0	0	0	0	0	2	0	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7
21:45	0	2	0	2	0	0	0	0	0	0	0	0	0	4	0	4	0	1	0	0	0	0	0	0	0	0	5	0	6	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	9
22:00	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	3	0	0	0	0	0	0	0	0	0	0	3	0	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8
22:15	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	2	0	4	0	0	0	0	0											

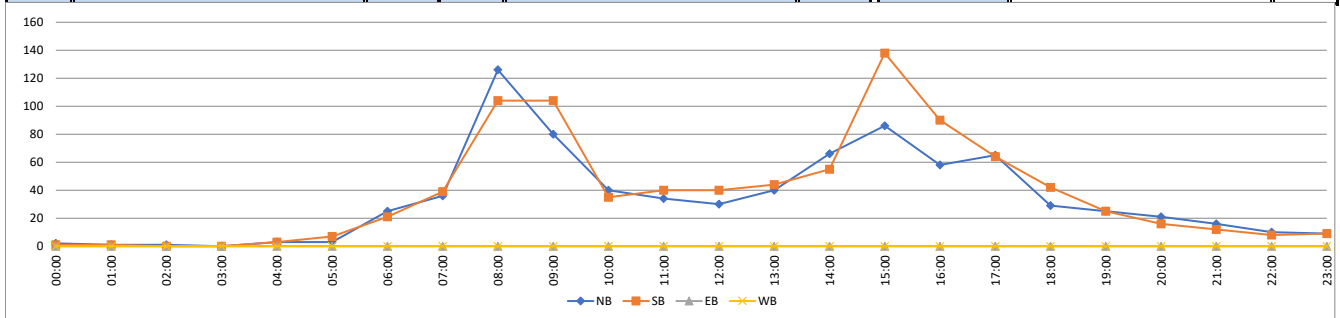
VOLUME

Ridge Rd Bet. Laurel Hill Rd & Plateau PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_001

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					806	898	0	0	1,704									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	0	0			0	12:00	6	13			19	00:00	01:00	2	1			3
00:15	1	0			1	12:15	9	12			21	01:00	02:00	1	1			2
00:30	1	1			2	12:30	4	8			12	02:00	03:00	1	0			1
00:45	0	0			0	12:45	11	7			18	03:00	04:00	0	0			0
01:00	0	0			0	13:00	13	8			21	04:00	05:00	3	3			6
01:15	1	0			1	13:15	8	10			18	05:00	06:00	3	7			10
01:30	0	0			0	13:30	8	11			19	06:00	07:00	25	21			46
01:45	0	1			1	13:45	11	15			26	07:00	08:00	36	39			75
02:00	0	0			0	14:00	13	12			25	08:00	09:00	126	104			230
02:15	0	0			0	14:15	13	17			30	09:00	10:00	80	104			184
02:30	0	0			0	14:30	19	15			34	10:00	11:00	40	35			75
02:45	1	0			1	14:45	21	11			32	11:00	12:00	34	40			74
03:00	0	0			0	15:00	26	22			48	12:00	13:00	30	40			70
03:15	0	0			0	15:15	30	50			80	13:00	14:00	40	44			84
03:30	0	0			0	15:30	15	39			54	14:00	15:00	66	55			121
03:45	0	0			0	15:45	15	27			42	15:00	16:00	86	138			224
04:00	1	1			2	16:00	17	19			36	16:00	17:00	58	90			148
04:15	1	1			2	16:15	18	16			34	17:00	18:00	65	64			129
04:30	0	0			0	16:30	14	35			49	18:00	19:00	29	42			71
04:45	1	1			2	16:45	9	20			29	19:00	20:00	25	25			50
05:00	0	4			4	17:00	14	18			32	20:00	21:00	21	16			37
05:15	0	0			0	17:15	20	15			35	21:00	22:00	16	12			28
05:30	2	2			4	17:30	21	17			38	22:00	23:00	10	8			18
05:45	1	1			2	17:45	10	14			24	23:00	00:00	9	9			18
06:00	7	2			9	18:00	15	14			29	STATISTICS						
06:15	5	5			10	18:15	5	9			14		NB	SB	EB	WB	TOTAL	
06:30	6	6			12	18:30	6	8			14	Peak Period	00:00	to	12:00			
06:45	7	8			15	18:45	3	11			14	Volume	351	355			706	
07:00	7	9			16	19:00	9	12			21	Peak Hour	8:15	8:30			8:30	
07:15	10	8			18	19:15	3	1			4	Peak Volume	146	152			295	
07:30	9	10			19	19:30	5	7			12	Peak Hour Factor	0.702	0.667			0.768	
07:45	10	12			22	19:45	8	5			13	Peak Period	12:00	to	00:00			
08:00	19	19			38	20:00	7	6			13	Volume	455	543			998	
08:15	31	19			50	20:15	1	3			4	Peak Hour	14:30	15:00			15:00	
08:30	24	29			53	20:30	4	1			5	Peak Volume	96	138			224	
08:45	52	37			89	20:45	9	6			15	Peak Hour Factor	0.800	0.690			0.700	
09:00	39	57			96	21:00	3	3			6	Peak Period	07:00	to	09:00			
09:15	28	29			57	21:15	4	2			6	Volume	162	143			305	
09:30	6	8			14	21:30	5	2			7	Peak Hour	8:00	8:00			8:00	
09:45	7	10			17	21:45	4	5			9	Peak Volume	126	104			230	
10:00	13	6			19	22:00	5	3			8	Peak Hour Factor	0.606	0.703			0.646	
10:15	12	10			22	22:15	2	2			4	Peak Period	16:00	to	18:00			
10:30	9	9			18	22:30	0	1			1	Volume	123	154			277	
10:45	6	10			16	22:45	3	2			5	Peak Hour	17:00	16:00			16:00	
11:00	8	8			16	23:00	2	2			4	Peak Volume	65	90			148	
11:15	9	12			21	23:15	1	2			3	Peak Hour Factor	0.774	0.643			0.755	
11:30	11	12			23	23:30	4	2			6							
11:45	6	8			14	23:45	2	3			5							
TOTALS	351	355	0	0	706	TOTALS	455	543	0	0	998							
SPLIT %	50%	50%	0%	0%	41%	SPLIT %	46%	54%	0%	0%	59%							



CLASSIFICATION

Ridge Rd Bet. Laurel Hill Rd & Plateau Pl

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_001

Time	NORTHBOUND													Total	SOUTHBOUND													Total	TOTALS													Total		
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13			
12:00	0	10	0	0	0	0	0	0	0	0	0	0	0	10	0	5	1	0	0	0	0	0	0	0	0	0	6	0	15	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16
12:15	1	13	0	1	0	0	0	0	0	0	0	0	0	15	0	13	0	1	1	0	0	0	0	0	0	0	15	1	26	0	2	1	0	0	0	0	0	0	0	0	0	0	30	
12:30	1	2	0	0	0	0	0	0	0	0	0	0	0	3	0	14	1	2	0	0	0	0	0	0	0	0	17	1	16	1	2	0	0	0	0	0	0	0	0	0	0	20		
12:45	0	7	0	0	1	0	0	0	0	0	0	0	0	8	0	7	0	0	0	0	0	0	0	0	0	0	7	0	14	0	0	1	0	0	0	0	0	0	0	0	0	0	15	
13:00	0	8	2	0	0	0	0	0	0	0	0	0	0	10	0	15	3	1	0	0	0	0	0	0	0	0	19	0	23	5	1	0	0	0	0	0	0	0	0	0	0	29		
13:15	0	9	3	0	0	0	0	0	0	0	0	0	0	12	0	8	3	0	0	0	0	0	0	0	0	0	11	0	17	6	0	0	0	0	0	0	0	0	0	0	0	23		
13:30	0	8	1	3	1	0	0	0	0	0	0	0	0	13	0	12	1	3	0	0	0	0	0	0	0	0	16	0	20	2	6	1	0	0	0	0	0	0	0	0	0	29		
13:45	0	11	1	0	0	0	0	0	0	0	0	0	0	12	0	10	1	0	0	0	0	0	0	0	0	0	11	0	21	2	0	0	0	0	0	0	0	0	0	0	0	23		
14:00	2	13	6	0	1	0	0	0	0	0	0	0	0	22	0	12	6	0	0	0	0	0	0	0	0	0	18	2	25	12	0	1	0	0	0	0	0	0	0	0	0	40		
14:15	1	11	3	2	0	0	0	0	0	0	0	0	0	17	1	6	2	1	0	0	0	0	0	0	0	0	10	2	17	5	3	0	0	0	0	0	0	0	0	0	0	27		
14:30	0	4	5	0	1	0	0	0	0	0	0	0	0	10	0	5	2	0	0	0	0	0	0	0	0	0	10	0	9	10	0	1	0	0	0	0	0	0	0	0	0	0	20	
14:45	0	15	3	1	1	0	0	0	0	0	0	0	0	20	0	13	3	1	0	0	0	0	0	0	0	0	17	0	28	6	2	1	0	0	0	0	0	0	0	0	0	0	37	
15:00	0	17	3	0	0	0	0	0	0	0	0	0	0	20	0	13	3	2	1	0	0	0	0	0	0	0	19	0	30	6	2	1	0	0	0	0	0	0	0	0	0	39		
15:15	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	50	6	2	2	0	0	0	0	0	0	0	60	0	53	6	2	2	0	0	0	0	0	0	0	0	63			
15:30	0	12	2	2	0	0	0	0	0	0	0	0	0	16	0	24	3	3	0	0	0	0	0	0	0	0	30	0	36	5	5	0	0	0	0	0	0	0	0	0	0	46		
15:45	0	11	1	2	0	0	0	0	0	0	0	0	0	14	0	24	3	3	0	0	0	0	0	0	0	0	30	0	35	4	5	0	0	0	0	0	0	0	0	0	0	44		
16:00	0	8	0	1	0	0	0	0	0	0	0	0	0	9	0	13	1	1	0	0	0	0	0	0	0	0	15	0	21	1	2	0	0	0	0	0	0	0	0	0	0	24		
16:15	1	13	0	3	0	0	0	0	0	0	0	0	0	17	0	18	0	2	0	0	0	0	0	0	0	0	20	1	31	0	5	0	0	0	0	0	0	0	0	0	0	37		
16:30	1	13	0	1	1	0	0	0	0	0	0	0	0	16	1	15	0	1	0	0	0	0	0	0	0	0	17	2	28	0	2	1	0	0	0	0	0	0	0	0	0	33		
16:45	1	12	0	0	1	0	0	0	0	0	0	0	0	14	0	11	0	0	0	0	0	0	0	0	0	0	11	1	23	0	0	1	0	0	0	0	0	0	0	0	0	25		
17:00	0	10	0	1	0	0	0	0	0	0	0	0	0	11	0	21	1	2	0	0	0	0	0	0	0	0	24	0	31	1	3	0	0	0	0	0	0	0	0	0	0	35		
17:15	0	9	1	0	0	0	0	0	0	0	0	0	0	10	0	9	1	0	0	0	0	0	0	0	0	0	10	0	18	2	0	0	0	0	0	0	0	0	0	0	0	20		
17:30	0	14	1	1	0	0	0	0	0	0	0	0	0	16	0	11	0	1	0	0	0	0	0	0	0	0	12	0	25	1	2	0	0	0	0	0	0	0	0	0	0	28		
17:45	0	14	2	1	1	0	0	0	0	0	0	0	0	18	0	11	2	1	0	0	0	0	0	0	0	0	14	0	25	4	2	1	0	0	0	0	0	0	0	0	0	32		
18:00	0	9	0	0	0	0	0	0	0	0	0	0	0	9	0	12	0	0	1	0	0	0	0	0	0	0	13	0	21	0	0	1	0	0	0	0	0	0	0	0	0	22		
18:15	0	8	0	2	0	0	0	0	0	0	0	0	0	10	0	12	0	1	0	0	0	0	0	0	0	0	13	0	20	0	3	0	0	0	0	0	0	0	0	0	0	23		
18:30	0	3	3	0	0	0	0	0	0	0	0	0	0	6	0	6	3	0	0	0	0	0	0	0	0	0	9	0	9	6	0	0	0	0	0	0	0	0	0	0	0	15		
18:45	0	5	1	1	0	0	0	0	0	0	0	0	0	7	0	7	1	1	0	0	0	0	0	0	0	0	9	0	12	2	2	0	0	0	0	0	0	0	0	0	0	16		
19:00	0	7	0	0	1	0	0	0	0	0	0	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	8	0	15	0	0	1	0	0	0	0	0	0	0	0	0	0	16	
19:15	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	4	0	0	0	0	0	0	0	0	0	0	4	0	9	0	0	0	0	0	0	0	0	0	0	0	0	9		
19:30	0	2	0	1	0	0	0	0	0	0	0	0	0	3	0	4	0	1	0	0	0	0	0	0	0	0	5	0	6	0	2	0	0	0	0	0	0	0	0	0	0	8		
19:45	0	4	0	2	0	0	0	0	0	0	0	0	0	6	0	8	0	1	0	0	0	0	0	0	0	0	9	0	12	0	3	0	0	0	0	0	0	0	0	0	0	15		
20:00	0	7	0	0	0	0	0	0	0	0	0	0	0	7	0	8	0	0	0	0	0	0	0	0	0	0	8	0	15	0	0	0	0	0	0	0	0	0	0	0	0	15		
20:15	0	8	1	0	0	0	0	0	0	0	0	0	0	9	0	8	1	0	0	0	0	0	0	0	0	0	9	0	16	2	0	0	0	0	0	0	0	0	0	0	0	18		
20:30	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	4	0	0	0	0	0	0	0	0	0	0	4	0	9	0	0	0	0	0	0	0	0	0	0	0	0	9		
20:45	0	8	0	1	0	0	0	0	0	0	0	0	0	9	0	5	0	1	0	0	0	0	0	0	0	0	6	0	13	0	2	0	0	0	0	0	0	0	0	0	0	0	15	
21:00	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	3	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
21:15	0	3	0	1	0	0	0	0	0	0	0	0	0	4	0	3	0	1	0	0	0	0	0	0	0	0	4	0	6	0	2	0	0	0	0	0	0	0	0	0	0	0	8	
21:30	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
21:45	0	2	0	1	0	0	0	0	0	0	0	0	0	3	0	3	0	1	0	0	0	0	0	0	0	0	4	0	5	0	2	0	0	0	0	0	0	0	0	0	0	0	7	
22:00	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	2	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
22:15	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	3	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
22:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
22:45	0	0	0	0																																								

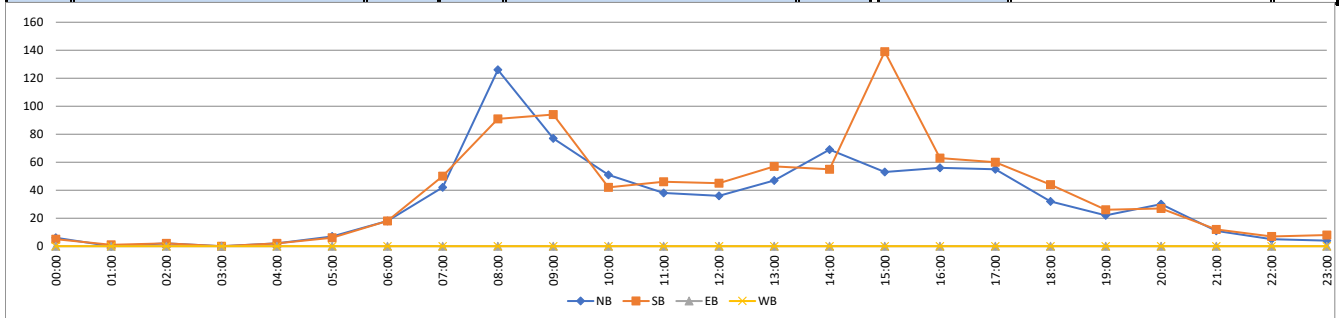
VOLUME

Ridge Rd Bet. Laurel Hill Rd & Plateau PI

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_001

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS						
						789	900	0	0	1,689							
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	1	1			2	12:00	10	6			16	00:00	01:00	6	5		11
00:15	4	3			7	12:15	15	15			30	01:00	02:00	0	1		1
00:30	0	0			0	12:30	3	17			20	02:00	03:00	2	2		4
00:45	1	1			2	12:45	8	7			15	03:00	04:00	0	0		0
01:00	0	1			1	13:00	10	19			29	04:00	05:00	2	2		4
01:15	0	0			0	13:15	12	11			23	05:00	06:00	7	6		13
01:30	0	0			0	13:30	13	16			29	06:00	07:00	18	18		36
01:45	0	0			0	13:45	12	11			23	07:00	08:00	42	50		92
02:00	2	2			4	14:00	22	18			40	08:00	09:00	126	91		217
02:15	0	0			0	14:15	17	10			27	09:00	10:00	77	94		171
02:30	0	0			0	14:30	10	10			20	10:00	11:00	51	42		93
02:45	0	0			0	14:45	20	17			37	11:00	12:00	38	46		84
03:00	0	0			0	15:00	20	19			39	12:00	13:00	36	45		81
03:15	0	0			0	15:15	3	60			63	13:00	14:00	47	57		104
03:30	0	0			0	15:30	16	30			46	14:00	15:00	69	55		124
03:45	0	0			0	15:45	14	30			44	15:00	16:00	53	139		192
04:00	0	0			0	16:00	9	15			24	16:00	17:00	56	63		119
04:15	1	1			2	16:15	17	20			37	17:00	18:00	55	60		115
04:30	1	1			2	16:30	16	17			33	18:00	19:00	32	44		76
04:45	0	0			0	16:45	14	11			25	19:00	20:00	22	26		48
05:00	0	1			1	17:00	11	24			35	20:00	21:00	30	27		57
05:15	3	1			4	17:15	10	10			20	21:00	22:00	11	12		23
05:30	1	1			2	17:30	16	12			28	22:00	23:00	5	7		12
05:45	3	3			6	17:45	18	14			32	23:00	00:00	4	8		12
06:00	4	4			8	18:00	9	13			22	STATISTICS					
06:15	4	4			8	18:15	10	13			23		NB	SB	EB	WB	TOTAL
06:30	3	2			5	18:30	6	9			15	Peak Period	00:00	to	12:00		
06:45	7	8			15	18:45	7	9			16	Volume	369	357			726
07:00	5	5			10	19:00	8	8			16	Peak Hour	8:15	8:30			8:30
07:15	11	11			22	19:15	5	4			9	Peak Volume	138	132			267
07:30	12	15			27	19:30	3	5			8	Peak Hour Factor	0.575	0.688			0.674
07:45	14	19			33	19:45	6	9			15	Peak Period	12:00	to	00:00		
08:00	22	13			35	20:00	7	8			15	Volume	420	543			963
08:15	21	18			39	20:15	9	9			18	Peak Hour	14:00	15:00			15:00
08:30	23	21			44	20:30	5	4			9	Peak Volume	69	139			192
08:45	60	39			99	20:45	9	6			15	Peak Hour Factor	0.784	0.579			0.762
09:00	34	48			82	21:00	3	3			6	Peak Period	07:00	to	09:00		
09:15	18	24			42	21:15	4	4			8	Volume	168	141			309
09:30	12	11			23	21:30	1	1			2	Peak Hour	8:00	8:00			8:00
09:45	13	11			24	21:45	3	4			7	Peak Volume	126	91			217
10:00	20	9			29	22:00	2	2			4	Peak Hour Factor	0.525	0.583			0.548
10:15	7	16			23	22:15	3	3			6	Peak Period	16:00	to	18:00		
10:30	10	6			16	22:30	0	0			0	Volume	111	123			234
10:45	14	11			25	22:45	0	2			2	Peak Hour	16:15	16:15			16:15
11:00	4	10			14	23:00	1	1			2	Peak Volume	58	72			130
11:15	16	21			37	23:15	0	3			3	Peak Hour Factor	0.853	0.750			0.878
11:30	7	8			15	23:30	2	2			4						
11:45	11	7			18	23:45	1	2			3						
TOTALS	369	357	0	0	726	TOTALS	420	543	0	0	963						
SPLIT %	51%	49%	0%	0%	43%	SPLIT %	44%	56%	0%	0%	57%						



Prepared by National Data & Surveying Services
CLASSIFICATION
 Ridge Rd Bet. Laurel Hill Rd & Plateau Pl

Day: Saturday
 Date: 11/18/2023

City: Greenbelt
 Project #: MD23_280047_001

Time	NORTHBOUND													Total	SOUTHBOUND													Total	TOTALS													Total
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	
12:00	1	6	2	2	0	0	0	0	0	0	0	0	0	11	0	7	2	1	0	0	0	0	0	0	0	10	1	13	4	3	0	0	0	0	0	0	0	0	0	0	0	21
12:15	0	8	0	0	0	0	0	0	0	0	0	0	0	8	0	5	0	0	0	0	0	0	0	0	0	5	0	13	0	0	0	0	0	0	0	0	0	0	0	0	0	13
12:30	0	4	1	0	0	0	0	0	0	0	0	0	0	5	0	4	1	0	0	0	0	0	0	0	5	0	8	2	0	0	0	0	0	0	0	0	0	0	0	0	10	
12:45	0	5	2	0	0	0	0	0	0	0	0	0	0	7	0	3	2	0	0	0	0	0	0	0	5	0	8	4	0	0	0	0	0	0	0	0	0	0	0	0	12	
13:00	0	7	0	1	0	0	0	0	0	0	0	0	0	8	0	9	0	1	0	0	0	0	0	0	10	0	16	0	2	0	0	0	0	0	0	0	0	0	0	0	21	
13:15	0	7	0	0	0	0	0	0	0	0	0	0	0	7	0	8	0	0	1	0	0	0	0	0	9	0	15	0	0	1	0	0	0	0	0	0	0	0	0	16		
13:30	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	9	1	0	0	0	0	0	0	0	10	0	13	1	0	0	0	0	0	0	0	0	0	0	0	14		
13:45	0	11	2	0	0	0	0	0	0	0	0	0	0	13	0	7	1	0	0	0	0	0	0	0	8	0	18	3	0	0	0	0	0	0	0	0	0	0	0	21		
14:00	0	8	1	2	0	0	0	0	0	0	0	0	0	11	0	9	1	1	0	0	0	0	0	0	11	0	17	2	3	0	0	0	0	0	0	0	0	0	0	22		
14:15	0	7	0	0	0	0	0	0	0	0	0	0	0	7	0	6	0	0	1	0	0	0	0	7	0	13	0	0	1	0	0	0	0	0	0	0	0	0	0	14		
14:30	1	1	0	0	0	0	0	0	0	0	0	0	0	2	0	7	1	0	0	0	0	0	0	8	1	8	1	0	0	0	0	0	0	0	0	0	0	0	0	10		
14:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	1	0	0	0	0	0	0	5	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5			
15:00	0	0	0	1	0	0	0	0	0	0	0	0	0	1	1	3	0	1	0	0	0	0	0	5	1	3	0	2	0	0	0	0	0	0	0	0	0	0	6			
15:15	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	5	0	0	0	0	0	0	0	5	0	10	0	0	0	0	0	0	0	0	0	0	0	0	10			
15:30	0	3	1	0	0	0	0	0	0	0	0	0	0	4	0	5	1	0	0	0	0	0	0	6	0	8	2	0	0	0	0	0	0	0	0	0	0	0	10			
15:45	0	5	1	0	0	0	0	0	0	0	0	0	0	6	0	5	1	0	0	0	0	0	0	6	0	10	2	0	0	0	0	0	0	0	0	0	0	0	12			
16:00	0	8	0	1	1	0	0	0	0	0	0	0	0	10	0	8	1	1	0	0	0	0	0	10	0	16	1	2	1	0	0	0	0	0	0	0	0	0	20			
16:15	0	6	1	0	0	0	0	0	0	0	0	0	0	7	1	4	1	0	0	0	0	0	0	6	1	10	2	0	0	0	0	0	0	0	0	0	0	0	13			
16:30	0	2	1	0	0	0	0	0	0	0	0	0	0	3	0	6	1	0	0	0	0	0	0	6	0	8	2	0	0	0	0	0	0	0	0	0	0	0	10			
16:45	0	6	0	0	0	0	0	0	0	0	0	0	0	6	0	6	0	0	0	0	0	0	0	6	0	12	0	0	0	0	0	0	0	0	0	0	0	0	12			
17:00	0	8	0	1	0	0	0	0	0	0	0	0	0	9	0	4	0	1	0	0	0	0	0	5	0	12	0	2	0	0	0	0	0	0	0	0	0	0	14			
17:15	0	6	0	0	0	0	0	0	0	0	0	0	0	6	0	10	0	0	0	0	0	0	0	10	0	16	0	0	0	0	0	0	0	0	0	0	0	0	16			
17:30	0	8	2	0	0	0	0	0	0	0	0	0	0	10	0	4	2	0	0	0	0	0	0	6	0	12	4	0	0	0	0	0	0	0	0	0	0	0	16			
17:45	0	6	0	0	0	0	0	0	0	0	0	0	0	6	0	6	0	0	0	0	0	0	0	6	0	12	0	0	0	0	0	0	0	0	0	0	0	0	12			
18:00	0	7	0	1	0	0	0	0	0	0	0	0	0	8	0	5	0	1	0	0	0	0	0	6	0	12	0	2	0	0	0	0	0	0	0	0	0	0	14			
18:15	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	5	0	1	0	0	0	0	0	6	0	9	0	1	0	0	0	0	0	0	0	0	0	0	10			
18:30	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	5	0	0	0	0	0	0	0	5	0	8	0	0	0	0	0	0	0	0	0	0	0	0	8			
18:45	0	1	1	0	0	0	0	0	0	0	0	0	0	2	0	2	1	0	1	0	0	0	0	4	0	3	2	0	1	0	0	0	0	0	0	0	0	0	6			
19:00	0	1	0	2	0	0	0	0	0	0	0	0	0	3	0	2	0	1	0	0	0	0	0	3	0	3	0	3	0	0	0	0	0	0	0	0	0	0	6			
19:15	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	5	0	0	0	0	0	0	0	5	0	8	0	0	0	0	0	0	0	0	0	0	0	0	8			
19:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
19:45	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	2	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4			
20:00	0	3	0	1	0	0	0	0	0	0	0	0	0	4	0	5	0	1	0	0	0	0	0	6	0	8	0	2	0	0	0	0	0	0	0	0	0	0	10			
20:15	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	5	0	0	0	0	0	0	0	5	0	10	0	0	0	0	0	0	0	0	0	0	0	0	10			
20:30	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	1	0	0	0	0	0	0	0	1	0	6	0	0	0	0	0	0	0	0	0	0	0	0	6			
20:45	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	6	0	0	0	0	0	0	0	6	0	10	0	0	0	0	0	0	0	0	0	0	0	0	10			
21:00	0	2	0	1	0	0	0	0	0	0	0	0	0	3	0	2	0	1	0	0	0	0	0	3	0	4	0	2	0	0	0	0	0	0	0	0	0	0	6			
21:15	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	2	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4			
21:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
21:45	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	4	0	0	0	0	0	0	0	4	0	8	0	0	0	0	0	0	0	0	0	0	0	0	8			
22:00	0	3	0	1	0	0	0	0	0	0	0	0	0	4	0	3	0	0	0	0	0	0	0	3	0	6	0	1	0	0	0	0	0	0	0	0	0	0	7			
22:15	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	1	0	0	0	0	0	0	0	1	0	5	0	0	0	0	0	0	0	0	0	0	0	0	5			
22:30	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	5	0	0	0	0	0	0	0	5	0	8	0	0	0	0	0	0	0	0	0	0	0	0	8			
22:45	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	2	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4			
23:00	0	6	0	0	0	0	0	0	0	0	0	0	0	6	0	3	0	0	0	0	0	0	0	3	0	9	0	0	0	0	0	0	0	0	0	0	0	0	9			
23:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2			
23:30	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	1	0	2	0	0	0													

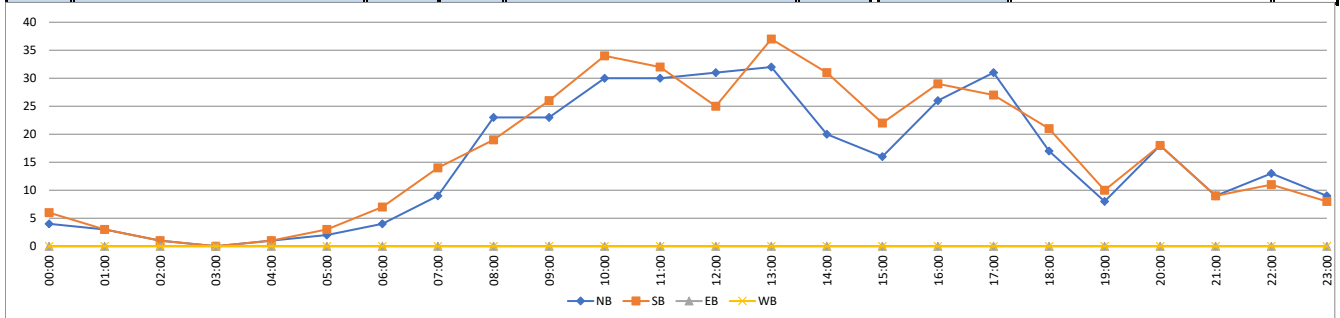
VOLUME

Ridge Rd Bet. Laurel Hill Rd & Plateau PI

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_001

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					360	394	0	0	754								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	1	2			3	12:00	11	10			21	00:00	01:00	4	6		10
00:15	1	0			1	12:15	8	5			13	01:00	02:00	3	3		6
00:30	2	3			5	12:30	5	5			10	02:00	03:00	1	1		2
00:45	0	1			1	12:45	7	5			12	03:00	04:00	0	0		0
01:00	0	1			1	13:00	8	10			18	04:00	05:00	1	1		2
01:15	0	0			0	13:15	7	9			16	05:00	06:00	2	3		5
01:30	1	2			3	13:30	4	10			14	06:00	07:00	4	7		11
01:45	2	0			2	13:45	13	8			21	07:00	08:00	9	14		23
02:00	1	1			2	14:00	11	11			22	08:00	09:00	23	19		42
02:15	0	0			0	14:15	7	7			14	09:00	10:00	23	26		49
02:30	0	0			0	14:30	2	8			10	10:00	11:00	30	34		64
02:45	0	0			0	14:45	0	5			5	11:00	12:00	30	32		62
03:00	0	0			0	15:00	1	5			6	12:00	13:00	31	25		56
03:15	0	0			0	15:15	5	5			10	13:00	14:00	32	37		69
03:30	0	0			0	15:30	4	6			10	14:00	15:00	20	31		51
03:45	0	0			0	15:45	6	6			12	15:00	16:00	16	22		38
04:00	0	0			0	16:00	10	10			20	16:00	17:00	26	29		55
04:15	1	1			2	16:15	7	6			13	17:00	18:00	31	27		58
04:30	0	0			0	16:30	3	7			10	18:00	19:00	17	21		38
04:45	0	0			0	16:45	6	6			12	19:00	20:00	8	10		18
05:00	1	1			2	17:00	9	5			14	20:00	21:00	18	18		36
05:15	0	0			0	17:15	6	10			16	21:00	22:00	9	9		18
05:30	1	1			2	17:30	10	6			16	22:00	23:00	13	11		24
05:45	0	1			1	17:45	6	6			12	23:00	00:00	9	8		17
06:00	0	2			2	18:00	8	6			14	STATISTICS					
06:15	1	1			2	18:15	4	6			10		NB	SB	EB	WB	TOTAL
06:30	2	1			3	18:30	3	5			8	Peak Period	00:00 to 12:00				
06:45	1	3			4	18:45	2	4			6	Volume	130	146			276
07:00	3	4			7	19:00	3	3			6	Peak Hour	10:45	10:30			10:30
07:15	1	1			2	19:15	3	5			8	Peak Volume	34	38			68
07:30	1	5			6	19:30	0	0			0	Peak Hour Factor	0.708	0.864			0.773
07:45	4	4			8	19:45	2	2			4	Peak Period	12:00 to 00:00				
08:00	10	6			16	20:00	4	6			10	Volume	230	248			478
08:15	2	5			7	20:15	5	5			10	Peak Hour	13:15	13:15			13:15
08:30	7	4			11	20:30	5	1			6	Peak Volume	35	38			73
08:45	4	4			8	20:45	4	6			10	Peak Hour Factor	0.673	0.864			0.830
09:00	6	7			13	21:00	3	3			6	Peak Period	07:00 to 09:00				
09:15	7	5			12	21:15	2	2			4	Volume	32	33			65
09:30	4	6			10	21:30	0	0			0	Peak Hour	7:45	7:30			7:45
09:45	6	8			14	21:45	4	4			8	Peak Volume	23	20			42
10:00	13	8			21	22:00	4	3			7	Peak Hour Factor	0.575	0.833			0.656
10:15	4	7			11	22:15	4	1			5	Peak Period	16:00 to 18:00				
10:30	5	11			16	22:30	3	5			8	Volume	57	56			113
10:45	8	8			16	22:45	2	2			4	Peak Hour	16:45	16:00			16:45
11:00	5	9			14	23:00	6	3			9	Peak Volume	31	29			58
11:15	12	10			22	23:15	0	2			2	Peak Hour Factor	0.775	0.725			0.906
11:30	9	7			16	23:30	1	1			2						
11:45	4	6			10	23:45	2	2			4						
TOTALS	130	146	0	0	276	TOTALS	230	248	0	0	478						
SPLIT %	47%	53%	0%	0%	37%	SPLIT %	48%	52%	0%	0%	63%						



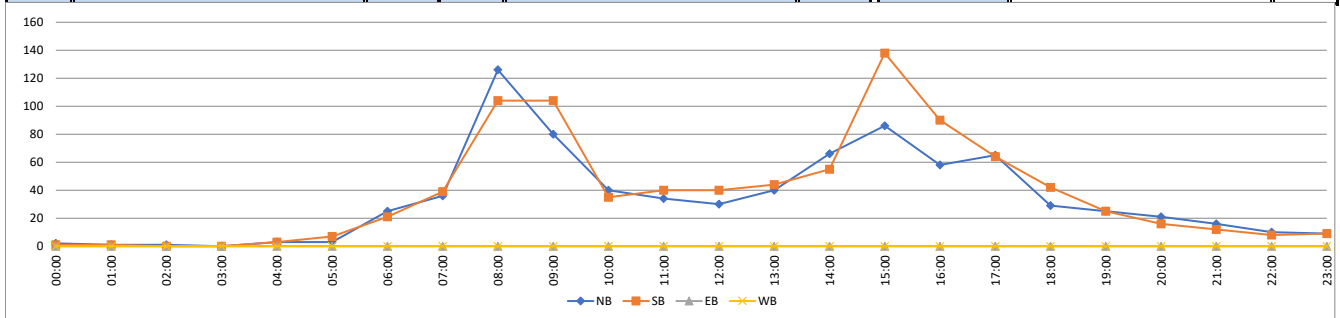
VOLUME

Ridge Rd Bet. Laurel Hill Rd & Plateau PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_001

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					806	898	0	0	1,704									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	0	0			0	12:00	6	13			19	00:00	01:00	2	1			3
00:15	1	0			1	12:15	9	12			21	01:00	02:00	1	1			2
00:30	1	1			2	12:30	4	8			12	02:00	03:00	1	0			1
00:45	0	0			0	12:45	11	7			18	03:00	04:00	0	0			0
01:00	0	0			0	13:00	13	8			21	04:00	05:00	3	3			6
01:15	1	0			1	13:15	8	10			18	05:00	06:00	3	7			10
01:30	0	0			0	13:30	8	11			19	06:00	07:00	25	21			46
01:45	0	1			1	13:45	11	15			26	07:00	08:00	36	39			75
02:00	0	0			0	14:00	13	12			25	08:00	09:00	126	104			230
02:15	0	0			0	14:15	13	17			30	09:00	10:00	80	104			184
02:30	0	0			0	14:30	19	15			34	10:00	11:00	40	35			75
02:45	1	0			1	14:45	21	11			32	11:00	12:00	34	40			74
03:00	0	0			0	15:00	26	22			48	12:00	13:00	30	40			70
03:15	0	0			0	15:15	30	50			80	13:00	14:00	40	44			84
03:30	0	0			0	15:30	15	39			54	14:00	15:00	66	55			121
03:45	0	0			0	15:45	15	27			42	15:00	16:00	86	138			224
04:00	1	1			2	16:00	17	19			36	16:00	17:00	58	90			148
04:15	1	1			2	16:15	18	16			34	17:00	18:00	65	64			129
04:30	0	0			0	16:30	14	35			49	18:00	19:00	29	42			71
04:45	1	1			2	16:45	9	20			29	19:00	20:00	25	25			50
05:00	0	4			4	17:00	14	18			32	20:00	21:00	21	16			37
05:15	0	0			0	17:15	20	15			35	21:00	22:00	16	12			28
05:30	2	2			4	17:30	21	17			38	22:00	23:00	10	8			18
05:45	1	1			2	17:45	10	14			24	23:00	00:00	9	9			18
06:00	7	2			9	18:00	15	14			29	STATISTICS						
06:15	5	5			10	18:15	5	9			14							
06:30	6	6			12	18:30	6	8			14	Peak Period	00:00 to 12:00			TOTAL		
06:45	7	8			15	18:45	3	11			14	Volume	351	355			706	
07:00	7	9			16	19:00	9	12			21	Peak Hour	8:15	8:30			8:30	
07:15	10	8			18	19:15	3	1			4	Peak Volume	146	152			295	
07:30	9	10			19	19:30	5	7			12	Peak Hour Factor	0.702	0.667			0.768	
07:45	10	12			22	19:45	8	5			13	Peak Period	12:00 to 00:00					
08:00	19	19			38	20:00	7	6			13	Volume	455	543			998	
08:15	31	19			50	20:15	1	3			4	Peak Hour	14:30	15:00			15:00	
08:30	24	29			53	20:30	4	1			5	Peak Volume	96	138			224	
08:45	52	37			89	20:45	9	6			15	Peak Hour Factor	0.800	0.690			0.700	
09:00	39	57			96	21:00	3	3			6	Peak Period	07:00 to 09:00					
09:15	28	29			57	21:15	4	2			6	Volume	162	143			305	
09:30	6	8			14	21:30	5	2			7	Peak Hour	8:00	8:00			8:00	
09:45	7	10			17	21:45	4	5			9	Peak Volume	126	104			230	
10:00	13	6			19	22:00	5	3			8	Peak Hour Factor	0.606	0.703			0.646	
10:15	12	10			22	22:15	2	2			4	Peak Period	16:00 to 18:00					
10:30	9	9			18	22:30	0	1			1	Volume	123	154			277	
10:45	6	10			16	22:45	3	2			5	Peak Hour	17:00	16:00			16:00	
11:00	8	8			16	23:00	2	2			4	Peak Volume	65	90			148	
11:15	9	12			21	23:15	1	2			3	Peak Hour Factor	0.774	0.643			0.755	
11:30	11	12			23	23:30	4	2			6							
11:45	6	8			14	23:45	2	3			5							
TOTALS	351	355	0	0	706	TOTALS	455	543	0	0	998							
SPLIT %	50%	50%	0%	0%	41%	SPLIT %	46%	54%	0%	0%	59%							



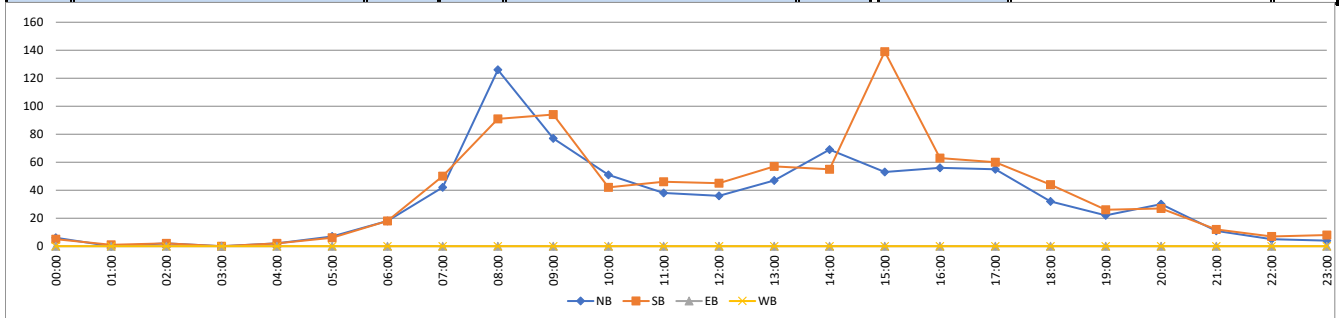
VOLUME

Ridge Rd Bet. Laurel Hill Rd & Plateau PI

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_001

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS																																																																																																																																										
						789	900	0	0	1,689																																																																																																																																											
15-Minutes Interval											Hourly Intervals																																																																																																																																										
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																				
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06:00	4	4			8	18:00	9	13			22	<table border="1"> <thead> <tr> <th colspan="6">STATISTICS</th> </tr> <tr> <th></th> <th>NB</th> <th>SB</th> <th>EB</th> <th>WB</th> <th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td> <td colspan="2">00:00 to 12:00</td> <td colspan="3"></td> </tr> <tr> <td>Volume</td> <td>369</td> <td>357</td> <td colspan="2"></td> <td>726</td> </tr> <tr> <td>Peak Hour</td> <td colspan="2">8:15 8:30</td> <td colspan="3"></td> </tr> <tr> <td>Peak Volume</td> <td>138</td> <td>132</td> <td colspan="2"></td> <td>267</td> </tr> <tr> <td>Peak Hour Factor</td> <td>0.575</td> <td>0.688</td> <td colspan="2"></td> <td>0.674</td> </tr> <tr> <td>Peak Period</td> <td colspan="2">12:00 to 00:00</td> <td colspan="3"></td> </tr> <tr> <td>Volume</td> <td>420</td> <td>543</td> <td colspan="2"></td> <td>963</td> </tr> <tr> <td>Peak Hour</td> <td colspan="2">14:00 15:00</td> <td colspan="3"></td> </tr> <tr> <td>Peak Volume</td> <td>69</td> <td>139</td> <td colspan="2"></td> <td>192</td> </tr> <tr> <td>Peak Hour Factor</td> <td>0.784</td> <td>0.579</td> <td colspan="2"></td> <td>0.762</td> </tr> <tr> <td>Peak Period</td> <td colspan="2">07:00 to 09:00</td> <td colspan="3"></td> </tr> <tr> <td>Volume</td> <td>168</td> <td>141</td> <td colspan="2"></td> <td>309</td> </tr> <tr> <td>Peak Hour</td> <td colspan="2">8:00 8:00</td> <td colspan="3"></td> </tr> <tr> <td>Peak Volume</td> <td>126</td> <td>91</td> <td colspan="2"></td> <td>217</td> </tr> <tr> <td>Peak Hour Factor</td> <td>0.525</td> <td>0.583</td> <td colspan="2"></td> <td>0.548</td> </tr> <tr> <td>Peak Period</td> <td colspan="2">16:00 to 18:00</td> <td colspan="3"></td> </tr> <tr> <td>Volume</td> <td>111</td> <td>123</td> <td colspan="2"></td> <td>234</td> </tr> <tr> <td>Peak Hour</td> <td colspan="2">16:15 16:15</td> <td colspan="3"></td> </tr> <tr> <td>Peak Volume</td> <td>58</td> <td>72</td> <td colspan="2"></td> <td>130</td> </tr> <tr> <td>Peak Hour Factor</td> <td>0.853</td> <td>0.750</td> <td colspan="2"></td> <td>0.878</td> </tr> </tbody> </table>						STATISTICS							NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume	369	357			726	Peak Hour	8:15 8:30					Peak Volume	138	132			267	Peak Hour Factor	0.575	0.688			0.674	Peak Period	12:00 to 00:00					Volume	420	543			963	Peak Hour	14:00 15:00					Peak Volume	69	139			192	Peak Hour Factor	0.784	0.579			0.762	Peak Period	07:00 to 09:00					Volume	168	141			309	Peak Hour	8:00 8:00					Peak Volume	126	91			217	Peak Hour Factor	0.525	0.583			0.548	Peak Period	16:00 to 18:00					Volume	111	123			234	Peak Hour	16:15 16:15					Peak Volume	58	72			130	Peak Hour Factor	0.853	0.750			0.878
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SPLIT %	51%	49%	0%	0%	43%	SPLIT %	44%	56%	0%	0%	57%																																																																																																																																										



SPEED

Ridge Rd Bet. Laurel Hill Rd & Plateau PI

Day: Saturday

City: Greenbelt

Date: 11/18/2023

Project #: MD23_280047_001

Time	NORTHBOUND										Total	SOUTHBOUND										Total	TOTALS										Total									
	5 15	15 20	20 25	25 30	30 35	35 40	40 45	45 50	50 55	55 60		60 65	65 70	70 99	5 15	15 20	20 25	25 30	30 35	35 40	40 45		45 50	50 55	55 60	60 65	65 70	70 99	5 15	15 20	20 25	25 30		30 35	35 40	40 45	45 50	50 55	55 60	60 65	65 70	70 99
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04:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
05:00	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	2	0	0	0	0	0	0	0	0	0	2		
05:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
05:30	0	0	0	0	1	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	2	0	0	0	0	0	0	0	0	0	2		
05:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	1		
06:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	0	0	1	1	0	0	0	0	0	0	0	0	2		
06:15	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	2	0	0	0	0	0	0	0	0	0	0	2		
06:30	0	0	0	2	0	0	0	0	0	0	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	3	0	0	0	0	0	0	0	0	0	3		
06:45	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	2	0	0	0	0	0	0	0	0	3	0	0	1	3	0	0	0	0	0	0	0	0	0	4		
07:00	0	0	2	1	0	0	0	0	0	0	0	0	3	0	0	2	2	0	0	0	0	0	0	0	0	4	0	0	4	3	0	0	0	0	0	0	0	0	0	7		
07:15	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	2	0	0	0	0	0	0	0	0	0	2		
07:30	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	1	2	1	1	0	0	0	0	0	5	0	0	2	2	1	1	0	0	0	0	0	0	0	0	6		
07:45	0	0	2	0	1	1	0	0	0	0	0	0	4	0	0	1	1	1	1	0	0	0	0	0	4	0	0	3	1	2	2	0	0	0	0	0	0	0	0	8		
08:00	0	0	2	7	1	0	0	0	0	0	0	0	10	0	1	0	5	0	0	0	0	0	0	0	6	0	0	1	2	12	1	0	0	0	0	0	0	0	16			
08:15	0	0	0	1	0	1	0	0	0	0	0	0	2	0	0	1	2	1	1	0	0	0	0	0	5	0	0	1	3	1	2	0	0	0	0	0	0	0	7			
08:30	0	0	1	4	2	0	0	0	0	0	0	0	7	0	0	0	3	1	0	0	0	0	0	0	4	0	0	1	7	3	0	0	0	0	0	0	0	0	11			
08:45	0	0	2	2	0	0	0	0	0	0	0	0	4	0	0	2	2	0	0	0	0	0	0	0	4	0	0	4	4	0	0	0	0	0	0	0	0	0	8			
09:00	0	0	2	2	2	0	0	0	0	0	0	0	6	0	0	2	3	2	0	0	0	0	0	0	7	0	0	4	5	4	0	0	0	0	0	0	0	0	13			
09:15	0	0	2	4	2	0	0	0	0	0	0	0	7	0	0	1	3	1	0	0	0	0	0	0	5	0	0	3	7	2	0	0	0	0	0	0	0	0	12			
09:30	0	0	1	1	2	0	0	0	0	0	0	0	4	0	0	2	2	0	0	0	0	0	0	0	6	0	0	3	3	4	0	0	0	0	0	0	0	0	10			
09:45	0	1	2	3	0	0	0	0	0	0	0	0	6	0	1	3	4	0	0	0	0	0	0	0	8	0	2	5	7	0	0	0	0	0	0	0	0	0	14			
10:00	0	4	8	1	0	0	0	0	0	0	0	0	13	0	1	3	3	1	0	0	0	0	0	0	8	0	5	11	4	1	0	0	0	0	0	0	0	0	21			
10:15	0	1	1	2	0	0	0	0	0	0	0	0	4	0	0	2	3	2	0	0	0	0	0	0	7	0	1	3	5	2	0	0	0	0	0	0	0	0	11			
10:30	0	2	1	2	0	0	0	0	0	0	0	0	5	0	2	3	4	2	0	0	0	0	0	0	11	0	4	4	6	2	0	0	0	0	0	0	0	0	0	16		
10:45	0	0	5	2	1	0	0	0	0	0	0	0	8	0	0	5	2	1	0	0	0	0	0	0	8	0	0	10	4	2	0	0	0	0	0	0	0	0	0	16		
11:00	0	0	3	0	1	1	0	0	0	0	0	0	5	0	0	4	2	2	1	0	0	0	0	0	9	0	0	7	2	3	2	0	0	0	0	0	0	0	0	14		
11:15	0	1	2																																							

SPEED

Ridge Rd Bet. Laurel Hill Rd & Plateau Pl

Day: Saturday

Date: 11/18/2023

City: Greenbelt

Project #: MD23_280047_001

Time	NORTHBOUND														Total	SOUTHBOUND														Total	TOTALS														Total		
	5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70			
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99						
12:00	1	1	2	2	5	0	0	0	0	0	0	0	0	11	0	1	2	2	4	1	0	0	0	0	0	0	10	1	2	4	4	9	1	0	0	0	0	0	0	0	0	0	0	0	21		
12:15	0	0	3	3	2	0	0	0	0	0	0	0	0	8	0	0	2	2	1	0	0	0	0	0	0	0	5	0	0	5	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13
12:30	0	1	3	1	0	0	0	0	0	0	0	0	0	5	0	1	3	1	0	0	0	0	0	0	0	0	5	0	2	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10
12:45	0	1	3	2	1	0	0	0	0	0	0	0	0	7	0	1	2	1	1	0	0	0	0	0	0	0	5	0	2	5	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
13:00	0	3	1	2	2	0	0	0	0	0	0	0	0	8	0	3	2	3	2	0	0	0	0	0	0	0	10	0	6	3	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18
13:15	0	2	2	2	1	0	0	0	0	0	0	0	0	7	0	2	3	3	1	0	0	0	0	0	0	0	9	0	4	5	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	16	
13:30	1	1	0	2	0	0	0	0	0	0	0	0	0	4	1	2	2	4	1	0	0	0	0	0	0	10	2	3	2	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
13:45	0	1	6	3	2	1	0	0	0	0	0	0	0	13	0	0	5	1	1	1	0	0	0	0	0	8	0	1	11	4	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	22	
14:00	0	2	1	7	1	0	0	0	0	0	0	0	0	11	0	2	1	7	1	0	0	0	0	0	0	11	0	4	2	14	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21	
14:15	0	0	0	3	4	0	0	0	0	0	0	0	0	7	0	0	1	2	4	0	0	0	0	0	0	7	0	0	1	5	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
14:30	0	1	1	0	0	0	0	0	0	0	0	0	0	2	0	2	3	2	1	0	0	0	0	0	0	8	0	3	4	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
14:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	1	0	0	0	0	0	0	5	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	
15:00	0	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	1	3	1	0	0	0	0	0	0	5	0	0	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
15:15	0	1	1	2	1	0	0	0	0	0	0	0	0	5	0	0	2	2	1	0	0	0	0	0	0	5	0	1	3	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
15:30	0	1	0	3	0	0	0	0	0	0	0	0	0	4	0	1	1	4	0	0	0	0	0	0	0	6	0	2	1	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
15:45	0	1	1	2	1	0	1	0	0	0	0	0	0	6	0	0	2	2	1	1	0	0	0	0	6	0	1	3	4	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	12	
16:00	0	1	2	6	1	0	0	0	0	0	0	0	0	10	0	1	2	6	1	0	0	0	0	0	0	10	0	2	4	12	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20	
16:15	0	1	2	3	1	0	0	0	0	0	0	0	0	7	0	0	2	2	1	1	0	0	0	0	6	0	1	4	5	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	13		
16:30	0	0	0	2	1	0	0	0	0	0	0	0	0	3	0	0	1	4	2	0	0	0	0	0	7	0	0	1	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
16:45	0	1	1	3	1	0	0	0	0	0	0	0	0	6	0	1	1	3	1	0	0	0	0	0	6	0	2	2	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	
17:00	0	0	3	5	1	0	0	0	0	0	0	0	0	9	0	0	2	3	0	0	0	0	0	0	5	0	0	5	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14		
17:15	0	1	2	3	0	0	0	0	0	0	0	0	0	6	0	1	3	5	1	0	0	0	0	0	10	0	2	5	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16		
17:30	0	1	5	3	1	0	0	0	0	0	0	0	0	10	0	0	4	1	1	0	0	0	0	0	6	0	1	9	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16		
17:45	0	0	2	3	1	0	0	0	0	0	0	0	0	6	0	0	2	3	1	0	0	0	0	0	6	0	0	4	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14		
18:00	0	0	2	6	0	0	0	0	0	0	0	0	0	8	0	0	1	5	0	0	0	0	0	0	6	0	0	3	11	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14		
18:15	0	0	1	1	2	0	0	0	0	0	0	0	0	4	0	0	2	1	2	1	0	0	0	0	6	0	0	3	2	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	10		
18:30	0	0	1	0	2	0	0	0	0	0	0	0	0	3	0	0	2	1	2	0	0	0	0	0	5	0	0	3	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8		
18:45	0	0	1	0	1	0	0	0	0	0	0	0	0	2	0	0	2	1	1	0	0	0	0	0	4	0	0	3	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6		
19:00	0	0	0	2	1	0	0	0	0	0	0	0	0	3	0	0	0	2	1	0	0	0	0	0	3	0	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6		
19:15	0	0	2	1	0	0	0	0	0	0	0	0	0	3	0	0	3	2	0	0	0	0	0	0	5	0	0	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8		
19:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
19:45	0	1	1	0	0	0	0	0	0	0	0	0	0	2	0	1	1	0	0	0	0	0	0	0	2	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
20:00	0	1	0	3	0	0	0	0	0	0	0	0	0	4	0	1	1	4	0	0	0	0	0	0	6	0	2	1	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
20:15	0	0	2	0	3	0	0	0	0	0	0	0	0	5	0	0	2	0	3	0	0	0	0	0	5	0	0	4	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10		
20:30	0	0	1	2	2	0	0	0	0	0	0	0	0	5	0	0	0	0	1	0	0	0	0	0	1	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6		
20:45	0	0	0	4	0	0	0	0	0	0	0	0	0	4	0	0	1	5	0	0	0	0	0	0	6	0	0	1	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10		
21:00	0	0	2	1	0	0	0	0	0	0	0	0	0	3	0	0	2	1	0	0	0	0	0	0	3	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6		
21:15	0	1	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	1	1	0	0	0	0	0	2	0	1	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4		
21:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
21:45	0	0	2	1	0</																																										

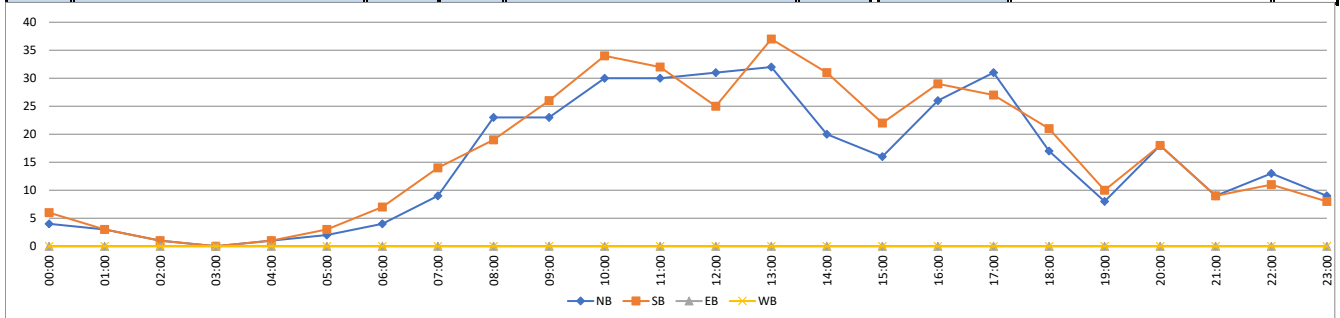
VOLUME

Ridge Rd Bet. Laurel Hill Rd & Plateau PI

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_001

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					360	394	0	0	754									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	1	2			3	12:00	11	10			21	00:00	01:00	4	6			10
00:15	1	0			1	12:15	8	5			13	01:00	02:00	3	3			6
00:30	2	3			5	12:30	5	5			10	02:00	03:00	1	1			2
00:45	0	1			1	12:45	7	5			12	03:00	04:00	0	0			0
01:00	0	1			1	13:00	8	10			18	04:00	05:00	1	1			2
01:15	0	0			0	13:15	7	9			16	05:00	06:00	2	3			5
01:30	1	2			3	13:30	4	10			14	06:00	07:00	4	7			11
01:45	2	0			2	13:45	13	8			21	07:00	08:00	9	14			23
02:00	1	1			2	14:00	11	11			22	08:00	09:00	23	19			42
02:15	0	0			0	14:15	7	7			14	09:00	10:00	23	26			49
02:30	0	0			0	14:30	2	8			10	10:00	11:00	30	34			64
02:45	0	0			0	14:45	0	5			5	11:00	12:00	30	32			62
03:00	0	0			0	15:00	1	5			6	12:00	13:00	31	25			56
03:15	0	0			0	15:15	5	5			10	13:00	14:00	32	37			69
03:30	0	0			0	15:30	4	6			10	14:00	15:00	20	31			51
03:45	0	0			0	15:45	6	6			12	15:00	16:00	16	22			38
04:00	0	0			0	16:00	10	10			20	16:00	17:00	26	29			55
04:15	1	1			2	16:15	7	6			13	17:00	18:00	31	27			58
04:30	0	0			0	16:30	3	7			10	18:00	19:00	17	21			38
04:45	0	0			0	16:45	6	6			12	19:00	20:00	8	10			18
05:00	1	1			2	17:00	9	5			14	20:00	21:00	18	18			36
05:15	0	0			0	17:15	6	10			16	21:00	22:00	9	9			18
05:30	1	1			2	17:30	10	6			16	22:00	23:00	13	11			24
05:45	0	1			1	17:45	6	6			12	23:00	00:00	9	8			17
06:00	0	2			2	18:00	8	6			14	STATISTICS						
06:15	1	1			2	18:15	4	6			10							
06:30	2	1			3	18:30	3	5			8							
06:45	1	3			4	18:45	2	4			6							
07:00	3	4			7	19:00	3	3			6	Peak Period	00:00 to 12:00					
07:15	1	1			2	19:15	3	5			8	Volume	130 146				276	
07:30	1	5			6	19:30	0	0			0	Peak Hour	10:45 10:30				10:30	
07:45	4	4			8	19:45	2	2			4	Peak Volume	34 38				68	
08:00	10	6			16	20:00	4	6			10	Peak Hour Factor	0.708 0.864				0.773	
08:15	2	5			7	20:15	5	5			10	Peak Period	12:00 to 00:00					
08:30	7	4			11	20:30	5	1			6	Volume	230 248				478	
08:45	4	4			8	20:45	4	6			10	Peak Hour	13:15 13:15				13:15	
09:00	6	7			13	21:00	3	3			6	Peak Volume	35 38				73	
09:15	7	5			12	21:15	2	2			4	Peak Hour Factor	0.673 0.864				0.830	
09:30	4	6			10	21:30	0	0			0	Peak Period	07:00 to 09:00					
09:45	6	8			14	21:45	4	4			8	Volume	32 33				65	
10:00	13	8			21	22:00	4	3			7	Peak Hour	7:45 7:30				7:45	
10:15	4	7			11	22:15	4	1			5	Peak Volume	23 20				42	
10:30	5	11			16	22:30	3	5			8	Peak Hour Factor	0.575 0.833				0.656	
10:45	8	8			16	22:45	2	2			4	Peak Period	16:00 to 18:00					
11:00	5	9			14	23:00	6	3			9	Volume	57 56				113	
11:15	12	10			22	23:15	0	2			2	Peak Hour	16:45 16:00				16:45	
11:30	9	7			16	23:30	1	1			2	Peak Volume	31 29				58	
11:45	4	6			10	23:45	2	2			4	Peak Hour Factor	0.775 0.725				0.906	
TOTALS	130	146	0	0	276	TOTALS	230	248	0	0	478							
SPLIT %	47%	53%	0%	0%	37%	SPLIT %	48%	52%	0%	0%	63%							



CLASSIFICATION
Ridge Rd Bet. Westway & Southway

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_002

Time	EASTBOUND													Total	WESTBOUND													Total	TOTALS													
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	
00:00	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	4	0	0	0	0	0	0	0	0	0	0	4	0	7	0	0	0	0	0	0	0	0	0	0	0	0	7
01:00	0	5	1	0	0	0	0	0	0	0	0	0	0	6	0	4	0	0	0	0	0	0	0	0	0	0	4	0	9	1	0	0	0	0	0	0	0	0	0	0	10	
02:00	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	2	
03:00	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	1	0	4	0	0	0	0	0	0	0	0	0	0	0	4	
04:00	0	6	1	0	0	0	0	0	0	0	0	0	0	7	0	1	0	0	0	0	0	0	0	0	0	0	1	0	7	1	0	0	0	0	0	0	0	0	0	8		
05:00	0	22	3	2	0	0	0	0	0	0	0	0	0	27	0	3	1	2	0	0	0	0	0	0	0	0	6	0	25	4	4	0	0	0	0	0	0	0	0	33		
06:00	0	46	2	5	1	0	0	0	0	0	0	0	0	54	0	4	1	4	0	0	0	0	0	0	0	0	9	0	50	3	9	1	0	0	0	0	0	0	0	63		
07:00	0	90	3	3	1	0	0	0	0	0	0	0	0	97	0	17	3	6	0	0	0	0	0	0	0	0	26	0	107	6	9	1	0	0	0	0	0	0	0	123		
08:00	0	85	4	6	1	0	0	0	0	0	0	0	0	96	0	17	2	5	1	0	0	0	0	0	0	0	25	0	102	6	11	2	0	0	0	0	0	0	0	121		
09:00	0	60	5	3	2	0	0	0	0	0	0	0	0	70	0	27	3	4	2	0	0	0	0	0	0	0	36	0	87	8	7	4	0	0	0	0	0	0	0	106		
10:00	0	44	2	4	1	0	0	0	0	0	0	0	0	51	0	17	1	2	1	0	0	0	0	0	0	0	21	0	61	3	6	2	0	0	0	0	0	0	0	72		
11:00	2	48	2	2	0	0	0	0	0	0	0	0	0	54	0	24	3	2	0	0	0	0	0	0	0	0	29	2	72	5	4	0	0	0	0	0	0	0	0	83		
12:00	0	54	3	2	0	0	0	0	0	0	0	0	0	59	1	26	2	2	1	0	0	0	0	0	0	0	32	1	80	5	4	1	0	0	0	0	0	0	0	91		
13:00	0	46	5	3	1	0	0	0	0	0	0	0	0	55	0	29	5	3	0	0	0	0	0	0	0	0	37	0	75	10	6	1	0	0	0	0	0	0	0	92		
14:00	1	55	4	4	0	0	0	0	0	0	0	0	0	64	1	34	6	2	0	0	0	0	0	0	0	0	43	2	89	10	6	0	0	0	0	0	0	0	0	107		
15:00	1	58	2	6	0	0	0	0	0	0	0	0	0	67	0	59	4	4	0	0	0	0	0	0	0	0	67	1	117	6	10	0	0	0	0	0	0	0	0	134		
16:00	0	58	6	6	0	0	0	0	0	0	0	0	0	70	0	47	6	5	0	0	0	0	0	0	0	0	58	0	105	12	11	0	0	0	0	0	0	0	0	128		
17:00	0	42	1	5	2	0	0	0	0	0	0	0	0	50	0	36	2	4	0	0	0	0	0	0	0	0	42	0	78	3	9	2	0	0	0	0	0	0	0	92		
18:00	0	58	3	5	1	0	0	0	0	0	0	0	0	67	0	35	0	5	0	0	0	0	0	0	0	0	40	0	93	3	10	1	0	0	0	0	0	0	0	0	107	
19:00	0	35	3	3	0	0	0	0	0	0	0	0	0	41	0	32	1	2	0	0	0	0	0	0	0	0	35	0	67	4	5	0	0	0	0	0	0	0	0	0	76	
20:00	0	22	1	2	0	0	0	0	0	0	0	0	0	25	0	19	2	3	0	0	0	0	0	0	0	0	24	0	41	3	5	0	0	0	0	0	0	0	0	0	49	
21:00	0	27	2	3	0	0	0	0	0	0	0	0	0	32	0	29	1	3	0	0	0	0	0	0	0	0	33	0	56	3	6	0	0	0	0	0	0	0	0	0	65	
22:00	0	17	1	1	0	0	0	0	0	0	0	0	0	19	0	9	0	0	0	0	0	0	0	0	0	0	9	0	26	1	1	0	0	0	0	0	0	0	0	0	28	
23:00	0	11	1	0	0	0	0	0	0	0	0	0	0	12	0	4	0	1	0	0	0	0	0	0	0	0	5	0	15	1	1	0	0	0	0	0	0	0	0	0	17	
Totals	4	895	56	65	10	0	0	0	0	0	0	0	0	1,030	2	479	43	59	5	0	0	0	0	0	0	0	588	6	1,374	99	124	15	0	0	0	0	0	0	0	0	1,618	
% of Total	0%	87%	5%	6%	1%	0%	0%	0%	0%	0%	0%	0%	0%	100%	0%	81%	7%	10%	1%	0%	0%	0%	0%	0%	0%	100%	0%	85%	6%	8%	1%	0%	0%	0%	0%	0%	0%	0%	0%	100%		

FHWA	CLASSIFICATION DEFINITIONS						
	#1 Motorcycles	#2 Passenger Cars	#3 2-Axle, 4-Tire Single Unit	#4 Buses	#5 2-Axle, 6-Tire Single Units	#6 3-Axle Single Units	#7 >=4-Axle Single Units
						ANY 7 OR MORE AXLE	
	#8 <=4-Axle Single Trailers	#9 5-Axle Single Trailers	#10 >=6-Axle Single Trailers	#11 <=5-Axle Multi-Trailers	#12 6-Axle Multi-Trailers	#13 >=7-Axle Multi-Trailers	

STATISTICS	Hourly Statistics																									
	00:00 - 12:00	01:00	02:00	03:00	04:00	05:00	06:00	07:00	08:00	09:00	10:00	11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00	21:00	22:00	23:00	632	
Peak Hour	11:00	7:00	7:15	7:45	7:45	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	8:15	11:15	8:30	7:30	6:45	8:30	0:00	0:00	0:00	0:00	0:00	0:00	8:30
Peak Volume	2	90	5	5	2	0	0	0	0	0	0	0	0	99	1	34	4	6	3	0	0	0	0	0	43	
07:00 - 09:00	0	175	7	9	2	0	0	0	0	0	0	0	0	193	0	34	5	11	1	0	0	0	0	0	51	
Peak Hour	7:00	17%	1%	1%	1%	0%	0%	0%	0%	0%	0%	0%	0%	19%	0%	6%	1%	2%	0%	0%	0%	0%	0%	0%	9%	
Peak Volume	0	90	5	5	2	0	0	0	0	0	0	0	0	98	0	17	4	6	1	0	0	0	0	0	26	
16:00 - 18:00	0	100	7	11	2	0	0	0	0	0	0	0	0	120	0	83	8	9	0	0	0	0	0	0	100	
Peak Hour	16:00	10%	1%	1%	1%	0%	0%	0%	0%	0%	0%	0%	0%	12%	0%	14%	1%	2%	0%	0%	0%	0%	0%	0%	17%	
Peak Volume	0	58	6	7	2	0	0	0	0	0	0	0	0	70	0	48	7	5	0	0	0	0	0	60		

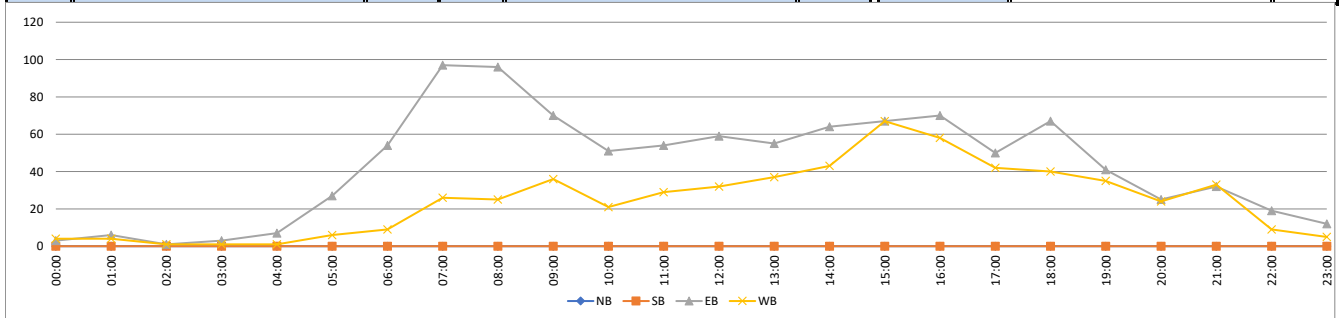
VOLUME

Ridge Rd Bet. Westway & Southway

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_002

DAILY TOTALS						NB					SB					EB					WB					Total																																																																																																																				
						0					0					1,030					588					1,618																																																																																																																				
15-Minutes Interval											Hourly Intervals																																																																																																																																			
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																													
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06:00			7	2	9	18:00			11	8	19	STATISTICS <table border="1"> <thead> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th></tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="4">00:00 to 12:00</td><td></td></tr> <tr> <td>Volume</td><td colspan="4"></td><td>632</td></tr> <tr> <td>Peak Hour</td><td colspan="4">8:15 8:30</td><td>8:30</td></tr> <tr> <td>Peak Volume</td><td colspan="4">99 43</td><td>139</td></tr> <tr> <td>Peak Hour Factor</td><td colspan="4">0.917 0.566</td><td>0.755</td></tr> <tr> <td>Peak Period</td><td colspan="4">12:00 to 00:00</td><td></td></tr> <tr> <td>Volume</td><td colspan="4"></td><td>986</td></tr> <tr> <td>Peak Hour</td><td colspan="4">14:45 15:00</td><td>14:45</td></tr> <tr> <td>Peak Volume</td><td colspan="4">78 67</td><td>143</td></tr> <tr> <td>Peak Hour Factor</td><td colspan="4">0.848 0.931</td><td>0.894</td></tr> <tr> <td>Peak Period</td><td colspan="4">07:00 to 09:00</td><td></td></tr> <tr> <td>Volume</td><td colspan="4"></td><td>244</td></tr> <tr> <td>Peak Hour</td><td colspan="4">7:45 7:00</td><td>7:45</td></tr> <tr> <td>Peak Volume</td><td colspan="4">98 26</td><td>124</td></tr> <tr> <td>Peak Hour Factor</td><td colspan="4">0.907 0.722</td><td>0.861</td></tr> <tr> <td>Peak Period</td><td colspan="4">16:00 to 18:00</td><td></td></tr> <tr> <td>Volume</td><td colspan="4"></td><td>220</td></tr> <tr> <td>Peak Hour</td><td colspan="4">16:00 16:15</td><td>16:00</td></tr> <tr> <td>Peak Volume</td><td colspan="4">70 60</td><td>128</td></tr> <tr> <td>Peak Hour Factor</td><td colspan="4">0.921 0.789</td><td>0.914</td></tr> </tbody> </table>						NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume					632	Peak Hour	8:15 8:30				8:30	Peak Volume	99 43				139	Peak Hour Factor	0.917 0.566				0.755	Peak Period	12:00 to 00:00					Volume					986	Peak Hour	14:45 15:00				14:45	Peak Volume	78 67				143	Peak Hour Factor	0.848 0.931				0.894	Peak Period	07:00 to 09:00					Volume					244	Peak Hour	7:45 7:00				7:45	Peak Volume	98 26				124	Peak Hour Factor	0.907 0.722				0.861	Peak Period	16:00 to 18:00					Volume					220	Peak Hour	16:00 16:15				16:00	Peak Volume	70 60				128	Peak Hour Factor	0.921 0.789				0.914
	NB	SB	EB	WB	TOTAL																																																																																																																																									
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TOTALS	0	0	469	163	632	TOTALS	0	0	561	425	986																																																																																																																																			
SPLIT %	0%	0%	74%	26%	39%	SPLIT %	0%	0%	57%	43%	61%																																																																																																																																			



VOLUME

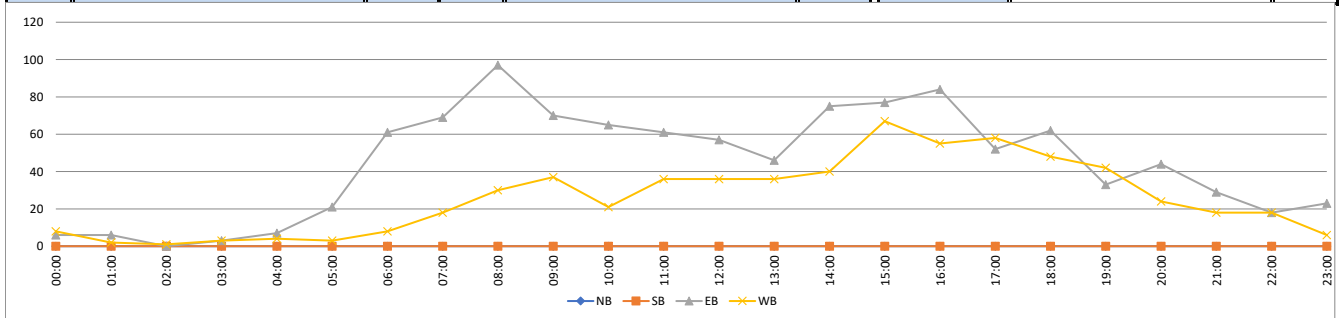
Ridge Rd Bet. Westway & Southway

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_002

DAILY TOTALS						NB					SB					EB					WB					Total				
						0					0					1,066					619					1,685				

15-Minutes Interval						Hourly Intervals																																																																																																																																																			
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																								
00:00			1	3	4	12:00			13	9	22	00:00	01:00			6	8	14																																																																																																																																							
00:15			1	3	4	12:15			9	10	19	01:00	02:00			6	2	8																																																																																																																																							
00:30			2	1	3	12:30			8	9	17	02:00	03:00			0	1	1																																																																																																																																							
00:45			2	1	3	12:45			27	8	35	03:00	04:00			3	3	6																																																																																																																																							
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06:00			12	4	16	18:00			15	14	29	<table border="1"> <thead> <tr> <th colspan="5">STATISTICS</th> </tr> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="4">00:00 to 12:00</td><td></td> </tr> <tr> <td>Volume</td><td></td><td></td><td>466</td><td>171</td><td>637</td> </tr> <tr> <td>Peak Hour</td><td></td><td></td><td>8:00</td><td>8:15</td><td>8:15</td> </tr> <tr> <td>Peak Volume</td><td></td><td></td><td>97</td><td>38</td><td>135</td> </tr> <tr> <td>Peak Hour Factor</td><td></td><td></td><td>0.836</td><td>0.633</td><td>0.938</td> </tr> <tr> <td>Peak Period</td><td colspan="4">12:00 to 00:00</td><td></td> </tr> <tr> <td>Volume</td><td></td><td></td><td>600</td><td>448</td><td>1048</td> </tr> <tr> <td>Peak Hour</td><td></td><td></td><td>14:45</td><td>14:45</td><td>14:45</td> </tr> <tr> <td>Peak Volume</td><td></td><td></td><td>84</td><td>67</td><td>151</td> </tr> <tr> <td>Peak Hour Factor</td><td></td><td></td><td>0.875</td><td>0.838</td><td>0.858</td> </tr> <tr> <td>Peak Period</td><td colspan="4">07:00 to 09:00</td><td></td> </tr> <tr> <td>Volume</td><td></td><td></td><td>166</td><td>48</td><td>214</td> </tr> <tr> <td>Peak Hour</td><td></td><td></td><td>8:00</td><td>8:00</td><td>8:00</td> </tr> <tr> <td>Peak Volume</td><td></td><td></td><td>97</td><td>30</td><td>127</td> </tr> <tr> <td>Peak Hour Factor</td><td></td><td></td><td>0.836</td><td>0.625</td><td>0.882</td> </tr> <tr> <td>Peak Period</td><td colspan="4">16:00 to 18:00</td><td></td> </tr> <tr> <td>Volume</td><td></td><td></td><td>136</td><td>113</td><td>249</td> </tr> <tr> <td>Peak Hour</td><td></td><td></td><td>16:00</td><td>16:30</td><td>16:00</td> </tr> <tr> <td>Peak Volume</td><td></td><td></td><td>84</td><td>65</td><td>139</td> </tr> <tr> <td>Peak Hour Factor</td><td></td><td></td><td>0.875</td><td>0.774</td><td>0.790</td> </tr> </tbody> </table>						STATISTICS						NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume			466	171	637	Peak Hour			8:00	8:15	8:15	Peak Volume			97	38	135	Peak Hour Factor			0.836	0.633	0.938	Peak Period	12:00 to 00:00					Volume			600	448	1048	Peak Hour			14:45	14:45	14:45	Peak Volume			84	67	151	Peak Hour Factor			0.875	0.838	0.858	Peak Period	07:00 to 09:00					Volume			166	48	214	Peak Hour			8:00	8:00	8:00	Peak Volume			97	30	127	Peak Hour Factor			0.836	0.625	0.882	Peak Period	16:00 to 18:00					Volume			136	113	249	Peak Hour			16:00	16:30	16:00	Peak Volume			84	65	139	Peak Hour Factor			0.875	0.774	0.790					
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TOTALS	0	0	466	171	637	TOTALS	0	0	600	448	1048																																																																																																																																														
SPLIT %	0%	0%	73%	27%	38%	SPLIT %	0%	0%	57%	43%	62%																																																																																																																																														



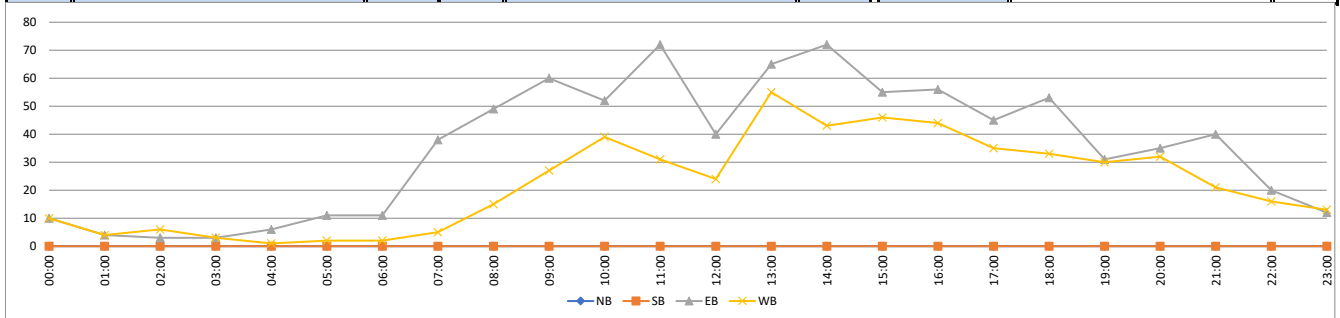
VOLUME

Ridge Rd Bet. Westway & Southway

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_002

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS										
						0	0	843	537	1,380											
15-Minutes Interval											Hourly Intervals										
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL				
00:00			5	3	8	12:00			10	6	16	00:00	01:00			10	10	20			
00:15			2	3	5	12:15			5	4	9	01:00	02:00			4	4	8			
00:30			1	3	4	12:30			13	8	21	02:00	03:00			3	6	9			
00:45			2	1	3	12:45			12	6	18	03:00	04:00			3	3	6			
01:00			4	2	6	13:00			16	12	28	04:00	05:00			6	1	7			
01:15			0	0	0	13:15			17	13	30	05:00	06:00			11	2	13			
01:30			0	1	1	13:30			15	17	32	06:00	07:00			11	2	13			
01:45			0	1	1	13:45			17	13	30	07:00	08:00			38	5	43			
02:00			1	1	2	14:00			19	12	31	08:00	09:00			49	15	64			
02:15			1	1	2	14:15			17	11	28	09:00	10:00			60	27	87			
02:30			0	2	2	14:30			21	10	31	10:00	11:00			52	39	91			
02:45			1	2	3	14:45			15	10	25	11:00	12:00			72	31	103			
03:00			0	0	0	15:00			10	13	23	12:00	13:00			40	24	64			
03:15			1	2	3	15:15			18	8	26	13:00	14:00			65	55	120			
03:30			1	0	1	15:30			13	15	28	14:00	15:00			72	43	115			
03:45			1	1	2	15:45			14	10	24	15:00	16:00			55	46	101			
04:00			0	1	1	16:00			16	14	30	16:00	17:00			56	44	100			
04:15			1	0	1	16:15			15	8	23	17:00	18:00			45	35	80			
04:30			0	0	0	16:30			15	13	28	18:00	19:00			53	33	86			
04:45			5	0	5	16:45			10	9	19	19:00	20:00			31	30	61			
05:00			3	1	4	17:00			14	8	22	20:00	21:00			35	32	67			
05:15			1	1	2	17:15			14	9	23	21:00	22:00			40	21	61			
05:30			2	0	2	17:30			8	8	16	22:00	23:00			20	16	36			
05:45			5	0	5	17:45			9	10	19	23:00	00:00			12	13	25			
06:00			1	0	1	18:00			13	12	25	STATISTICS									
06:15			0	1	1	18:15			17	7	24						NB SB EB WB TOTAL				
06:30			3	0	3	18:30			13	5	18	Peak Period	00:00 to 12:00								
06:45			7	1	8	18:45			10	9	19	Volume			319	145	464				
07:00			15	1	16	19:00			9	8	17	Peak Hour	11:00 to 11:00				11:00				
07:15			6	1	7	19:15			7	6	13	Peak Volume			72	39	103				
07:30			9	3	12	19:30			2	7	9	Peak Hour Factor			0.857	0.886	0.858				
07:45			8	0	8	19:45			13	9	22	Peak Period	12:00 to 00:00								
08:00			9	6	15	20:00			9	9	18	Volume			524	392	916				
08:15			9	0	9	20:15			5	7	12	Peak Hour	13:45 to 13:00				13:15				
08:30			14	6	20	20:30			13	7	20	Peak Volume			74	55	123				
08:45			17	3	20	20:45			8	9	17	Peak Hour Factor			0.881	0.809	0.961				
09:00			13	7	20	21:00			13	4	17	Peak Period	07:00 to 09:00								
09:15			13	4	17	21:15			13	4	17	Volume			87	20	107				
09:30			13	10	23	21:30			7	10	17	Peak Hour	8:00 to 8:00				8:00				
09:45			21	6	27	21:45			7	3	10	Peak Volume			49	15	64				
10:00			17	11	28	22:00			5	6	11	Peak Hour Factor			0.721	0.625	0.800				
10:15			13	9	22	22:15			5	3	8	Peak Period	16:00 to 18:00								
10:30			9	10	19	22:30			5	2	7	Volume			101	79	180				
10:45			13	9	22	22:45			5	5	10	Peak Hour	16:00 to 16:00				16:00				
11:00			21	7	28	23:00			2	4	6	Peak Volume			56	44	100				
11:15			9	7	16	23:15			5	1	6	Peak Hour Factor			0.875	0.786	0.833				
11:30			21	9	30	23:30			5	4	9										
11:45			21	8	29	23:45			0	4	4										
TOTALS	0	0	319	145	464	TOTALS	0	0	524	392	916										
SPLIT %	0%	0%	69%	31%	34%	SPLIT %	0%	0%	57%	43%	66%										



SPEED Ridge Rd Bet. Westway & Southway

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_002

Time	EASTBOUND														Total	WESTBOUND														Total	TOTALS														Total			
	5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70				
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99							
00:00	0	0	1	2	0	0	0	0	0	0	0	0	0	3	0	1	1	2	0	0	0	0	0	0	0	0	4	0	1	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7
01:00	0	0	3	1	2	0	0	0	0	0	0	0	0	6	0	0	1	1	1	0	0	1	0	0	0	0	4	0	0	4	2	3	0	0	1	0	0	0	0	0	0	0	0	0	0	0	10	
02:00	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
03:00	0	0	0	2	0	1	0	0	0	0	0	0	0	3	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
04:00	0	0	3	1	2	1	0	0	0	0	0	0	0	7	1	0	0	0	0	0	0	0	0	0	0	0	1	1	0	3	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	8	
05:00	0	3	9	9	5	1	0	0	0	0	0	0	0	27	0	0	2	3	1	0	0	0	0	0	0	0	6	0	3	11	12	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	33	
06:00	0	9	16	15	8	5	1	0	0	0	0	0	0	54	0	1	4	3	1	0	0	0	0	0	0	9	0	10	20	18	9	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	63	
07:00	0	11	32	30	15	6	3	0	0	0	0	0	0	97	1	6	10	7	2	0	0	0	0	0	0	26	1	17	42	37	17	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	123	
08:00	1	12	38	32	13	0	0	0	0	0	0	0	0	96	2	11	8	3	1	0	0	0	0	0	0	25	3	23	46	35	14	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	121	
09:00	2	11	28	22	6	1	0	0	0	0	0	0	0	70	2	4	12	10	7	1	0	0	0	0	0	36	4	15	40	32	13	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	106	
10:00	1	3	24	15	7	1	0	0	0	0	0	0	0	51	4	1	4	11	1	0	0	0	0	0	0	21	5	4	28	26	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	72	
11:00	0	5	27	17	5	0	0	0	0	0	0	0	0	54	4	0	8	16	1	0	0	0	0	0	0	29	4	5	35	33	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	83	
12:00	1	10	25	15	6	1	1	0	0	0	0	0	0	59	2	3	12	13	2	0	0	0	0	0	0	32	3	13	37	28	8	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	91	
13:00	3	8	25	11	7	1	0	0	0	0	0	0	0	55	4	1	12	14	6	0	0	0	0	0	0	37	7	9	37	25	13	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	92	
14:00	1	7	24	22	8	2	0	0	0	0	0	0	0	64	4	5	8	21	5	0	0	0	0	0	0	43	5	12	32	43	13	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	107
15:00	1	8	24	23	10	1	0	0	0	0	0	0	0	67	2	6	19	32	7	1	0	0	0	0	0	67	3	14	43	55	17	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	134
16:00	2	8	27	21	9	3	0	0	0	0	0	0	0	70	1	7	28	19	3	0	0	0	0	0	0	58	3	15	55	40	12	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	128	
17:00	1	13	19	10	5	2	0	0	0	0	0	0	0	50	5	4	18	14	1	0	0	0	0	0	0	42	6	17	37	24	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	92	
18:00	4	8	28	18	7	2	0	0	0	0	0	0	0	67	1	6	22	9	2	0	0	0	0	0	0	40	5	14	50	27	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	107	
19:00	0	5	20	12	4	0	0	0	0	0	0	0	0	41	3	3	15	13	1	0	0	0	0	0	0	35	3	8	35	25	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	76		
20:00	0	5	9	6	4	1	0	0	0	0	0	0	0	25	2	0	15	5	2	0	0	0	0	0	0	24	2	5	24	11	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	49		
21:00	0	3	11	14	2	0	2	0	0	0	0	0	0	32	5	6	15	6	1	0	0	0	0	0	0	33	5	9	26	20	3	0	2	0	0	0	0	0	0	0	0	0	0	0	0	65		
22:00	0	3	8	5	2	1	0	0	0	0	0	0	0	19	3	1	2	3	0	0	0	0	0	0	0	9	3	4	10	8	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	28		
23:00	0	3	1	3	4	0	1	0	0	0	0	0	0	12	0	0	4	0	0	0	1	0	0	0	0	5	0	3	5	3	4	0	2	0	0	0	0	0	0	0	0	0	0	0	0	17		
Totals	17	135	403	306	131	30	8	0	0	0	0	0	0	1,030	46	66	220	206	46	2	1	1	0	0	588	63	201	623	512	177	32	9	1	0	0	0	0	0	0	0	0	0	0	1,618				
% of Totals	2%	13%	39%	30%	13%	3%	1%							100%	8%	11%	37%	35%	8%	0%	0%	0%		100%	4%	12%	39%	32%	11%	2%	1%	0%													100%			

STATISTICS	00:00 - 12:00	EASTBOUND														Total	WESTBOUND														Total																																																																																																																																																																																																																																	
		4	5	6	7	8	9	10	11	12	13	14	15	16	17		14	15	16	17	18	19	20	21	22	23	24	25	26	27		28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100																																																																																																																																																								
		0%	5%	10%	15%	20%	25%	30%	35%	40%	45%	50%	55%	60%	65%		70%	10:15	10:30	10:45	11:00	11:15	11:30	11:45	12:00	12:15	12:30	12:45	13:00	13:15		13:30	13:45	14:00	14:15	14:30	14:45	15:00	15:15	15:30	15:45	16:00	16:15	16:30	16:45	17:00	17:15	17:30	17:45	18:00	18:15	18:30	18:45	19:00	19:15	19:30	19:45	20:00	20:15	20:30	20:45	21:00	21:15	21:30	21:45	22:00	22:15	22:30	22:45	23:00	23:15	23:30	23:45	24:00	24:15	24:30	24:45	25:00	25:15	25:30	25:45	26:00	26:15	26:30	26:45	27:00	27:15	27:30	27:45	28:00	28:15	28:30	28:45	29:00	29:15	29:30	29:45	30:00	30:15	30:30	30:45	31:00	31:15	31:30	31:45	32:00	32:15	32:30	32:45	33:00	33:15	33:30	33:45	34:00	34:15	34:30	34:45	35:00	35:15	35:30	35:45	36:00	36:15	36:30	36:45	37:00	37:15	37:30	37:45	38:00	38:15	38:30	38:45	39:00	39:15	39:30	39:45	40:00	40:15	40:30	40:45	41:00	41:15	41:30	41:45	42:00	42:15	42:30	42:45	43:00	43:15	43:30	43:45	44:00	44:15	44:30	44:45	45:00	45:15	45:30	45:45	46:00	46:15	46:30	46:45	47:00	47:15	47:30	47:45	48:00	48:15	48:30	48:45	49:00	49:15	49:30	49:45	50:00	50:15	50:30	50:45	51:00	51:15	51:30	51:45	52:00	52:15	52:30	52:45	53:00	53:15	53:30	53:45	54:00	54:15	54:30	54:45	55:00	55:15	55:30	55:45	56:00	56:15	56:30	56:45	57:00	57:15	57:30	57:45	58:00	58:15	58:30	58:45	59:00	59:15	59:30	59:45	60:00	60:15	60:30	60:45	61:00	61:15	61:30	61:45	62:00	62:15	62:30	62:45	63:00	63:15	63:30	63:45	64:00	64:15	64:30	64:45	65:00	65:15	65:30	65:45	66:00	66:15	66:30	66:45	67:00	67:15	67:30	67:45	68:00	68:15	68:30	68:45	69:00	69:15	69:30

SPEED Ridge Rd Bet. Westway & Southway

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_002

Time	EASTBOUND														Total	WESTBOUND														Total	TOTALS														Total							
	5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70								
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99											
12:00	0	2	7	5	1	0	0	0	0	0	0	0	0	15	1	1	2	2	0	0	0	0	0	0	0	0	6	1	3	9	7	1	0	0	0	0	0	0	0	0	0	0	0	0	21							
12:15	0	2	4	3	0	0	0	0	0	0	0	0	0	9	0	0	4	3	1	0	0	0	0	0	0	0	8	0	2	8	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	17						
12:30	1	3	5	3	2	1	1	0	0	0	0	0	0	16	0	0	4	4	0	0	0	0	0	0	0	0	8	1	3	9	7	2	1	1	0	0	0	0	0	0	0	0	0	0	0	24						
12:45	0	3	9	4	3	0	0	0	0	0	0	0	0	19	1	2	2	4	1	0	0	0	0	0	0	0	10	1	5	11	8	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	29					
13:00	0	1	9	1	2	0	0	0	0	0	0	0	0	13	1	0	4	6	1	0	0	0	0	0	0	0	12	1	1	13	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	25					
13:15	2	2	4	3	2	0	0	0	0	0	0	0	0	13	0	1	2	2	1	0	0	0	0	0	0	0	6	2	3	6	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	19						
13:30	1	2	6	4	2	0	0	0	0	0	0	0	0	15	1	0	1	2	0	0	0	0	0	0	0	0	5	2	2	7	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20					
13:45	0	3	6	3	1	1	0	0	0	0	0	0	0	14	2	0	5	5	2	0	0	0	0	0	0	0	14	2	3	11	8	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	28				
14:00	0	2	1	6	1	0	0	0	0	0	0	0	0	10	0	0	2	6	2	0	0	0	0	0	0	0	10	0	2	3	12	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20				
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14:30	0	1	6	6	0	1	0	0	0	0	0	0	0	14	0	0	1	4	1	0	0	0	0	0	0	6	0	1	7	10	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20				
14:45	1	4	10	3	4	1	0	0	0	0	0	0	0	23	3	3	1	7	0	0	0	0	0	0	0	0	14	4	7	11	10	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	37			
15:00	0	2	8	5	2	1	0	0	0	0	0	0	0	18	0	2	2	7	3	1	0	0	0	0	0	0	15	0	4	10	12	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	33			
15:15	1	3	6	10	2	0	0	0	0	0	0	0	0	22	1	0	6	10	1	0	0	0	0	0	0	0	18	2	3	12	20	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	40		
15:30	0	1	5	5	4	0	0	0	0	0	0	0	0	15	1	2	4	10	1	0	0	0	0	0	0	0	18	1	3	9	15	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	33		
15:45	0	2	5	3	2	0	0	0	0	0	0	0	0	12	0	2	7	5	2	0	0	0	0	0	0	0	16	0	4	12	8	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	28		
16:00	0	4	6	4	3	1	0	0	0	0	0	0	0	18	0	1	8	4	0	0	0	0	0	0	0	0	13	0	5	14	8	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	31		
16:15	0	0	6	6	3	2	0	0	0	0	0	0	0	17	1	1	3	5	0	0	0	0	0	0	0	0	10	1	1	9	11	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	27		
16:30	2	2	6	6	3	0	0	0	0	0	0	0	0	19	0	0	8	7	1	0	0	0	0	0	0	0	16	2	2	14	13	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	35		
16:45	0	2	9	5	0	0	0	0	0	0	0	0	0	16	0	5	9	3	2	0	0	0	0	0	0	0	19	0	7	18	8	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	35	
17:00	0	2	7	3	3	0	0	0	0	0	0	0	0	15	1	1	7	6	0	0	0	0	0	0	0	0	15	1	3	14	9	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	30		
17:15	0	4	6	6	0	0	0	0	0	0	0	0	0	16	1	1	3	3	1	0	0	0	0	0	0	0	9	1	5	9	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	25		
17:30	0	3	3	0	1	1	0	0	0	0	0	0	0	8	3	0	7	2	0	0	0	0	0	0	0	0	12	3	3	10	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20		
17:45	1	4	3	1	1	1	0	0	0	0	0	0	0	11	0	2	1	3	0	0	0	0	0	0	0	0	6	1	6	4	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17		
18:00	1	2	4	3	1	1	0	0	0	0	0	0	0	11	1	1	4	2	0	0	0	0	0	0	0	8	2	3	8	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	
18:15	2	2	12	2	2	1	0	0	0	0	0	0	0	21	0	2	5	2	0	0	0	0	0	0	0	9	2	4	17	4	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	30	
18:30	0	3	7	7	3	1	0	0	0	0	0	0	0	21	0	2	5	2	0	0	0	0	0	0	0	9	0	5	12	9	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	30
18:45	1	1	5	6	1	0	0	0	0	0	0	0	0	14	0	1	8	3	2	0	0	0	0	0	0	14	1	2	13	9	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	28	
19:00	0	0	4	4	1	0	0	0	0	0	0	0	0	9	1	1	4	5	0	0	0	0	0	0	0	11	1	1	8	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20	
19:15	0	1	5	2	0	0	0	0	0	0	0	0	0	8	0	2	6	0	0	0	0	0	0	0	0	8	0	3	11	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	
19:30	0	0	6	5	1	0	0	0	0	0	0	0	0	12	1	0	4	4	1	0	0	0	0	0	0	10	1	0	10	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	22
19:45	0	4	5	1	2	0	0	0	0	0	0	0	0	12	1	0	1	4	0	0	0	0	0	0	0	6	1	4	6	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18
20:00	0	2	2	4	2	0	0	0	0	0	0	0	0	10	0	0	8	1	2	0	0	0	0	0	0	11	0	2	10	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21	
20:15	0	2	3	0	0	0	0	0	0	0	0	0	0	5	0	0	1	1	0	0	0	0	0	0	0	2	0	2	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
20:30	0	1	2	1	2	1	0	0	0	0	0	0	0	7	2	0	3	2	0	0	0	0	0	0	0	7	2	1	5	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14
20:45	0	0	2	1	0	0	0	0	0	0	0	0	0	3	0	0	3	1	0	0	0	0	0	0	0	4	0	0	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
21:00	0	0	0	2	0	0	1	0	0	0	0	0	0	3	0	3																																				

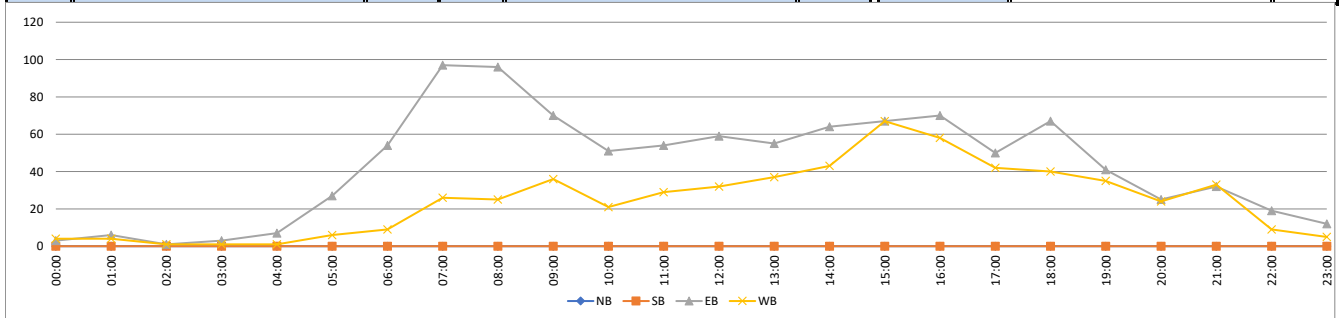
VOLUME

Ridge Rd Bet. Westway & Southway

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_002

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS																																																																																																																																													
						0	0	1,030	588	1,618																																																																																																																																														
15-Minutes Interval											Hourly Intervals																																																																																																																																													
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																							
00:00			0	2	2	12:00			15	6	21	00:00	01:00		3	4	7																																																																																																																																							
00:15			2	1	3	12:15			9	8	17	01:00	02:00		6	4	10																																																																																																																																							
00:30			1	0	1	12:30			16	8	24	02:00	03:00		1	1	2																																																																																																																																							
00:45			0	1	1	12:45			19	10	29	03:00	04:00		3	1	4																																																																																																																																							
01:00			3	0	3	13:00			13	12	25	04:00	05:00		7	1	8																																																																																																																																							
01:15			1	1	2	13:15			13	6	19	05:00	06:00		27	6	33																																																																																																																																							
01:30			1	3	4	13:30			15	5	20	06:00	07:00		54	9	63																																																																																																																																							
01:45			1	0	1	13:45			14	14	28	07:00	08:00		97	26	123																																																																																																																																							
02:00			0	0	0	14:00			10	10	20	08:00	09:00		96	25	121																																																																																																																																							
02:15			1	0	1	14:15			17	13	30	09:00	10:00		70	36	106																																																																																																																																							
02:30			0	1	1	14:30			14	6	20	10:00	11:00		51	21	72																																																																																																																																							
02:45			0	0	0	14:45			23	14	37	11:00	12:00		54	29	83																																																																																																																																							
03:00			1	0	1	15:00			18	15	33	12:00	13:00		59	32	91																																																																																																																																							
03:15			0	0	0	15:15			22	18	40	13:00	14:00		55	37	92																																																																																																																																							
03:30			2	1	3	15:30			15	18	33	14:00	15:00		64	43	107																																																																																																																																							
03:45			0	0	0	15:45			12	16	28	15:00	16:00		67	67	134																																																																																																																																							
04:00			0	1	1	16:00			18	13	31	16:00	17:00		70	58	128																																																																																																																																							
04:15			0	0	0	16:15			17	10	27	17:00	18:00		50	42	92																																																																																																																																							
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06:00			7	2	9	18:00			11	8	19	<table border="1"> <thead> <tr> <th colspan="6">STATISTICS</th> </tr> <tr> <th></th> <th>NB</th> <th>SB</th> <th>EB</th> <th>WB</th> <th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td> <td colspan="2">00:00 to 12:00</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Volume</td> <td colspan="2"></td> <td>469</td> <td>163</td> <td>632</td> </tr> <tr> <td>Peak Hour</td> <td colspan="2">8:15 to 8:30</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Peak Volume</td> <td colspan="2"></td> <td>99</td> <td>43</td> <td>139</td> </tr> <tr> <td>Peak Hour Factor</td> <td colspan="2"></td> <td>0.917</td> <td>0.566</td> <td>0.755</td> </tr> <tr> <td>Peak Period</td> <td colspan="2">12:00 to 00:00</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Volume</td> <td colspan="2"></td> <td>561</td> <td>425</td> <td>986</td> </tr> <tr> <td>Peak Hour</td> <td colspan="2">14:45 to 15:00</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Peak Volume</td> <td colspan="2"></td> <td>78</td> <td>67</td> <td>143</td> </tr> <tr> <td>Peak Hour Factor</td> <td colspan="2"></td> <td>0.848</td> <td>0.931</td> <td>0.894</td> </tr> <tr> <td>Peak Period</td> <td colspan="2">07:00 to 09:00</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Volume</td> <td colspan="2"></td> <td>193</td> <td>51</td> <td>244</td> </tr> <tr> <td>Peak Hour</td> <td colspan="2">7:45 to 7:00</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Peak Volume</td> <td colspan="2"></td> <td>98</td> <td>26</td> <td>124</td> </tr> <tr> <td>Peak Hour Factor</td> <td colspan="2"></td> <td>0.907</td> <td>0.722</td> <td>0.861</td> </tr> <tr> <td>Peak Period</td> <td colspan="2">16:00 to 18:00</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Volume</td> <td colspan="2"></td> <td>120</td> <td>100</td> <td>220</td> </tr> <tr> <td>Peak Hour</td> <td colspan="2">16:00 to 16:15</td> <td></td> <td></td> <td></td> </tr> <tr> <td>Peak Volume</td> <td colspan="2"></td> <td>70</td> <td>60</td> <td>128</td> </tr> <tr> <td>Peak Hour Factor</td> <td colspan="2"></td> <td>0.921</td> <td>0.789</td> <td>0.914</td> </tr> </tbody> </table>					STATISTICS							NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume			469	163	632	Peak Hour	8:15 to 8:30					Peak Volume			99	43	139	Peak Hour Factor			0.917	0.566	0.755	Peak Period	12:00 to 00:00					Volume			561	425	986	Peak Hour	14:45 to 15:00					Peak Volume			78	67	143	Peak Hour Factor			0.848	0.931	0.894	Peak Period	07:00 to 09:00					Volume			193	51	244	Peak Hour	7:45 to 7:00					Peak Volume			98	26	124	Peak Hour Factor			0.907	0.722	0.861	Peak Period	16:00 to 18:00					Volume			120	100	220	Peak Hour	16:00 to 16:15					Peak Volume			70	60	128	Peak Hour Factor			0.921	0.789	0.914				
STATISTICS																																																																																																																																																								
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SPLIT %	0%	0%	74%	26%	39%	SPLIT %	0%	0%	57%	43%	61%																																																																																																																																													



SPEED

Ridge Rd Bet. Westway & Southway

Day: Friday

Date: 11/17/2023

City: Greenbelt

Project #: MD23_280047_002

Time	EASTBOUND														Total	WESTBOUND														Total	TOTALS														Total							
	5 15	15 20	20 25	25 30	30 35	35 40	40 45	45 50	50 55	55 60	60 65	65 70	70 75	75 80		5 15	15 20	20 25	25 30	30 35	35 40	40 45	45 50	50 55	55 60	60 65	65 70	70 75	75 80		5 15	15 20	20 25	25 30	30 35	35 40	40 45	45 50	50 55	55 60	60 65	65 70	70 75	75 80								
00:00	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	0	1	1	0	0	0	0	0	0	0	0	3	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4			
00:15	0	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	1	2	0	0	0	0	0	0	0	0	3	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4		
00:30	0	0	0	1	0	0	0	0	0	0	0	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3		
00:45	0	0	0	2	0	0	0	0	0	0	0	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3		
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04:45	0	0	2	3	0	0	0	0	0	0	0	0	0	5	0	1	0	0	1	0	0	0	0	0	0	0	2	0	0	1	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
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07:00	0	1	10	5	5	2	0	0	0	0	0	0	0	23	0	1	5	0	0	0	0	0	0	0	0	0	6	0	2	15	5	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24
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08:45	2	2	11	8	4	0	0	0	0	0	0	0	0	27	0	0	2	3	1	0	0	0	0	0	0	6	2	2	13	11	5	0	0	0	0																	

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Ridge Rd Bet. Westway & Southway

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Time	EASTBOUND													Total	WESTBOUND													Total	TOTALS													Total		
	5	15	20	25	30	35	40	45	50	55	60	65	70		5	15	20	25	30	35	40	45	50	55	60	65	70		5	15	20	25	30	35	40	45	50	55	60	65	70			
12:00	3	0	5	4	1	0	0	0	0	0	0	0	0	13	1	1	1	5	1	0	0	0	0	0	0	9	4	1	6	9	2	0	0	0	0	0	0	0	0	0	0	0	22	
12:15	0	1	6	0	1	1	0	0	0	0	0	0	0	9	1	1	3	5	0	0	0	0	0	0	0	10	1	2	9	5	1	1	0	0	0	0	0	0	0	0	0	0	19	
12:30	0	0	3	4	0	1	0	0	0	0	0	0	0	8	0	1	6	0	2	0	0	0	0	0	0	9	0	1	9	4	2	1	0	0	0	0	0	0	0	0	0	0	17	
12:45	3	1	7	14	1	0	1	0	0	0	0	0	0	27	1	0	4	3	0	0	0	0	0	0	0	8	4	1	11	17	1	0	1	0	0	0	0	0	0	0	0	0	35	
13:00	0	3	4	3	1	0	0	0	0	0	0	0	0	11	2	2	2	2	2	0	0	0	0	0	0	10	2	5	6	5	3	0	0	0	0	0	0	0	0	0	0	0	21	
13:15	0	3	4	3	1	0	0	0	0	0	0	0	0	11	1	1	2	4	0	0	0	0	0	0	0	8	1	4	6	7	1	0	0	0	0	0	0	0	0	0	0	0	19	
13:30	0	4	3	0	0	1	0	0	0	0	0	0	0	8	0	0	3	3	0	0	0	0	0	0	0	6	0	4	6	3	0	1	0	0	0	0	0	0	0	0	0	0	14	
13:45	1	2	5	4	3	1	0	0	0	0	0	0	0	16	2	1	3	5	1	0	0	0	0	0	0	12	3	3	8	9	4	1	0	0	0	0	0	0	0	0	0	0	28	
14:00	2	1	6	6	5	0	1	0	0	0	0	0	0	21	1	2	1	1	1	0	0	0	0	0	0	6	3	3	7	7	6	0	1	0	0	0	0	0	0	0	0	0	27	
14:15	0	1	5	9	2	1	0	1	0	0	0	0	0	19	1	0	3	9	0	0	0	0	0	0	0	13	1	1	8	18	2	1	0	1	0	0	0	0	0	0	0	0	32	
14:30	0	3	7	3	2	0	0	0	0	0	0	0	0	15	0	1	4	3	0	0	0	0	0	0	0	8	0	4	11	6	2	0	0	0	0	0	0	0	0	0	0	0	0	23
14:45	0	1	11	8	0	0	0	0	0	0	0	0	0	20	1	0	6	6	0	0	0	0	0	0	0	13	1	1	17	14	0	0	0	0	0	0	0	0	0	0	0	0	0	33
15:00	0	3	10	4	1	0	0	0	0	0	0	0	0	18	3	0	6	5	0	0	0	0	0	0	0	14	3	3	16	9	1	0	0	0	0	0	0	0	0	0	0	0	32	
15:15	1	0	11	10	0	0	0	0	0	0	0	0	0	22	1	4	7	5	3	0	0	0	0	0	0	20	2	4	18	15	3	0	0	0	0	0	0	0	0	0	0	0	0	42
15:30	1	3	9	6	4	0	1	0	0	0	0	0	0	24	4	4	6	3	2	0	1	0	0	0	0	20	5	7	15	9	6	0	2	0	0	0	0	0	0	0	0	0	0	44
15:45	0	0	5	5	2	0	0	1	0	0	0	0	0	13	0	0	6	7	0	0	0	0	0	0	0	13	0	0	11	12	2	0	0	1	0	0	0	0	0	0	0	0	0	26
16:00	0	5	12	4	2	1	0	0	0	0	0	0	0	24	0	0	2	4	2	0	0	0	0	0	0	8	0	5	14	8	4	1	0	0	0	0	0	0	0	0	0	0	0	32
16:15	0	4	7	7	2	0	0	0	0	0	0	0	0	20	0	0	4	7	1	0	0	0	0	0	0	12	0	4	11	14	3	0	0	0	0	0	0	0	0	0	0	0	0	32
16:30	0	2	12	8	1	0	0	0	0	0	0	0	0	23	2	4	2	12	1	0	0	0	0	0	0	21	2	6	14	20	2	0	0	0	0	0	0	0	0	0	0	0	0	44
16:45	1	2	4	8	1	1	0	0	0	0	0	0	0	17	0	0	11	3	0	0	0	0	0	0	0	14	1	2	15	11	1	1	0	0	0	0	0	0	0	0	0	0	31	
17:00	0	2	4	6	2	1	0	0	0	0	0	0	0	15	2	2	9	2	1	0	0	0	0	0	0	16	2	4	13	8	3	1	0	0	0	0	0	0	0	0	0	0	31	
17:15	1	2	6	1	1	0	0	0	0	0	0	0	0	11	0	1	8	5	0	0	0	0	0	0	0	14	1	3	14	6	1	0	0	0	0	0	0	0	0	0	0	0	25	
17:30	0	1	2	6	1	0	0	0	0	0	0	0	0	10	3	3	7	2	2	0	0	0	0	0	0	17	3	4	9	8	3	0	0	0	0	0	0	0	0	0	0	0	0	27
17:45	1	4	6	3	2	0	0	0	0	0	0	0	0	16	1	1	8	0	1	0	0	0	0	0	0	11	2	5	14	3	3	0	0	0	0	0	0	0	0	0	0	0	29	
18:00	0	0	5	8	2	0	0	0	0	0	0	0	0	15	1	2	6	5	0	0	0	0	0	0	0	14	1	2	11	13	2	0	0	0	0	0	0	0	0	0	0	0	0	27
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18:30	0	2	8	5	1	0	0	0	0	0	0	0	0	16	0	1	6	3	2	0	0	0	0	0	0	12	0	3	14	8	3	0	0	0	0	0	0	0	0	0	0	0	0	28
18:45	0	3	6	2	1	0	0	0	0	0	0	0	0	12	2	1	6	0	1	0	0	0	0	0	0	10	2	4	12	2	2	0	0	0	0	0	0	0	0	0	0	0	0	22
19:00	0	4	4	2	0	0	0	0	0	0	0	0	0	10	0	1	5	2	2	0	0	0	0	0	0	10	0	5	9	4	2	0	0	0	0	0	0	0	0	0	0	0	0	20
19:15	0	1	3	2	1	0	0	0	0	0	0	0	0	7	1	1	4	4	1	0	0	0	0	0	0	11	1	2	7	6	2	0	0	0	0	0	0	0	0	0	0	0	18	
19:30	1	0	4	2	0	0	0	0	0	0	0	0	0	7	0	1	7	2	0	0	0	0	0	0	0	10	1	1	11	4	0	0	0	0	0	0	0	0	0	0	0	0	17	
19:45	0	1	5	3	0	0	0	0	0	0	0	0	0	9	2	1	3	5	0	0	0	0	0	0	0	11	2	2	8	8	0	0	0	0	0	0	0	0	0	0	0	0	0	20
20:00	1	0	6	3	0	0	0	0	0	0	0	0	0	10	0	2	2	1	0	0	0	0	0	0	0	5	1	2	8	4	0	0	0	0	0	0	0	0	0	0	0	0	15	
20:15	0	0	4	5	1	1	0	0	0	0	0	0	0	11	0	1	2	2	1	0	0	0	0	0	0	6	0	1	6	7	2	1	0	0	0	0	0	0	0	0	0	0	0	17
20:30	0	2	7	2	4	0	0	0	0	0	0	0	0	15	0	1	1	3	0	0	0	0	0	0	0	6	0	3	8	3	7	0	0	0	0	0	0	0	0	0	0	0	21	
20:45	0	0	3	2	2	0	0	1	0	0	0	0	0	8	1	0	4	2	0	0	0	0	0	0	0	7	1	0	7	4	2	0	0	1	0	0	0	0	0	0	0	0	15	
21:00	0	0	3	0	0	0	0	0	0	0	0	0	0	3	0	0	2	3	0	0	0	0	0	0	0	5	0	0	5	3	0	0	0	0	0	0	0	0	0	0	0	0	8	
21:15	0	1	1	6	2	2	0	0	0	0	0	0	0	12	0	1	3	0	0	0	0	0	0	0	0	4	0	2	4	6	2	2	0	0	0	0	0	0	0	0	0	0	0	16
21:30	0	0	3	3	1	1	0	0	0	0	0	0	0	8	0	1	1	1	0	0	0	0	0	0	0	3	0	1	4	4	1	1	0	0	0	0	0	0	0	0	0	0	0	11
21:45	0	2	3	1	0	0	0	0	0	0	0	0	0	6	2	2	0	2	0	0	0	0	0	0	0	6	2	4	3	1	2	0	0	0	0	0	0	0	0	0	0	0	0	12
22:00	0	1	3	1	0	0	1	0	0	0	0	0	0	6	0	0	2	2	0	0	0	0	0	0	0	4	0	1	5	3	0	0	1	0	0	0	0	0	0	0	0	0	0	10
22:15	0	0	0	2	1	0	0	0	0	0	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	1	0	1	0	2	1	0	0	0										

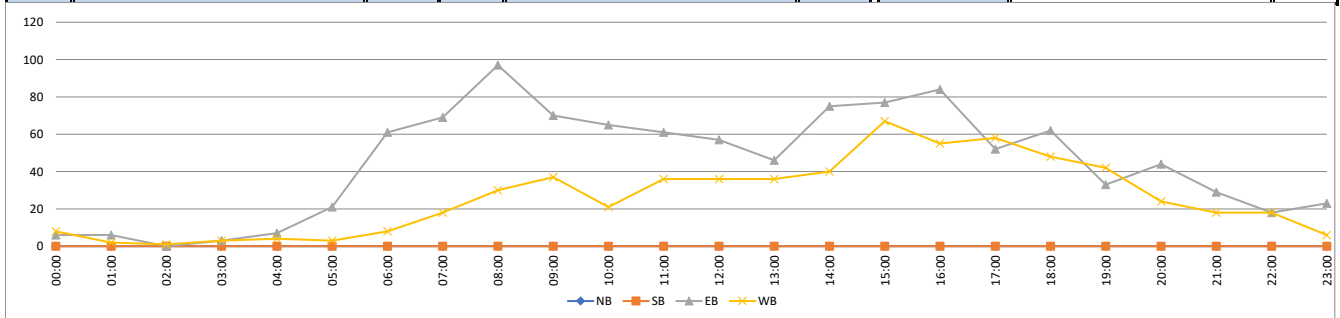
VOLUME

Ridge Rd Bet. Westway & Southway

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_002

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS						
						0	0	1,066	619	1,685							
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00			1	3	4	12:00			13	9	22	00:00	01:00		6	8	14
00:15			1	3	4	12:15			9	10	19	01:00	02:00		6	2	8
00:30			2	1	3	12:30			8	9	17	02:00	03:00		0	1	1
00:45			2	1	3	12:45			27	8	35	03:00	04:00		3	3	6
01:00			2	0	2	13:00			11	10	21	04:00	05:00		7	4	11
01:15			1	2	3	13:15			11	8	19	05:00	06:00		21	3	24
01:30			3	0	3	13:30			8	6	14	06:00	07:00		61	8	69
01:45			0	0	0	13:45			16	12	28	07:00	08:00		69	18	87
02:00			0	0	0	14:00			21	6	27	08:00	09:00		97	30	127
02:15			0	0	0	14:15			19	13	32	09:00	10:00		70	37	107
02:30			0	1	1	14:30			15	8	23	10:00	11:00		65	21	86
02:45			0	0	0	14:45			20	13	33	11:00	12:00		61	36	97
03:00			0	0	0	15:00			18	14	32	12:00	13:00		57	36	93
03:15			0	1	1	15:15			22	20	42	13:00	14:00		46	36	82
03:30			2	1	3	15:30			24	20	44	14:00	15:00		75	40	115
03:45			1	1	2	15:45			13	13	26	15:00	16:00		77	67	144
04:00			0	1	1	16:00			24	8	32	16:00	17:00		84	55	139
04:15			1	1	2	16:15			20	12	32	17:00	18:00		52	58	110
04:30			1	0	1	16:30			23	21	44	18:00	19:00		62	48	110
04:45			5	2	7	16:45			17	14	31	19:00	20:00		33	42	75
05:00			2	0	2	17:00			15	16	31	20:00	21:00		44	24	68
05:15			5	0	5	17:15			11	14	25	21:00	22:00		29	18	47
05:30			8	3	11	17:30			10	17	27	22:00	23:00		18	18	36
05:45			6	0	6	17:45			16	11	27	23:00	00:00		23	6	29
06:00			12	4	16	18:00			15	14	29	STATISTICS					
06:15			11	0	11	18:15			19	12	31						
06:30			14	4	18	18:30			16	12	28	Peak Period	00:00	to	12:00		
06:45			24	0	24	18:45			12	10	22	Volume			466	171	637
07:00			23	6	29	19:00			10	10	20	Peak Hour			8:00	8:15	8:15
07:15			13	1	14	19:15			7	11	18	Peak Volume			97	38	135
07:30			12	9	21	19:30			7	10	17	Peak Hour Factor			0.836	0.633	0.938
07:45			21	2	23	19:45			9	11	20	Peak Period	12:00	to	00:00		
08:00			17	7	24	20:00			10	5	15	Volume			600	448	1048
08:15			29	5	34	20:15			11	6	17	Peak Hour			14:45	14:45	14:45
08:30			24	12	36	20:30			15	6	21	Peak Volume			84	67	151
08:45			27	6	33	20:45			8	7	15	Peak Hour Factor			0.875	0.838	0.858
09:00			17	15	32	21:00			3	5	8	Peak Period	07:00	to	09:00		
09:15			19	5	24	21:15			12	4	16	Volume			166	48	214
09:30			13	9	22	21:30			8	3	11	Peak Hour			8:00	8:00	8:00
09:45			21	8	29	21:45			6	6	12	Peak Volume			97	30	127
10:00			20	4	24	22:00			6	4	10	Peak Hour Factor			0.836	0.625	0.882
10:15			19	4	23	22:15			3	1	4	Peak Period	16:00	to	18:00		
10:30			12	7	19	22:30			7	4	11	Volume			136	113	249
10:45			14	6	20	22:45			2	9	11	Peak Hour			16:00	16:30	16:00
11:00			11	5	16	23:00			6	0	6	Peak Volume			84	65	139
11:15			13	10	23	23:15			4	2	6	Peak Hour Factor			0.875	0.774	0.790
11:30			20	8	28	23:30			7	3	10						
11:45			17	13	30	23:45			6	1	7						
TOTALS	0	0	466	171	637	TOTALS	0	0	600	448	1048						
SPLIT %	0%	0%	73%	27%	38%	SPLIT %	0%	0%	57%	43%	62%						



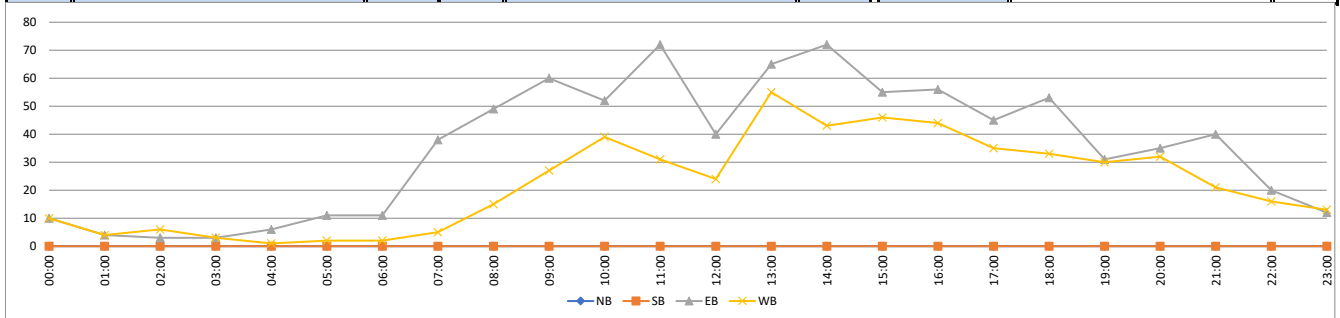
VOLUME

Ridge Rd Bet. Westway & Southway

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_002

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS																																																																																																																																													
						0	0	843	537	1,380																																																																																																																																														
15-Minutes Interval											Hourly Intervals																																																																																																																																													
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																							
00:00			5	3	8	12:00			10	6	16	00:00	01:00			10	10	20																																																																																																																																						
00:15			2	3	5	12:15			5	4	9	01:00	02:00			4	4	8																																																																																																																																						
00:30			1	3	4	12:30			13	8	21	02:00	03:00			3	6	9																																																																																																																																						
00:45			2	1	3	12:45			12	6	18	03:00	04:00			3	3	6																																																																																																																																						
01:00			4	2	6	13:00			16	12	28	04:00	05:00			6	1	7																																																																																																																																						
01:15			0	0	0	13:15			17	13	30	05:00	06:00			11	2	13																																																																																																																																						
01:30			0	1	1	13:30			15	17	32	06:00	07:00			11	2	13																																																																																																																																						
01:45			0	1	1	13:45			17	13	30	07:00	08:00			38	5	43																																																																																																																																						
02:00			1	1	2	14:00			19	12	31	08:00	09:00			49	15	64																																																																																																																																						
02:15			1	1	2	14:15			17	11	28	09:00	10:00			60	27	87																																																																																																																																						
02:30			0	2	2	14:30			21	10	31	10:00	11:00			52	39	91																																																																																																																																						
02:45			1	2	3	14:45			15	10	25	11:00	12:00			72	31	103																																																																																																																																						
03:00			0	0	0	15:00			10	13	23	12:00	13:00			40	24	64																																																																																																																																						
03:15			1	2	3	15:15			18	8	26	13:00	14:00			65	55	120																																																																																																																																						
03:30			1	0	1	15:30			13	15	28	14:00	15:00			72	43	115																																																																																																																																						
03:45			1	1	2	15:45			14	10	24	15:00	16:00			55	46	101																																																																																																																																						
04:00			0	1	1	16:00			16	14	30	16:00	17:00			56	44	100																																																																																																																																						
04:15			1	0	1	16:15			15	8	23	17:00	18:00			45	35	80																																																																																																																																						
04:30			0	0	0	16:30			15	13	28	18:00	19:00			53	33	86																																																																																																																																						
04:45			5	0	5	16:45			10	9	19	19:00	20:00			31	30	61																																																																																																																																						
05:00			3	1	4	17:00			14	8	22	20:00	21:00			35	32	67																																																																																																																																						
05:15			1	1	2	17:15			14	9	23	21:00	22:00			40	21	61																																																																																																																																						
05:30			2	0	2	17:30			8	8	16	22:00	23:00			20	16	36																																																																																																																																						
05:45			5	0	5	17:45			9	10	19	23:00	00:00			12	13	25																																																																																																																																						
06:00			1	0	1	18:00			13	12	25	<table border="1"> <thead> <tr> <th colspan="5">STATISTICS</th> </tr> <tr> <th></th> <th>NB</th> <th>SB</th> <th>EB</th> <th>WB</th> <th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td> <td colspan="4">00:00 to 12:00</td> <td></td> </tr> <tr> <td>Volume</td> <td></td> <td></td> <td>319</td> <td>145</td> <td>464</td> </tr> <tr> <td>Peak Hour</td> <td></td> <td></td> <td>11:00</td> <td>10:00</td> <td>11:00</td> </tr> <tr> <td>Peak Volume</td> <td></td> <td></td> <td>72</td> <td>39</td> <td>103</td> </tr> <tr> <td>Peak Hour Factor</td> <td></td> <td></td> <td>0.857</td> <td>0.886</td> <td>0.858</td> </tr> <tr> <td>Peak Period</td> <td colspan="4">12:00 to 00:00</td> <td></td> </tr> <tr> <td>Volume</td> <td></td> <td></td> <td>524</td> <td>392</td> <td>916</td> </tr> <tr> <td>Peak Hour</td> <td></td> <td></td> <td>13:45</td> <td>13:00</td> <td>13:15</td> </tr> <tr> <td>Peak Volume</td> <td></td> <td></td> <td>74</td> <td>55</td> <td>123</td> </tr> <tr> <td>Peak Hour Factor</td> <td></td> <td></td> <td>0.881</td> <td>0.809</td> <td>0.961</td> </tr> <tr> <td>Peak Period</td> <td colspan="4">07:00 to 09:00</td> <td></td> </tr> <tr> <td>Volume</td> <td></td> <td></td> <td>87</td> <td>20</td> <td>107</td> </tr> <tr> <td>Peak Hour</td> <td></td> <td></td> <td>8:00</td> <td>8:00</td> <td>8:00</td> </tr> <tr> <td>Peak Volume</td> <td></td> <td></td> <td>49</td> <td>15</td> <td>64</td> </tr> <tr> <td>Peak Hour Factor</td> <td></td> <td></td> <td>0.721</td> <td>0.625</td> <td>0.800</td> </tr> <tr> <td>Peak Period</td> <td colspan="4">16:00 to 18:00</td> <td></td> </tr> <tr> <td>Volume</td> <td></td> <td></td> <td>101</td> <td>79</td> <td>180</td> </tr> <tr> <td>Peak Hour</td> <td></td> <td></td> <td>16:00</td> <td>16:00</td> <td>16:00</td> </tr> <tr> <td>Peak Volume</td> <td></td> <td></td> <td>56</td> <td>44</td> <td>100</td> </tr> <tr> <td>Peak Hour Factor</td> <td></td> <td></td> <td>0.875</td> <td>0.786</td> <td>0.833</td> </tr> </tbody> </table>					STATISTICS						NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume			319	145	464	Peak Hour			11:00	10:00	11:00	Peak Volume			72	39	103	Peak Hour Factor			0.857	0.886	0.858	Peak Period	12:00 to 00:00					Volume			524	392	916	Peak Hour			13:45	13:00	13:15	Peak Volume			74	55	123	Peak Hour Factor			0.881	0.809	0.961	Peak Period	07:00 to 09:00					Volume			87	20	107	Peak Hour			8:00	8:00	8:00	Peak Volume			49	15	64	Peak Hour Factor			0.721	0.625	0.800	Peak Period	16:00 to 18:00					Volume			101	79	180	Peak Hour			16:00	16:00	16:00	Peak Volume			56	44	100	Peak Hour Factor			0.875	0.786	0.833					
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07:15			6	1	7	19:15			7	6	13																																																																																																																																													
07:30			9	3	12	19:30			2	7	9																																																																																																																																													
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09:00			13	7	20	21:00			13	4	17																																																																																																																																													
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10:15			13	9	22	22:15			5	3	8																																																																																																																																													
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11:15			9	7	16	23:15			5	1	6																																																																																																																																													
11:30			21	9	30	23:30			5	4	9																																																																																																																																													
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TOTALS	0	0	319	145	464	TOTALS	0	0	524	392	916																																																																																																																																													
SPLIT %	0%	0%	69%	31%	34%	SPLIT %	0%	0%	57%	43%	66%																																																																																																																																													



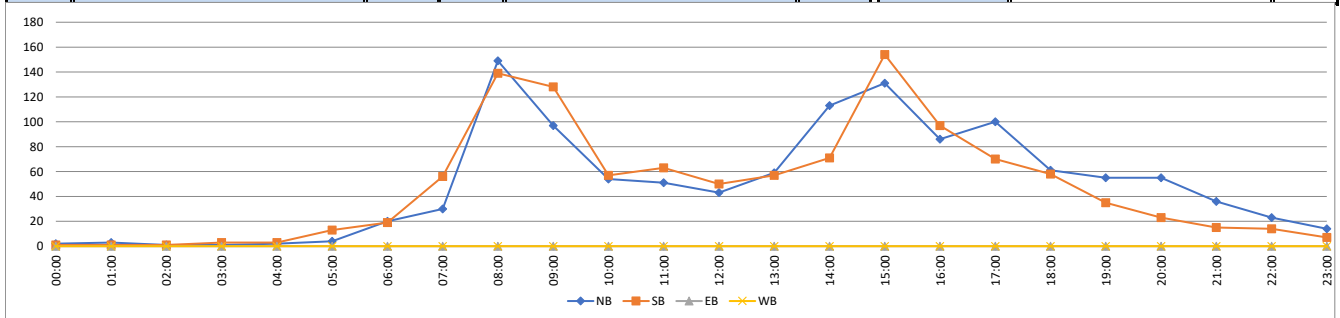
VOLUME

Ridge Rd Bet. Eastway & Hamilton PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_003

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					1,190	1,135	0	0	2,325								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	1	0			1	12:00	7	11			18	00:00	01:00	2	1		3
00:15	1	0			1	12:15	16	16			32	01:00	02:00	3	1		4
00:30	0	1			1	12:30	9	13			22	02:00	03:00	1	1		2
00:45	0	0			0	12:45	11	10			21	03:00	04:00	1	3		4
01:00	1	0			1	13:00	20	15			35	04:00	05:00	2	3		5
01:15	1	1			2	13:15	14	13			27	05:00	06:00	4	13		17
01:30	1	0			1	13:30	12	13			25	06:00	07:00	20	19		39
01:45	0	0			0	13:45	13	16			29	07:00	08:00	30	56		86
02:00	0	0			0	14:00	18	17			35	08:00	09:00	149	139		288
02:15	0	1			1	14:15	27	23			50	09:00	10:00	97	128		225
02:30	0	0			0	14:30	31	16			47	10:00	11:00	54	57		111
02:45	1	0			1	14:45	37	15			52	11:00	12:00	51	63		114
03:00	0	1			1	15:00	38	26			64	12:00	13:00	43	50		93
03:15	1	1			2	15:15	44	50			94	13:00	14:00	59	57		116
03:30	0	1			1	15:30	28	50			78	14:00	15:00	113	71		184
03:45	0	0			0	15:45	21	28			49	15:00	16:00	131	154		285
04:00	2	2			4	16:00	20	23			43	16:00	17:00	86	97		183
04:15	0	0			0	16:15	29	24			53	17:00	18:00	100	70		170
04:30	0	1			1	16:30	19	26			45	18:00	19:00	61	58		119
04:45	0	0			0	16:45	18	24			42	19:00	20:00	55	35		90
05:00	1	5			6	17:00	24	21			45	20:00	21:00	55	23		78
05:15	1	3			4	17:15	28	18			46	21:00	22:00	36	15		51
05:30	0	3			3	17:30	28	11			39	22:00	23:00	23	14		37
05:45	2	2			4	17:45	20	20			40	23:00	00:00	14	7		21
06:00	5	3			8	18:00	27	22			49	STATISTICS					
06:15	4	2			6	18:15	14	10			24		NB	SB	EB	WB	TOTAL
06:30	2	6			8	18:30	8	11			19	Peak Period	00:00	to	12:00		
06:45	9	8			17	18:45	12	15			27	Volume	414	484			898
07:00	5	9			14	19:00	16	14			30	Peak Hour	8:15	8:30			8:15
07:15	11	14			25	19:15	9	7			16	Peak Volume	170	184			347
07:30	6	12			18	19:30	16	8			24	Peak Hour Factor	0.746	0.742			0.834
07:45	8	21			29	19:45	14	6			20	Peak Period	12:00	to	00:00		
08:00	21	24			45	20:00	18	8			26	Volume	776	651			1427
08:15	33	25			58	20:15	13	5			18	Peak Hour	14:30	15:00			14:45
08:30	38	49			87	20:30	9	3			12	Peak Volume	150	154			288
08:45	57	41			98	20:45	15	7			22	Peak Hour Factor	0.852	0.770			0.766
09:00	42	62			104	21:00	8	4			12	Peak Period	07:00	to	09:00		
09:15	21	32			53	21:15	10	2			12	Volume	179	195			374
09:30	19	18			37	21:30	8	4			12	Peak Hour	8:00	8:00			8:00
09:45	15	16			31	21:45	10	5			15	Peak Volume	149	139			288
10:00	12	10			22	22:00	7	5			12	Peak Hour Factor	0.654	0.709			0.735
10:15	17	15			32	22:15	6	4			10	Peak Period	16:00	to	18:00		
10:30	12	17			29	22:30	4	3			7	Volume	186	167			353
10:45	13	15			28	22:45	6	2			8	Peak Hour	17:00	16:00			16:15
11:00	13	12			25	23:00	4	3			7	Peak Volume	100	97			185
11:15	15	18			33	23:15	3	1			4	Peak Hour Factor	0.893	0.933			0.873
11:30	13	23			36	23:30	5	1			6						
11:45	10	10			20	23:45	2	2			4						
TOTALS	414	484	0	0	898	TOTALS	776	651	0	0	1427						
SPLIT %	46%	54%	0%	0%	39%	SPLIT %	54%	46%	0%	0%	61%						



Prepared by National Data & Surveying Services
CLASSIFICATION
 Ridge Rd Bet. Eastway & Hamilton PI

Day: Friday
 Date: 11/17/2023

City: Greenbelt
 Project #: MD23_280047_003

Time	NORTHBOUND													Total	SOUTHBOUND													Total	TOTALS													Total		
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13			
12:00	0	18	0	0	1	0	0	0	0	0	0	0	0	19	0	13	2	0	0	0	0	0	0	0	0	0	15	0	31	2	0	1	0	0	0	0	0	0	0	0	0	0	0	34
12:15	4	21	1	1	0	0	1	0	0	0	0	0	0	28	1	9	4	1	1	0	0	0	0	0	0	0	16	5	30	5	2	1	0	1	0	0	0	0	0	0	0	0	44	
12:30	1	9	0	0	0	0	0	0	0	0	0	0	0	10	2	15	6	0	0	0	0	0	0	0	0	0	23	3	24	6	0	0	0	0	0	0	0	0	0	0	0	0	33	
12:45	1	5	4	0	1	0	0	0	0	0	0	0	0	11	0	9	1	0	0	0	0	0	0	0	0	0	10	1	14	5	0	1	0	0	0	0	0	0	0	0	0	0	21	
13:00	1	14	2	0	0	0	0	0	0	0	0	0	0	17	5	17	3	1	1	0	0	0	0	0	0	0	27	6	31	5	1	1	0	0	0	0	0	0	0	0	0	0	44	
13:15	0	15	4	0	0	0	0	0	0	0	0	0	0	19	1	8	1	1	0	0	0	0	0	0	0	0	11	1	23	5	1	0	0	0	0	0	0	0	0	0	0	0	30	
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15:30	0	20	2	2	1	0	0	0	0	0	0	0	0	25	0	41	1	1	1	0	0	0	0	0	0	0	44	0	61	3	3	2	0	0	0	0	0	0	0	0	0	69		
15:45	0	21	1	1	0	0	0	0	0	0	0	0	0	23	0	20	3	3	2	0	0	0	0	0	0	0	28	0	41	4	4	2	0	0	0	0	0	0	0	0	0	51		
16:00	0	23	1	0	0	0	0	0	0	0	0	0	0	24	0	19	3	2	1	0	0	0	0	0	0	0	25	0	42	4	2	1	0	0	0	0	0	0	0	0	0	49		
16:15	0	17	0	3	0	0	0	0	0	0	0	0	0	20	1	16	2	0	0	0	0	0	0	0	0	0	19	1	33	2	3	0	0	0	0	0	0	0	0	0	0	39		
16:30	1	19	1	1	1	0	0	0	0	0	0	0	0	23	0	14	3	2	1	0	0	0	0	0	0	0	20	1	33	4	3	2	0	0	0	0	0	0	0	0	0	43		
16:45	1	25	1	0	1	0	0	0	0	0	0	0	0	28	0	16	0	0	0	0	0	0	0	0	0	0	16	1	41	1	0	1	0	0	0	0	0	0	0	0	0	0	44	
17:00	0	15	2	1	0	0	0	0	0	0	0	0	0	18	0	24	1	1	0	0	0	0	0	0	0	0	26	0	39	3	2	0	0	0	0	0	0	0	0	0	0	44		
17:15	0	20	1	0	0	0	0	0	0	0	0	0	0	21	1	8	0	0	1	0	0	0	0	0	0	0	10	1	28	1	0	1	0	0	0	0	0	0	0	0	0	31		
17:30	1	27	0	1	0	0	0	0	0	0	0	0	0	29	0	10	0	1	0	0	0	0	0	0	0	0	11	1	37	0	2	0	0	0	0	0	0	0	0	0	0	40		
17:45	0	21	1	1	0	0	0	0	0	0	0	0	0	23	0	8	1	1	0	0	0	0	0	0	0	0	10	0	29	2	2	0	0	0	0	0	0	0	0	0	0	0	33	
18:00	0	15	0	0	0	0	0	0	0	0	0	0	0	15	0	12	0	1	0	0	0	0	0	0	0	0	13	0	27	0	1	0	0	0	0	0	0	0	0	0	0	28		
18:15	0	13	1	2	0	0	0	0	0	0	0	0	0	16	0	16	2	0	0	0	0	0	0	0	0	0	18	0	29	3	2	0	0	0	0	0	0	0	0	0	0	34		
18:30	0	13	3	0	0	0	0	0	0	0	0	0	0	16	0	7	1	1	0	0	0	0	0	0	0	0	9	0	20	4	1	0	0	0	0	0	0	0	0	0	0	35		
18:45	0	16	1	1	0	0	0	0	0	0	0	0	0	18	0	16	0	0	0	0	0	0	0	0	0	0	16	0	32	1	1	0	0	0	0	0	0	0	0	0	0	0	24	
19:00	0	20	1	0	0	0	0	0	0	0	0	0	0	21	0	8	1	1	1	0	0	0	0	0	0	0	11	0	28	2	1	1	0	0	0	0	0	0	0	0	0	0	32	
19:15	0	8	0	0	1	0	0	0	0	0	0	0	0	9	0	9	0	0	0	0	0	0	0	0	0	0	9	0	17	0	0	1	0	0	0	0	0	0	0	0	0	0	18	
19:30	0	5	1	1	0	0	0	0	0	0	0	0	0	7	0	9	0	0	1	0	0	0	0	0	0	0	10	0	14	1	1	1	0	0	0	0	0	0	0	0	0	0	17	
19:45	0	8	0	1	0	0	0	0	0	0	0	0	0	9	0	11	0	0	0	0	0	0	0	0	0	0	11	0	19	0	1	0	0	0	0	0	0	0	0	0	0	0	20	
20:00	0	12	0	0	0	0	0	0	0	0	0	0	0	12	0	7	0	1	0	0	0	0	0	0	0	0	8	0	19	0	1	0	0	0	0	0	0	0	0	0	0	0	32	
20:15	0	12	3	0	0	0	0	0	0	0	0	0	0	15	0	12	5	0	0	0	0	0	0	0	0	0	17	0	24	8	0	0	0	0	0	0	0	0	0	0	0	0	19	
20:30	0	15	0	0	0	0	0	0	0	0	0	0	0	15	0	3	1	0	0	0	0	0	0	0	0	0	4	0	18	1	0	0	0	0	0	0	0	0	0	0	0	0	32	
20:45	0	11	1	1	0	0	0	0	0	0	0	0	0	13	0	4	0	2	0	0	0	0	0	0	0	0	6	0	15	1	3	0	0	0	0	0	0	0	0	0	0	0	19	
21:00	0	9	0	0	0	0	0	0	0	0	0	0	0	9	0	8	2	0	0	0	0	0	0	0	0	0	10	0	17	2	0	0	0	0	0	0	0	0	0	0	0	0	19	
21:15	0	6	0	1	0	0	0	0	0	0	0	0	0	7	0	2	0	0	0	0	0	0	0	0	0	0	2	0	8	0	1	0	0	0	0	0	0	0	0	0	0	0	9	
21:30	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	2	1	0	0	0	0	0	0	0	0	0	3	0	6	1	0	0	0	0	0	0	0	0	0	0	0	0	7	
21:45	0	6	0	1	0	0	0	0	0	0	0	0	0	7	0	2	0	1	0	0	0	0	0	0	0	0	3	0	8	0	2	0	0	0	0	0	0	0	0	0	0	0	10	
22:00	0	11	0	0	0	0	0	0	0	0	0	0	0	11	0	8	0	0	0	0	0	0	0	0	0	0	8	0	19	0	0	0	0	0	0	0	0	0	0	0	0	0	19	
22:15	0	8	0	0	0	0	0	0	0	0	0	0	0	8	0	4	0	0	0	0	0	0	0	0	0	0	4	0	12	0	0	0	0	0	0	0	0	0	0	0	0	0	12	
22:30	0	2	0	0	0	0	0	0	0	0	0	0	0	2	1	3	0	0	0	0	0	0	0	0	0	0	4	1	5	0	0	0												

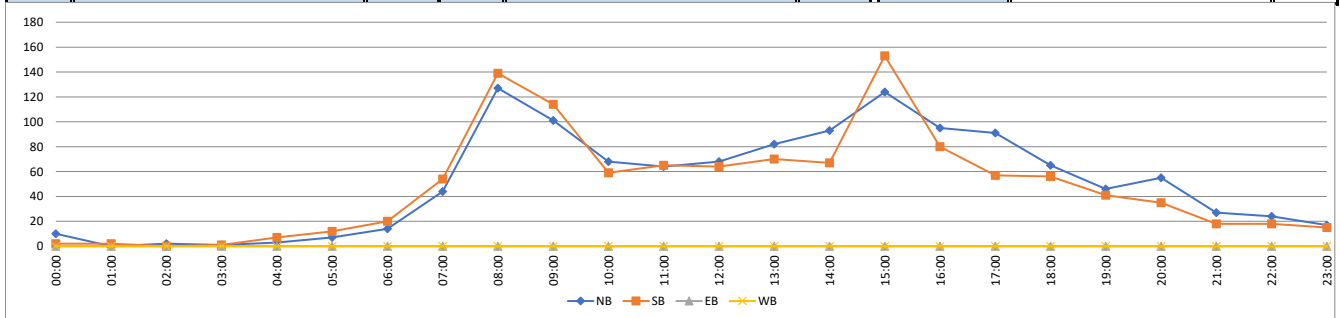
VOLUME

Ridge Rd Bet. Eastway & Hamilton PI

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_003

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					1,228	1,149	0	0	2,377								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	2	0			2	12:00	19	15			34	00:00	10	2			12
00:15	3	0			3	12:15	28	16			44	01:00	0	2			2
00:30	3	2			5	12:30	10	23			33	02:00	2	0			2
00:45	2	0			2	12:45	11	10			21	03:00	1	1			2
01:00	0	1			1	13:00	17	27			44	04:00	3	7			10
01:15	0	1			1	13:15	19	11			30	05:00	7	12			19
01:30	0	0			0	13:30	25	21			46	06:00	14	20			34
01:45	0	0			0	13:45	21	11			32	07:00	44	54			98
02:00	2	0			2	14:00	18	22			40	08:00	127	139			266
02:15	0	0			0	14:15	25	17			42	09:00	101	114			215
02:30	0	0			0	14:30	21	11			32	10:00	68	59			127
02:45	0	0			0	14:45	29	17			46	11:00	64	65			129
03:00	0	1			1	15:00	37	20			57	12:00	68	64			132
03:15	0	0			0	15:15	39	61			100	13:00	82	70			152
03:30	1	0			1	15:30	25	44			69	14:00	93	67			160
03:45	0	0			0	15:45	23	28			51	15:00	124	153			277
04:00	1	1			2	16:00	24	25			49	16:00	95	80			175
04:15	1	2			3	16:15	20	19			39	17:00	91	57			148
04:30	1	4			5	16:30	23	20			43	18:00	65	56			121
04:45	0	0			0	16:45	28	16			44	19:00	46	41			87
05:00	1	2			3	17:00	18	26			44	20:00	55	35			90
05:15	2	3			5	17:15	21	10			31	21:00	27	18			45
05:30	0	5			5	17:30	29	11			40	22:00	24	18			42
05:45	4	2			6	17:45	23	10			33	23:00	17	15			32
06:00	5	4			9	18:00	15	13			28	STATISTICS					
06:15	1	3			4	18:15	16	18			34		NB	SB	EB	WB	TOTAL
06:30	3	2			5	18:30	16	9			25	Peak Period	00:00	to	12:00		
06:45	5	11			16	18:45	18	16			34	Volume	441	475			916
07:00	8	12			20	19:00	21	11			32	Peak Hour	8:30	8:30			8:30
07:15	8	9			17	19:15	9	9			18	Peak Volume	149	172			321
07:30	11	17			28	19:30	7	10			17	Peak Hour Factor	0.642	0.796			0.750
07:45	17	16			33	19:45	9	11			20	Peak Period	12:00	to	00:00		
08:00	24	22			46	20:00	12	8			20	Volume	787	674			1461
08:15	15	29			44	20:15	15	17			32	Peak Hour	14:45	15:15			15:00
08:30	30	39			69	20:30	15	4			19	Peak Volume	130	158			277
08:45	58	49			107	20:45	13	6			19	Peak Hour Factor	0.833	0.648			0.693
09:00	38	54			92	21:00	9	10			19	Peak Period	07:00	to	09:00		
09:15	23	30			53	21:15	7	2			9	Volume	171	193			364
09:30	22	18			40	21:30	4	3			7	Peak Hour	8:00	8:00			8:00
09:45	18	12			30	21:45	7	3			10	Peak Volume	127	139			266
10:00	25	15			40	22:00	11	8			19	Peak Hour Factor	0.547	0.709			0.621
10:15	11	17			28	22:15	8	4			12	Peak Period	16:00	to	18:00		
10:30	13	12			25	22:30	2	4			6	Volume	186	137			323
10:45	19	15			34	22:45	3	2			5	Peak Hour	16:45	16:15			16:00
11:00	9	15			24	23:00	6	5			11	Peak Volume	96	81			175
11:15	23	19			42	23:15	3	4			7	Peak Hour Factor	0.828	0.779			0.893
11:30	12	16			28	23:30	4	3			7						
11:45	20	15			35	23:45	4	3			7						
TOTALS	441	475	0	0	916	TOTALS	787	674	0	0	1461						
SPLIT %	48%	52%	0%	0%	39%	SPLIT %	54%	46%	0%	0%	61%						



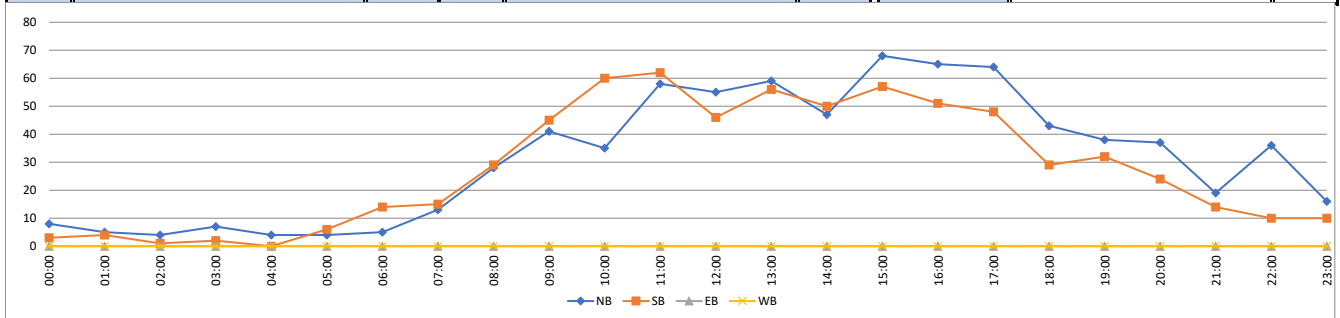
VOLUME

Ridge Rd Bet. Eastway & Hamilton PI

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_003

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					759	668	0	0	1,427								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	3	2			5	12:00	10	15			25	00:00	8	3			11
00:15	2	0			2	12:15	18	10			28	01:00	5	4			9
00:30	2	1			3	12:30	15	9			24	02:00	4	1			5
00:45	1	0			1	12:45	12	12			24	03:00	7	2			9
01:00	1	1			2	13:00	20	14			34	04:00	4	0			4
01:15	0	1			1	13:15	10	12			22	05:00	4	6			10
01:30	1	2			3	13:30	8	16			24	06:00	5	14			19
01:45	3	0			3	13:45	21	14			35	07:00	13	15			28
02:00	1	0			1	14:00	10	11			21	08:00	28	29			57
02:15	1	0			1	14:15	17	12			29	09:00	41	45			86
02:30	0	0			0	14:30	8	15			23	10:00	35	60			95
02:45	2	1			3	14:45	12	12			24	11:00	58	62			120
03:00	3	0			3	15:00	17	13			30	12:00	55	46			101
03:15	2	1			3	15:15	18	12			30	13:00	59	56			115
03:30	2	1			3	15:30	15	20			35	14:00	47	50			97
03:45	0	0			0	15:45	18	12			30	15:00	68	57			125
04:00	1	0			1	16:00	18	9			27	16:00	65	51			116
04:15	1	0			1	16:15	18	15			33	17:00	64	48			112
04:30	0	0			0	16:30	9	14			23	18:00	43	29			72
04:45	2	0			2	16:45	20	13			33	19:00	38	32			70
05:00	0	0			0	17:00	13	10			23	20:00	37	24			61
05:15	1	1			2	17:15	16	16			32	21:00	19	14			33
05:30	3	1			4	17:30	18	9			27	22:00	36	10			46
05:45	0	4			4	17:45	17	13			30	23:00	16	10			26
06:00	0	4			4	18:00	16	4			20	STATISTICS					
06:15	1	2			3	18:15	8	8			16		NB	SB	EB	WB	TOTAL
06:30	3	3			6	18:30	3	8			11	Peak Period	00:00 to 12:00				
06:45	1	5			6	18:45	16	9			25	Volume	212	241			453
07:00	4	2			6	19:00	13	11			24	Peak Hour	11:00	10:15			11:00
07:15	1	3			4	19:15	12	5			17	Peak Volume	58	68			120
07:30	3	5			8	19:30	5	9			14	Peak Hour Factor	0.806	0.850			0.938
07:45	5	5			10	19:45	8	7			15	Peak Period	12:00 to 00:00				
08:00	9	4			13	20:00	13	10			23	Volume	547	427			974
08:15	6	10			16	20:15	9	9			18	Peak Hour	15:15	14:45			15:00
08:30	4	9			13	20:30	7	1			8	Peak Volume	69	57			125
08:45	9	6			15	20:45	8	4			12	Peak Hour Factor	0.958	0.713			0.893
09:00	7	16			23	21:00	6	3			9	Peak Period	07:00 to 09:00				
09:15	10	8			18	21:15	2	3			5	Volume	41	44			85
09:30	13	14			27	21:30	6	3			9	Peak Hour	8:00	8:00			8:00
09:45	11	7			18	21:45	5	5			10	Peak Volume	28	29			57
10:00	10	9			19	22:00	8	1			9	Peak Hour Factor	0.778	0.725			0.891
10:15	10	20			30	22:15	11	1			12	Peak Period	16:00 to 18:00				
10:30	3	16			19	22:30	10	4			14	Volume	129	99			228
10:45	12	15			27	22:45	7	4			11	Peak Hour	16:45	16:30			16:00
11:00	13	17			30	23:00	10	3			13	Peak Volume	67	53			116
11:15	16	16			32	23:15	1	1			2	Peak Hour Factor	0.838	0.828			0.879
11:30	11	19			30	23:30	4	4			8						
11:45	18	10			28	23:45	1	2			3						
TOTALS	212	241	0	0	453	TOTALS	547	427	0	0	974						
SPLIT %	47%	53%	0%	0%	32%	SPLIT %	56%	44%	0%	0%	68%						



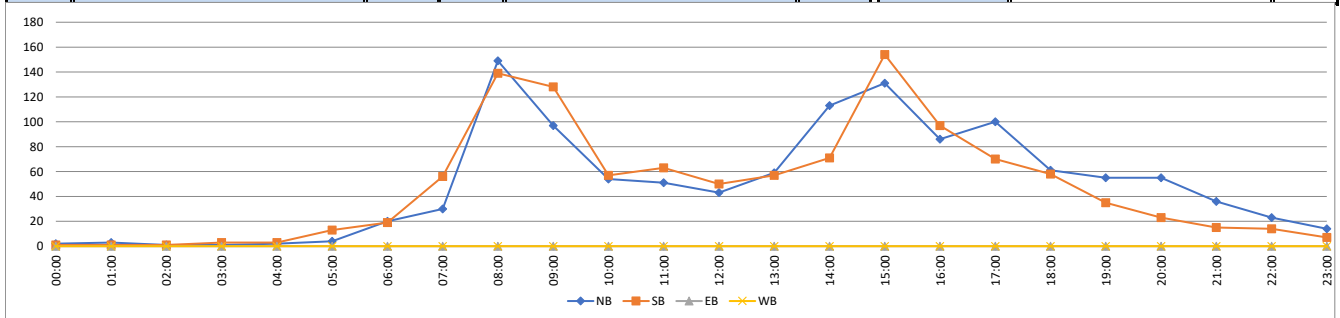
VOLUME

Ridge Rd Bet. Eastway & Hamilton PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_003

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					1,190	1,135	0	0	2,325								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	1	0			1	12:00	7	11			18	00:00	01:00	2	1		3
00:15	1	0			1	12:15	16	16			32	01:00	02:00	3	1		4
00:30	0	1			1	12:30	9	13			22	02:00	03:00	1	1		2
00:45	0	0			0	12:45	11	10			21	03:00	04:00	1	3		4
01:00	1	0			1	13:00	20	15			35	04:00	05:00	2	3		5
01:15	1	1			2	13:15	14	13			27	05:00	06:00	4	13		17
01:30	1	0			1	13:30	12	13			25	06:00	07:00	20	19		39
01:45	0	0			0	13:45	13	16			29	07:00	08:00	30	56		86
02:00	0	0			0	14:00	18	17			35	08:00	09:00	149	139		288
02:15	0	1			1	14:15	27	23			50	09:00	10:00	97	128		225
02:30	0	0			0	14:30	31	16			47	10:00	11:00	54	57		111
02:45	1	0			1	14:45	37	15			52	11:00	12:00	51	63		114
03:00	0	1			1	15:00	38	26			64	12:00	13:00	43	50		93
03:15	1	1			2	15:15	44	50			94	13:00	14:00	59	57		116
03:30	0	1			1	15:30	28	50			78	14:00	15:00	113	71		184
03:45	0	0			0	15:45	21	28			49	15:00	16:00	131	154		285
04:00	2	2			4	16:00	20	23			43	16:00	17:00	86	97		183
04:15	0	0			0	16:15	29	24			53	17:00	18:00	100	70		170
04:30	0	1			1	16:30	19	26			45	18:00	19:00	61	58		119
04:45	0	0			0	16:45	18	24			42	19:00	20:00	55	35		90
05:00	1	5			6	17:00	24	21			45	20:00	21:00	55	23		78
05:15	1	3			4	17:15	28	18			46	21:00	22:00	36	15		51
05:30	0	3			3	17:30	28	11			39	22:00	23:00	23	14		37
05:45	2	2			4	17:45	20	20			40	23:00	00:00	14	7		21
06:00	5	3			8	18:00	27	22			49	STATISTICS					
06:15	4	2			6	18:15	14	10			24						NB
06:30	2	6			8	18:30	8	11			19	Peak Period	00:00	to	12:00		
06:45	9	8			17	18:45	12	15			27	Volume	414	484		898	
07:00	5	9			14	19:00	16	14			30	Peak Hour	8:15	8:30		8:15	
07:15	11	14			25	19:15	9	7			16	Peak Volume	170	184		347	
07:30	6	12			18	19:30	16	8			24	Peak Hour Factor	0.746	0.742		0.834	
07:45	8	21			29	19:45	14	6			20	Peak Period	12:00	to	00:00		
08:00	21	24			45	20:00	18	8			26	Volume	776	651		1427	
08:15	33	25			58	20:15	13	5			18	Peak Hour	14:30	15:00		14:45	
08:30	38	49			87	20:30	9	3			12	Peak Volume	150	154		288	
08:45	57	41			98	20:45	15	7			22	Peak Hour Factor	0.852	0.770		0.766	
09:00	42	62			104	21:00	8	4			12	Peak Period	07:00	to	09:00		
09:15	21	32			53	21:15	10	2			12	Volume	179	195		374	
09:30	19	18			37	21:30	8	4			12	Peak Hour	8:00	8:00		8:00	
09:45	15	16			31	21:45	10	5			15	Peak Volume	149	139		288	
10:00	12	10			22	22:00	7	5			12	Peak Hour Factor	0.654	0.709		0.735	
10:15	17	15			32	22:15	6	4			10	Peak Period	16:00	to	18:00		
10:30	12	17			29	22:30	4	3			7	Volume	186	167		353	
10:45	13	15			28	22:45	6	2			8	Peak Hour	17:00	16:00		16:15	
11:00	13	12			25	23:00	4	3			7	Peak Volume	100	97		185	
11:15	15	18			33	23:15	3	1			4	Peak Hour Factor	0.893	0.933		0.873	
11:30	13	23			36	23:30	5	1			6						
11:45	10	10			20	23:45	2	2			4						
TOTALS	414	484	0	0	898	TOTALS	776	651	0	0	1427						
SPLIT %	46%	54%	0%	0%	39%	SPLIT %	54%	46%	0%	0%	61%						



SPEED Ridge Rd Bet. Eastway & Hamilton PI

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_003

Time	NORTHBOUND														Total	SOUTHBOUND														Total	TOTALS														Total
	5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70	
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99				
00:00	0	1	1	4	4	0	0	0	0	0	0	0	0	10	0	1	0	1	0	0	0	0	0	0	0	0	2	0	2	1	5	4	0	0	0	0	0	0	0	0	0	0	0	0	12
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
02:00	0	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00	0	1	1	1	0	0	0	0	0	0	0	0	0	3	1	0	4	1	1	0	0	0	0	0	0	0	7	1	1	5	2	1	0	0	0	0	0	0	0	0	0	0	0	0	10
05:00	0	0	2	0	3	1	1	0	0	0	0	0	0	7	0	1	0	6	4	0	1	0	0	0	0	0	12	0	1	2	6	7	1	2	0	0	0	0	0	0	0	0	0	19	
06:00	0	1	5	5	1	2	0	0	0	0	0	0	0	14	0	0	2	13	5	0	0	0	0	0	0	20	0	1	7	18	6	2	0	0	0	0	0	0	0	0	0	0	0	34	
07:00	0	4	14	18	4	3	1	0	0	0	0	0	0	44	0	4	12	24	12	2	0	0	0	0	0	54	0	8	26	42	16	5	1	0	0	0	0	0	0	0	0	0	0	98	
08:00	0	4	34	64	23	2	0	0	0	0	0	0	0	127	5	7	32	61	32	2	0	0	0	0	0	139	5	11	66	125	55	4	0	0	0	0	0	0	0	0	0	0	0	0	266
09:00	0	8	28	47	16	2	0	0	0	0	0	0	0	101	1	6	23	55	29	0	0	0	0	0	0	114	1	14	51	102	45	2	0	0	0	0	0	0	0	0	0	0	0	215	
10:00	6	8	26	21	7	0	0	0	0	0	0	0	0	68	1	4	17	28	9	0	0	0	0	0	0	59	7	12	43	49	16	0	0	0	0	0	0	0	0	0	0	0	0	127	
11:00	2	5	13	33	9	2	0	0	0	0	0	0	0	64	2	6	20	26	11	0	0	0	0	0	0	65	4	11	33	59	20	2	0	0	0	0	0	0	0	0	0	0	0	129	
12:00	4	6	18	26	13	1	0	0	0	0	0	0	0	68	2	2	16	31	12	0	1	0	0	0	0	64	6	8	34	57	25	1	0	1	0	0	0	0	0	0	0	0	0	132	
13:00	1	8	21	38	13	1	0	0	0	0	0	0	0	82	3	9	17	31	9	0	1	0	0	0	0	70	4	17	38	69	22	1	1	0	0	0	0	0	0	0	0	0	0	152	
14:00	1	8	36	32	15	1	0	0	0	0	0	0	0	93	1	3	18	34	8	3	0	0	0	0	0	67	2	11	54	66	23	4	0	0	0	0	0	0	0	0	0	0	0	0	160
15:00	2	8	31	48	28	6	1	0	0	0	0	0	0	124	3	11	35	83	19	2	0	0	0	0	0	153	5	19	66	131	47	8	1	0	0	0	0	0	0	0	0	0	0	0	277
16:00	3	12	26	41	11	1	1	0	0	0	0	0	0	95	1	11	19	36	11	1	1	0	0	0	0	80	4	23	45	77	22	2	2	0	0	0	0	0	0	0	0	0	0	0	175
17:00	1	9	42	31	7	1	0	0	0	0	0	0	0	91	2	2	18	25	8	2	0	0	0	0	0	57	3	11	60	56	15	3	0	0	0	0	0	0	0	0	0	0	0	0	148
18:00	1	6	32	19	7	0	0	0	0	0	0	0	0	65	1	3	15	24	12	1	0	0	0	0	0	56	2	9	47	43	19	1	0	0	0	0	0	0	0	0	0	0	0	0	121
19:00	0	4	24	17	1	0	0	0	0	0	0	0	0	46	0	4	16	15	4	2	0	0	0	0	0	41	0	8	40	32	5	2	0	0	0	0	0	0	0	0	0	0	0	0	87
20:00	0	5	19	27	2	1	1	0	0	0	0	0	0	55	1	2	11	17	3	1	0	0	0	0	0	35	1	7	30	44	5	2	1	0	0	0	0	0	0	0	0	0	0	90	
21:00	1	0	13	12	1	0	0	0	0	0	0	0	0	27	0	4	6	6	2	0	0	0	0	0	0	18	1	4	19	18	3	0	0	0	0	0	0	0	0	0	0	0	0	45	
22:00	1	3	12	7	1	0	0	0	0	0	0	0	0	24	1	4	9	3	1	0	0	0	0	0	0	18	2	7	21	10	2	0	0	0	0	0	0	0	0	0	0	0	0	42	
23:00	0	4	4	4	2	0	0	0	0	0	0	0	0	17	0	2	4	6	3	0	0	0	0	0	0	15	0	6	8	13	5	0	0	0	0	0	0	0	0	0	0	0	0	0	32
Totals	23	105	403	499	169	24	5	0	0	0	0	0	0	1,228	25	87	294	528	195	16	3	1	0	0	0	1,149	48	192	697	1,027	364	40	8	1	0	0	0	0	0	0	0	0	0	0	2,377
% of Totals	2%	9%	33%	41%	14%	2%	0%							100%	2%	8%	26%	46%	17%	1%	0%	0%			100%	2%	8%	29%	43%	15%	2%	0%	0%											100%	

STATISTICS	00:00 - 12:00	NORTHBOUND														Total	SOUTHBOUND														Total													
		5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70														
		15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99																
00:00 - 12:00	8	32	125	194	68	12	2	0	0	0	0	0	0	441	10	30	110	217	103	4	1	0	0	0	475	18	62	235	411	171	16	3	0	0	0	0	0	0	0	0	0	0	916	
%	1%	3%	10%	16%	6%	1%	0%	0%	0%	0%	0%	0%	0%	36%	1%	2%	9%	18%	8%	0%	0%	0%	0%	0%	39%	1%	5%	19%	33%	14%	1%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	75%
Peak Hour	10:00	10:30	8:30	8:15	8:30	6:45	4:30	0:00	0:00	0:00	0:00	0:00	0:00	8:30	8:15	7:45	8:15	8:30	8:30	7:15	4:15	11:45	0:00	8:30	10:00	9:15	8:30	8:30	6:45	4:30	11:45	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	8:30	
Peak Volume	6	10	39	72	32	5	1	0	0	0	0	0	0	149	6	8	33	84	42	3	1	1	0	0	122	7	15	72	156	74	6	2	1	0	0	0	0	0	0	0	0	0	321	
12:00 - 24:00	15	73	278	305	101	12	3	0	0	0	0	0	0	787	15	57	184	311	92	12	2	1	0	0	674	30	130	462	616	193	24	5	1	0	0	0	0	0	0	0	0	0	1461	
%	1%	6%	23%	25%	8%	1%	0%	0%	0%	0%	0%	0%	0%	64%	1%	5%	15%	25%	7%	1%	0%	0%	0%	55%	2%	11%	38%	50%	16%	2%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	119%		
Peak Hour	12:15	16:45	17:15	14:45	14:30	15:00	14:30	12:00	12:00	12:00	12:00	12:00	12:00	14:45	12:45	15:00	15:00	15:15	15:15	13:30	12:45	12:00	12:00	15:15	12:15	16:45	15:00	15:15	14:45	15:00	16:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	15:00		
Peak Volume	5	15	45	53	30	6	1	0	0	0	0	0	0	130	3	11	35	92	22	3	1	1	0	0	158	7	24	66	145	47	8	2	1	0	0	0	0	0	0	0	0	277		
07:00 - 09:00	0	8	48	82	27	5	1	0	0	0	0	0	0	171	5	11	44	85	44	4	0	0	0	0	193	5	19	92	167	71	9	1	0	0	0	0	0	0	0	0	0	0	364	
%	0%	1%	4%	7%	2%	0%	0%	0%	0%	0%	0%	0%	0%	14%	0%	1%	4%	7%	4%	0%	0%	0%	0%	16%	0%	2%	7%	14%	6%	6%	1%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	30%	
Peak Hour	7:00	7:15	8:00	8:00	8:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	8:00	7:45	7:45	8:00	8:00																										

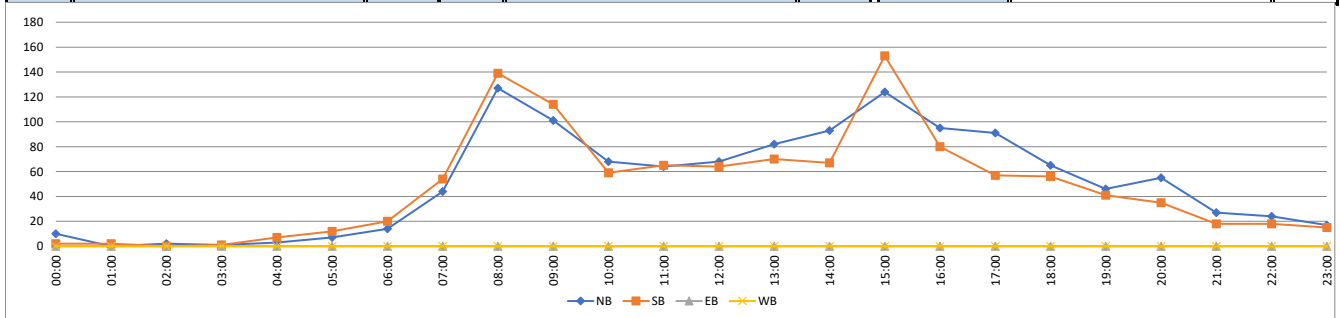
VOLUME

Ridge Rd Bet. Eastway & Hamilton PI

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_003

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					1,228	1,149	0	0	2,377								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	2	0			2	12:00	19	15			34	00:00	10	2			12
00:15	3	0			3	12:15	28	16			44	01:00	0	2			2
00:30	3	2			5	12:30	10	23			33	02:00	2	0			2
00:45	2	0			2	12:45	11	10			21	03:00	1	1			2
01:00	0	1			1	13:00	17	27			44	04:00	3	7			10
01:15	0	1			1	13:15	19	11			30	05:00	7	12			19
01:30	0	0			0	13:30	25	21			46	06:00	14	20			34
01:45	0	0			0	13:45	21	11			32	07:00	44	54			98
02:00	2	0			2	14:00	18	22			40	08:00	127	139			266
02:15	0	0			0	14:15	25	17			42	09:00	101	114			215
02:30	0	0			0	14:30	21	11			32	10:00	68	59			127
02:45	0	0			0	14:45	29	17			46	11:00	64	65			129
03:00	0	1			1	15:00	37	20			57	12:00	68	64			132
03:15	0	0			0	15:15	39	61			100	13:00	82	70			152
03:30	1	0			1	15:30	25	44			69	14:00	93	67			160
03:45	0	0			0	15:45	23	28			51	15:00	124	153			277
04:00	1	1			2	16:00	24	25			49	16:00	95	80			175
04:15	1	2			3	16:15	20	19			39	17:00	91	57			148
04:30	1	4			5	16:30	23	20			43	18:00	65	56			121
04:45	0	0			0	16:45	28	16			44	19:00	46	41			87
05:00	1	2			3	17:00	18	26			44	20:00	55	35			90
05:15	2	3			5	17:15	21	10			31	21:00	27	18			45
05:30	0	5			5	17:30	29	11			40	22:00	24	18			42
05:45	4	2			6	17:45	23	10			33	23:00	17	15			32
06:00	5	4			9	18:00	15	13			28	STATISTICS					
06:15	1	3			4	18:15	16	18			34		NB	SB	EB	WB	TOTAL
06:30	3	2			5	18:30	16	9			25	Peak Period	00:00	to	12:00		
06:45	5	11			16	18:45	18	16			34	Volume	441	475			916
07:00	8	12			20	19:00	21	11			32	Peak Hour	8:30	8:30			8:30
07:15	8	9			17	19:15	9	9			18	Peak Volume	149	172			321
07:30	11	17			28	19:30	7	10			17	Peak Hour Factor	0.642	0.796			0.750
07:45	17	16			33	19:45	9	11			20	Peak Period	12:00	to	00:00		
08:00	24	22			46	20:00	12	8			20	Volume	787	674			1461
08:15	15	29			44	20:15	15	17			32	Peak Hour	14:45	15:15			15:00
08:30	30	39			69	20:30	15	4			19	Peak Volume	130	158			277
08:45	58	49			107	20:45	13	6			19	Peak Hour Factor	0.833	0.648			0.693
09:00	38	54			92	21:00	9	10			19	Peak Period	07:00	to	09:00		
09:15	23	30			53	21:15	7	2			9	Volume	171	193			364
09:30	22	18			40	21:30	4	3			7	Peak Hour	8:00	8:00			8:00
09:45	18	12			30	21:45	7	3			10	Peak Volume	127	139			266
10:00	25	15			40	22:00	11	8			19	Peak Hour Factor	0.547	0.709			0.621
10:15	11	17			28	22:15	8	4			12	Peak Period	16:00	to	18:00		
10:30	13	12			25	22:30	2	4			6	Volume	186	137			323
10:45	19	15			34	22:45	3	2			5	Peak Hour	16:45	16:15			16:00
11:00	9	15			24	23:00	6	5			11	Peak Volume	96	81			175
11:15	23	19			42	23:15	3	4			7	Peak Hour Factor	0.828	0.779			0.893
11:30	12	16			28	23:30	4	3			7						
11:45	20	15			35	23:45	4	3			7						
TOTALS	441	475	0	0	916	TOTALS	787	674	0	0	1461						
SPLIT %	48%	52%	0%	0%	39%	SPLIT %	54%	46%	0%	0%	61%						



SPEED

Ridge Rd Bet. Eastway & Hamilton PI

Day: Saturday

City: Greenbelt

Date: 11/18/2023

Project #: MD23_280047_003

Time	NORTHBOUND										Total	SOUTHBOUND										Total	TOTALS										Total									
	5 15	15 20	20 25	25 30	30 35	35 40	40 45	45 50	50 55	55 60		60 65	65 70	70 99	5 15	15 20	20 25	25 30	30 35	35 40	40 45		45 50	50 55	55 60	60 65	65 70	70 99	5 15	15 20	20 25	25 30		30 35	35 40	40 45	45 50	50 55	55 60	60 65	65 70	70 99
00:00	0	1	2	0	0	0	0	0	0	0	0	0	3	0	0	2	0	0	0	0	0	0	0	0	0	2	0	1	4	0	0	0	0	0	0	0	0	0	0	0	0	5
00:15	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	
00:30	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	
00:45	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
01:00	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	
01:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	
01:30	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	
01:45	0	0	0	3	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
02:00	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	
02:15	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	
02:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
02:45	1	0	1	0	0	0	0	0	0	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	1	1	0	2	0	0	0	0	0	0	0	0	0	0	0	3	
03:00	1	2	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	
03:15	0	1	0	1	0	0	0	0	0	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	3	
03:30	0	0	2	0	0	0	0	0	0	0	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	
03:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
04:00	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
04:15	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
04:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
04:45	0	0	0	2	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
05:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
05:15	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	
05:30	0	2	0	1	0	0	0	0	0	0	0	0	3	0	0	0	1	0	0	0	0	0	0	0	1	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	4	
05:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	3	0	0	0	0	0	0	0	0	4	0	0	1	3	0	0	0	0	0	0	0	0	0	0	4	
06:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	4	0	0	2	2	0	0	0	0	0	0	0	0	0	0	4	
06:15	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	1	0	0	0	0	0	0	0	2	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	
06:30	0	0	1	2	0	0	0	0	0	0	0	0	3	0	0	0	2	1	0	0	0	0	0	0	3	0	0	1	4	1	0	0	0	0	0	0	0	0	0	0	6	
06:45	0	0	0	0	1	0	0	0	0	0	0	0	1	0	0	3	2	0	0	0	0	0	0	0	5	0	0	3	2	1	0	0	0	0	0	0	0	0	0	0	6	
07:00	0	0	1	3	0	0	0	0	0	0	0	0	4	0	0	1	1	0	0	0	0	0	0	0	2	0	0	2	4	0	0	0	0	0	0	0	0	0	0	0	6	
07:15	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	1	2	0	0	0	0	0	0	0	3	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4	
07:30	0	0	2	1	0	0	0	0	0	0	0	0	3	0	0	3	1	1	0	0	0	0	0	0	5	0	0	5	2	1	0	0	0	0	0	0	0	0	0	0	8	
07:45	0	0	2	1	2	0	0	0	0	0	0	0	5	2	0	0	2	1	0	0	0	0	0	0	5	2	0	2	3	3	0	0	0	0	0	0	0	0	0	0	10	
08:00	0	0	5	4	0	0	0	0	0	0	0	0	9	0	0	0	4	0	0	0	0	0	0	0	4	0	0	5	8	0	0	0	0	0	0	0	0	0	0	0	13	
08:15	0	0	2	3	0	1	0	0	0	0	0	0	6	0	0	1	7	2	0	0	0	0	0	0	10	0	0	3	10	2	1	0	0	0	0	0	0	0	0	0	16	
08:30	0	1	0	2	1	0	0	0	0	0	0	0	4	0	0	5	4	0	0	0	0	0	0	0	9	0	1	5	6	1	0	0	0	0	0	0	0	0	0	0	13	
08:45	0	1	3	4	1	0	0	0	0	0	0	0	9	0	1	4	0	1	0	0	0	0	0	0	6	0	2	7	4	2	0	0	0	0	0	0	0	0	0	0	15	
09:00	0	0	3	2	2	0	0	0	0	0	0	0	7	0	1	9	4	1	0	0	0	0	0	0	16	0	1	4	11	6	1	0	0	0	0	0	0	0	0	0	23	
09:15	0	0	6	3	1	0	0	0	0	0	0	0	10	0	0	3	3	2	0	0	0	0	0	8	0	0	9	6	3	0	0	0	0	0	0	0	0	0	0	0	18	
09:30	0	1	2	7	3	0	0	0	0	0	0	0	13	0	0	5	8	1	0	0	0	0	0	14	0	1	7	15	4	0	0	0	0	0	0	0	0	0	0	0	27	
09:45	0	0	5	6	0	0	0	0	0	0	0	0	11	0	0	4	2	1	0	0	0	0	0	7	0	0	9	8	1	0	0	0	0	0	0	0	0	0	0	0	18	
10:00	0	2	3	2	3	0	0	0	0	0	0	0	10	0	0	3	6	0	0	0	0	0	0	9	0	2	6	8	3	0	0	0	0	0	0	0	0	0	0	0	19	
10:15	0	0	2	4	3	1	0	0	0	0	0	0	10	0	1	7	8	3	1	0	0	0	0	20	0	1	9	12	6	2	0	0	0	0	0	0	0	0	0	0	30	
10:30	0	1	0	2	0	0	0	0	0	0	0	0	3	1	0	6	7	1	0	1	0	0	0	16	1	1	6	9	1	0	1	0	0	0	0	0	0	0	0	0	19	
10:45	0	1	2	6	3	0	0	0	0	0	0	0	12	0	0	2	7	5	1	0	0	0	0	15	0	1	4	13	8	1	0	0	0	0								

SPEED

Ridge Rd Bet. Eastway & Hamilton PI

Day: Saturday

City: Greenbelt

Date: 11/18/2023

Project #: MD23_280047_003

Time	NORTHBOUND														Total	SOUTHBOUND														Total	TOTALS														Total					
	5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	35	40	45	50	55	60	65	70		70	5	15	20	25	30	35	40	45	50	55	60	65	70		70				
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99									
12:00	0	0	5	3	1	1	0	0	0	0	0	0	0	10	0	2	4	6	3	0	0	0	0	0	0	0	0	0	15	0	2	9	9	4	1	0	0	0	0	0	0	0	0	0	0	0	25			
12:15	1	1	4	7	3	1	1	0	0	0	0	0	0	18	1	0	3	2	3	1	0	0	0	0	0	0	0	10	2	1	7	9	6	2	1	0	0	0	0	0	0	0	0	0	0	0	28			
12:30	0	1	2	7	3	2	0	0	0	0	0	0	0	15	0	1	3	3	1	1	0	0	0	0	0	0	0	9	0	2	5	10	4	3	0	0	0	0	0	0	0	0	0	0	0	0	24			
12:45	0	1	4	5	2	0	0	0	0	0	0	0	0	12	0	2	3	6	1	0	0	0	0	0	0	0	0	12	0	3	7	11	3	0	0	0	0	0	0	0	0	0	0	0	0	0	34			
13:00	1	0	8	7	4	0	0	0	0	0	0	0	0	20	0	3	3	4	4	0	0	0	0	0	0	0	0	14	1	3	11	11	8	0	0	0	0	0	0	0	0	0	0	0	0	0	34			
13:15	0	0	2	7	1	0	0	0	0	0	0	0	0	10	0	0	3	5	4	0	0	0	0	0	0	0	12	0	0	5	12	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	22			
13:30	0	0	0	7	1	0	0	0	0	0	0	0	0	8	0	1	5	7	3	0	0	0	0	0	0	0	16	0	1	5	14	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24			
13:45	0	1	7	7	6	0	0	0	0	0	0	0	0	21	0	1	6	5	2	0	0	0	0	0	0	0	14	0	2	13	12	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	35			
14:00	0	1	7	1	1	0	0	0	0	0	0	0	0	10	1	1	5	4	0	0	0	0	0	0	0	0	11	1	2	12	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21			
14:15	0	0	9	6	2	0	0	0	0	0	0	0	0	17	2	0	6	2	2	0	0	0	0	0	0	12	2	0	15	8	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	29		
14:30	0	0	2	5	1	0	0	0	0	0	0	0	0	8	0	5	3	6	1	0	0	0	0	0	0	15	0	5	5	11	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23		
14:45	0	1	8	2	1	0	0	0	0	0	0	0	0	12	0	3	5	3	1	0	0	0	0	0	0	12	0	4	13	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24		
15:00	0	0	8	7	2	0	0	0	0	0	0	0	0	17	0	1	4	6	2	0	0	0	0	0	0	13	0	1	12	13	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	30		
15:15	0	0	10	8	0	0	0	0	0	0	0	0	0	18	1	2	3	6	0	0	0	0	0	0	0	12	1	2	13	14	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	30		
15:30	0	1	7	6	1	0	0	0	0	0	0	0	0	15	0	0	7	13	0	0	0	0	0	0	20	0	1	14	19	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	35	
15:45	0	6	8	3	0	1	0	0	0	0	0	0	0	18	1	0	4	7	0	0	0	0	0	0	12	1	6	12	10	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	30	
16:00	0	1	7	8	2	0	0	0	0	0	0	0	0	18	0	1	3	5	0	0	0	0	0	0	9	0	2	10	13	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	27	
16:15	0	1	7	8	2	0	0	0	0	0	0	0	0	18	0	1	3	10	1	0	0	0	0	0	15	0	2	10	18	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	33
16:30	0	1	5	1	2	0	0	0	0	0	0	0	0	9	0	3	8	3	0	0	0	0	0	0	14	0	4	13	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23	
16:45	0	2	7	8	3	0	0	0	0	0	0	0	0	20	0	1	3	7	2	0	0	0	0	0	13	0	3	10	15	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	33
17:00	0	1	6	5	1	0	0	0	0	0	0	0	0	13	0	0	1	5	4	0	0	0	0	0	10	0	1	7	10	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23	
17:15	0	2	7	6	1	0	0	0	0	0	0	0	0	16	0	0	8	8	0	0	0	0	0	0	16	0	2	15	14	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	32	
17:30	0	1	7	8	1	1	0	0	0	0	0	0	0	18	0	0	1	6	2	0	0	0	0	0	9	0	1	8	14	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	27	
17:45	0	1	6	9	1	0	0	0	0	0	0	0	0	17	0	1	5	7	0	0	0	0	0	0	13	0	2	11	16	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	30	
18:00	0	2	4	6	4	0	0	0	0	0	0	0	0	16	0	0	0	2	2	0	0	0	0	0	4	0	2	4	8	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20	
18:15	0	1	1	5	1	0	0	0	0	0	0	0	0	8	0	0	2	5	1	0	0	0	0	0	8	0	1	3	10	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	
18:30	0	0	1	2	0	0	0	0	0	0	0	0	0	3	0	0	4	3	1	0	0	0	0	0	8	0	0	5	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	
18:45	0	0	7	8	1	0	0	0	0	0	0	0	0	16	0	0	2	5	2	0	0	0	0	0	9	0	0	9	13	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	25	
19:00	0	1	4	7	1	0	0	0	0	0	0	0	0	13	1	1	4	4	1	0	0	0	0	0	11	1	2	8	11	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24	
19:15	0	0	6	4	2	0	0	0	0	0	0	0	0	12	0	0	1	2	2	0	0	0	0	0	5	0	0	7	6	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17	
19:30	0	1	2	3	1	0	0	0	0	0	0	0	0	5	0	1	3	4	1	0	0	0	0	0	9	0	2	4	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
19:45	0	2	3	2	1	0	0	0	0	0	0	0	0	8	0	0	5	2	0	0	0	0	0	0	7	0	2	8	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	15	
20:00	0	1	7	5	0	0	0	0	0	0	0	0	0	13	0	2	3	3	2	0	0	0	0	0	10	0	3	10	8	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23
20:15	0	0	4	4	1	0	0	0	0	0	0	0	0	9	0	0	2	7	0	0	0	0	0	0	9	0	0	6	11	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18
20:30	0	1	2	3	1	0	0	0	0	0	0	0	0	7	0	0	1	0	0	0	0	0	0	1	0	1	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	
20:45	0	1	3	3	1	0	0	0	0	0	0	0	0	8	0	1	0	2	1	0	0	0	0	0	4	0	2	3	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
21:00	0	0	2	3	1	0	0	0	0	0	0	0	0	6	0	0	2	0	0	1	0	0	0	0	3	0	0	4	3	1	1	0	0																	

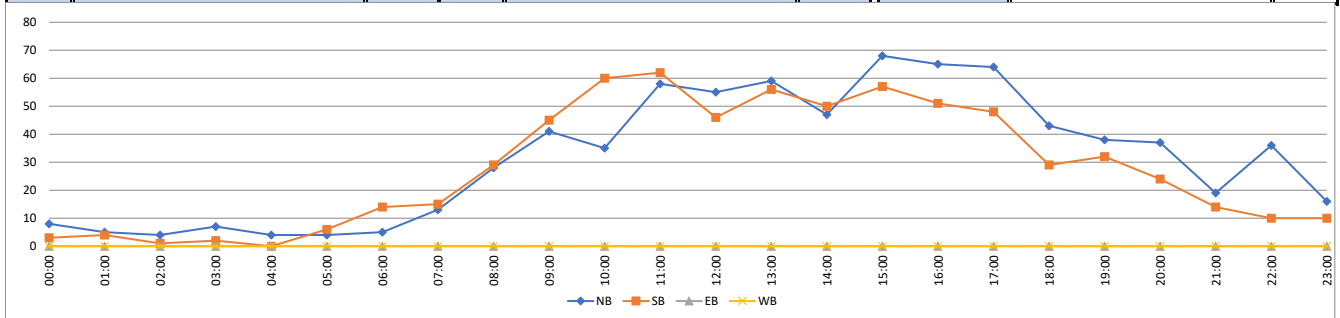
VOLUME

Ridge Rd Bet. Eastway & Hamilton PI

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_003

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					759	668	0	0	1,427								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	3	2			5	12:00	10	15			25	00:00	8	3			11
00:15	2	0			2	12:15	18	10			28	01:00	5	4			9
00:30	2	1			3	12:30	15	9			24	02:00	4	1			5
00:45	1	0			1	12:45	12	12			24	03:00	7	2			9
01:00	1	1			2	13:00	20	14			34	04:00	4	0			4
01:15	0	1			1	13:15	10	12			22	05:00	4	6			10
01:30	1	2			3	13:30	8	16			24	06:00	5	14			19
01:45	3	0			3	13:45	21	14			35	07:00	13	15			28
02:00	1	0			1	14:00	10	11			21	08:00	28	29			57
02:15	1	0			1	14:15	17	12			29	09:00	41	45			86
02:30	0	0			0	14:30	8	15			23	10:00	35	60			95
02:45	2	1			3	14:45	12	12			24	11:00	58	62			120
03:00	3	0			3	15:00	17	13			30	12:00	55	46			101
03:15	2	1			3	15:15	18	12			30	13:00	59	56			115
03:30	2	1			3	15:30	15	20			35	14:00	47	50			97
03:45	0	0			0	15:45	18	12			30	15:00	68	57			125
04:00	1	0			1	16:00	18	9			27	16:00	65	51			116
04:15	1	0			1	16:15	18	15			33	17:00	64	48			112
04:30	0	0			0	16:30	9	14			23	18:00	43	29			72
04:45	2	0			2	16:45	20	13			33	19:00	38	32			70
05:00	0	0			0	17:00	13	10			23	20:00	37	24			61
05:15	1	1			2	17:15	16	16			32	21:00	19	14			33
05:30	3	1			4	17:30	18	9			27	22:00	36	10			46
05:45	0	4			4	17:45	17	13			30	23:00	16	10			26
06:00	0	4			4	18:00	16	4			20	STATISTICS					
06:15	1	2			3	18:15	8	8			16						NB
06:30	3	3			6	18:30	3	8			11	Peak Period	00:00 to 12:00				
06:45	1	5			6	18:45	16	9			25	Volume	212	241		453	
07:00	4	2			6	19:00	13	11			24	Peak Hour	11:00	10:15		11:00	
07:15	1	3			4	19:15	12	5			17	Peak Volume	58	68		120	
07:30	3	5			8	19:30	5	9			14	Peak Hour Factor	0.806	0.850		0.938	
07:45	5	5			10	19:45	8	7			15	Peak Period	12:00 to 00:00				
08:00	9	4			13	20:00	13	10			23	Volume	547	427		974	
08:15	6	10			16	20:15	9	9			18	Peak Hour	15:15	14:45		15:00	
08:30	4	9			13	20:30	7	1			8	Peak Volume	69	57		125	
08:45	9	6			15	20:45	8	4			12	Peak Hour Factor	0.958	0.713		0.893	
09:00	7	16			23	21:00	6	3			9	Peak Period	07:00 to 09:00				
09:15	10	8			18	21:15	2	3			5	Volume	41	44		85	
09:30	13	14			27	21:30	6	3			9	Peak Hour	8:00	8:00		8:00	
09:45	11	7			18	21:45	5	5			10	Peak Volume	28	29		57	
10:00	10	9			19	22:00	8	1			9	Peak Hour Factor	0.778	0.725		0.891	
10:15	10	20			30	22:15	11	1			12	Peak Period	16:00 to 18:00				
10:30	3	16			19	22:30	10	4			14	Volume	129	99		228	
10:45	12	15			27	22:45	7	4			11	Peak Hour	16:45	16:30		16:00	
11:00	13	17			30	23:00	10	3			13	Peak Volume	67	53		116	
11:15	16	16			32	23:15	1	1			2	Peak Hour Factor	0.838	0.828		0.879	
11:30	11	19			30	23:30	4	4			8						
11:45	18	10			28	23:45	1	2			3						
TOTALS	212	241	0	0	453	TOTALS	547	427	0	0	974						
SPLIT %	47%	53%	0%	0%	32%	SPLIT %	56%	44%	0%	0%	68%						



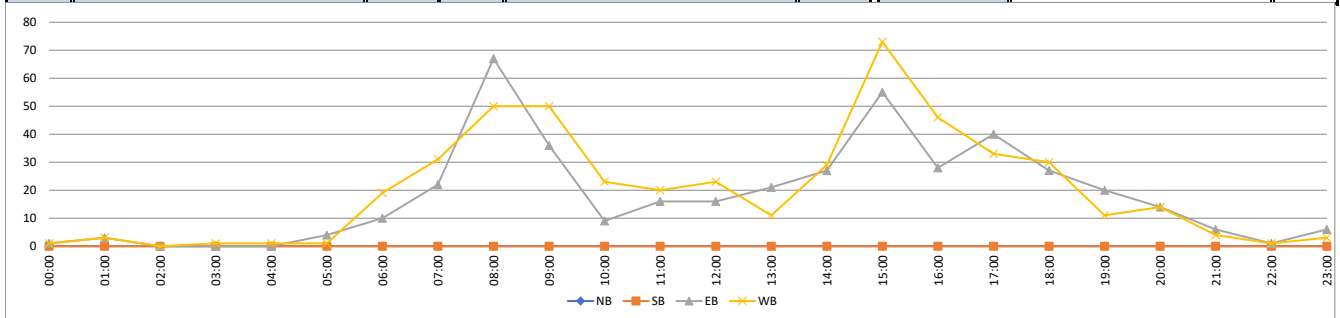
VOLUME

Ridge Rd Bet. Lastner Ln & Research Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_004

DAILY TOTALS						NB					SB					EB					WB					Total																																																																																																																																
						0					0					429					478					907																																																																																																																																
15-Minutes Interval											Hourly Intervals																																																																																																																																															
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																									
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06:00			2	4	6	18:00			7	4	11	<table border="1"> <thead> <tr> <th colspan="6">STATISTICS</th> </tr> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="5">00:00 to 12:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>168</td><td>200</td><td>368</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>8:30</td><td>8:15</td><td>8:30</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>69</td><td>67</td><td>136</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.616</td><td>0.644</td><td>0.680</td> </tr> <tr> <td>Peak Period</td><td colspan="5">12:00 to 00:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>261</td><td>278</td><td>539</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>14:45</td><td>15:00</td><td>15:00</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>59</td><td>73</td><td>128</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.702</td><td>0.760</td><td>0.780</td> </tr> <tr> <td>Peak Period</td><td colspan="5">07:00 to 09:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>89</td><td>81</td><td>170</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>8:00</td><td>8:00</td><td>8:00</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>67</td><td>50</td><td>117</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.598</td><td>0.568</td><td>0.585</td> </tr> <tr> <td>Peak Period</td><td colspan="5">16:00 to 18:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>68</td><td>79</td><td>147</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>17:00</td><td>16:15</td><td>16:30</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>40</td><td>47</td><td>80</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.833</td><td>0.783</td><td>0.909</td> </tr> </tbody> </table>						STATISTICS							NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume			168	200	368	Peak Hour			8:30	8:15	8:30	Peak Volume			69	67	136	Peak Hour Factor			0.616	0.644	0.680	Peak Period	12:00 to 00:00					Volume			261	278	539	Peak Hour			14:45	15:00	15:00	Peak Volume			59	73	128	Peak Hour Factor			0.702	0.760	0.780	Peak Period	07:00 to 09:00					Volume			89	81	170	Peak Hour			8:00	8:00	8:00	Peak Volume			67	50	117	Peak Hour Factor			0.598	0.568	0.585	Peak Period	16:00 to 18:00					Volume			68	79	147	Peak Hour			17:00	16:15	16:30	Peak Volume			40	47	80	Peak Hour Factor			0.833	0.783	0.909					
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SPLIT %	0%	0%	46%	54%	41%	SPLIT %	0%	0%	48%	52%	59%																																																																																																																																															



Prepared by National Data & Surveying Services
CLASSIFICATION
 Ridge Rd Bet. Lastner Ln & Research Rd

Day: Friday
 Date: 11/17/2023

City: Greenbelt
 Project #: MD23_280047_004

Time	EASTBOUND													Total	WESTBOUND													Total	TOTALS													Total
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	
12:00	0	2	0	0	1	0	0	0	0	0	0	0	0	3	0	11	0	0	0	0	0	0	0	0	0	0	11	0	13	0	0	1	0	0	0	0	0	0	0	0	0	14
12:15	0	0	1	1	0	0	0	0	0	0	0	0	0	2	0	4	0	1	0	0	1	0	0	0	0	0	6	0	4	1	2	0	0	0	1	0	0	0	0	0	8	
12:30	0	4	0	0	0	0	0	0	0	0	0	0	0	4	1	1	0	0	0	0	0	0	0	0	0	2	1	5	0	0	0	0	0	0	0	0	0	0	0	6		
12:45	0	6	0	0	0	0	0	0	0	0	0	0	0	6	0	3	0	0	0	0	0	0	0	0	0	3	0	9	0	0	0	0	0	0	0	0	0	0	0	9		
13:00	0	8	2	1	0	0	0	0	0	0	0	0	0	11	0	3	3	0	0	0	0	0	0	0	0	6	0	11	5	1	0	0	0	0	0	0	0	0	0	17		
13:15	0	1	0	1	0	0	0	0	0	0	0	0	0	2	0	3	0	0	0	0	0	0	0	0	0	3	0	4	0	1	0	0	0	0	0	0	0	0	0	5		
13:30	0	4	2	0	0	0	0	0	0	0	0	0	0	6	0	2	0	2	1	0	0	0	0	0	0	5	0	6	2	2	1	0	0	0	0	0	0	0	0	11		
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14:00	0	3	1	0	1	0	0	0	0	0	0	0	0	5	0	4	1	0	1	0	0	0	0	0	6	0	7	2	0	2	0	0	0	0	0	0	0	0	11			
14:15	0	4	2	1	1	0	0	0	0	0	0	0	0	8	0	4	0	1	0	0	0	0	0	0	5	0	8	2	2	1	0	0	0	0	0	0	0	0	13			
14:30	0	7	0	0	0	0	0	0	0	0	0	0	0	7	0	5	2	1	0	0	0	0	0	0	8	0	12	2	1	0	0	0	0	0	0	0	0	0	15			
14:45	0	12	0	2	0	0	0	0	0	0	0	0	0	14	0	4	1	1	0	0	0	0	0	0	6	0	16	1	3	0	0	0	0	0	0	0	0	20				
15:00	1	18	1	1	0	0	0	0	0	0	0	0	0	21	0	9	0	0	0	0	0	0	0	0	9	1	27	1	1	0	0	0	0	0	0	0	0	30				
15:15	1	14	0	1	0	0	0	0	0	0	0	0	0	16	0	20	1	0	0	0	0	0	0	0	21	1	34	1	1	0	0	0	0	0	0	0	37					
15:30	0	3	0	2	0	0	0	0	0	0	0	0	0	5	1	22	1	2	0	0	0	0	0	0	26	1	25	1	4	0	0	0	0	0	0	0	31					
15:45	0	6	0	1	0	0	0	0	0	0	0	0	0	7	0	9	0	0	0	0	0	0	0	0	9	0	15	0	1	0	0	0	0	0	0	0	16					
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16:15	1	14	0	0	0	0	0	0	0	0	0	0	0	15	1	10	1	1	0	0	0	0	0	0	13	2	24	1	1	0	0	0	0	0	0	0	28					
16:30	0	4	1	2	0	0	0	0	0	0	0	0	0	7	1	9	1	2	0	0	0	0	0	0	13	1	13	2	4	0	0	0	0	0	0	0	20					
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17:00	1	9	0	1	0	0	0	0	0	0	0	0	0	11	0	5	0	2	1	0	0	0	0	0	8	1	14	0	3	1	0	0	0	0	0	0	19					
17:15	0	7	0	0	0	0	0	0	0	0	0	0	0	7	0	13	0	0	0	0	0	0	0	0	13	0	20	0	0	0	0	0	0	0	0	0	20					
17:30	0	3	1	1	0	0	0	0	0	0	0	0	0	5	0	7	0	1	0	0	0	0	0	0	8	0	10	1	2	0	0	0	0	0	0	0	13					
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19:15	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	4	0	0	0	0	0	0	0	0	4	0	6	0	0	0	0	0	0	0	0	0	6					
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20:45	0	0	0	2	0	0	0	0	0	0	0	0	0	2	0	2	0	1	0	0	0	0	0	0	3	0	2	0	3	0	0	0	0	0	0	0	5					
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21:30	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	1	0	0	0	0	0	0	0	0	1	0	5	0	0	0	0	0	0	0	0	0	5					
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22:15	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2					
22:30	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0	4					
22:45	1	1	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	2	1	3	0	0	0	0	0	0	0	0	0	4					
23:00	0	0	0	1	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	2	0	2	0	1	0	0	0	0	0	0	0	3					
23:15	1	1	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2					
23:30	0	0	0	1	0	0	0	0	0	0	0	0	0	1	0	2	0	1	0	0	0	0	0	0	3	0	2	0	2	0	0	0	0	0	0	0	4					
23:45	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	0	0	0	3					
Totals	9	351	33	44	11	1	0	0	0	0	0	0	0	449	11	397	27	43	5	0	1	0	0																			

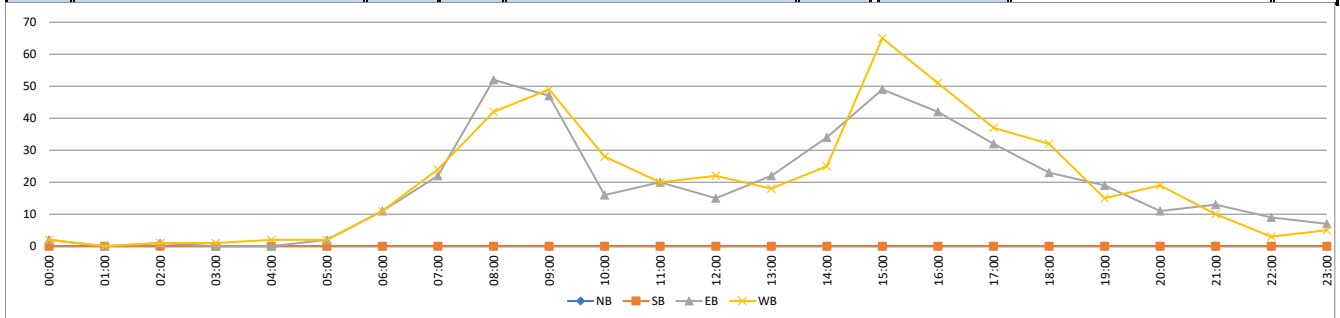
VOLUME

Ridge Rd Bet. Lastner Ln & Research Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_004

DAILY TOTALS						NB					SB					EB					WB					Total				
						0					0					449					484					933				
DAILY TOTALS																														
15-Minutes Interval												Hourly Intervals																		
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL													
00:00			0	0	0	12:00			3	11	14	00:00	01:00			2	2	4												
00:15			0	2	2	12:15			2	6	8	01:00	02:00			0	0	0												
00:30			2	0	2	12:30			4	2	6	02:00	03:00			1	1	2												
00:45			0	0	0	12:45			6	3	9	03:00	04:00			0	1	1												
01:00			0	0	0	13:00			11	6	17	04:00	05:00			0	2	2												
01:15			0	0	0	13:15			2	3	5	05:00	06:00			2	2	4												
01:30			0	0	0	13:30			6	5	11	06:00	07:00			11	11	22												
01:45			0	0	0	13:45			3	4	7	07:00	08:00			22	24	46												
02:00			0	0	0	14:00			5	6	11	08:00	09:00			52	42	94												
02:15			0	1	1	14:15			8	5	13	09:00	10:00			47	49	96												
02:30			0	0	0	14:30			7	8	15	10:00	11:00			16	28	44												
02:45			1	0	1	14:45			14	6	20	11:00	12:00			20	20	40												
03:00			0	0	0	15:00			21	9	30	12:00	13:00			15	22	37												
03:15			0	0	0	15:15			16	21	37	13:00	14:00			22	18	40												
03:30			0	1	1	15:30			5	26	31	14:00	15:00			34	25	59												
03:45			0	0	0	15:45			7	9	16	15:00	16:00			49	65	114												
04:00			0	0	0	16:00			10	13	23	16:00	17:00			42	51	93												
04:15			0	0	0	16:15			15	13	28	17:00	18:00			32	37	69												
04:30			0	1	1	16:30			7	13	20	18:00	19:00			23	32	55												
04:45			0	1	1	16:45			10	12	22	19:00	20:00			19	15	34												
05:00			0	0	0	17:00			11	8	19	20:00	21:00			11	19	30												
05:15			0	1	1	17:15			7	13	20	21:00	22:00			13	10	23												
05:30			1	0	1	17:30			5	8	13	22:00	23:00			9	3	12												
05:45			1	1	2	17:45			9	8	17	23:00	00:00			7	5	12												
06:00			3	2	5	18:00			10	9	19	STATISTICS																		
06:15			1	1	2	18:15			5	7	12							NB	SB	EB	WB	TOTAL								
06:30			3	2	5	18:30			3	11	14	Peak Period	00:00	to	12:00															
06:45			4	6	10	18:45			5	5	10	Volume																		
07:00			4	5	9	19:00			5	3	8	Peak Hour			173	182	355													
07:15			6	5	11	19:15			2	4	6	Peak Hour			8:30	8:30	8:30													
07:30			4	8	12	19:30			5	5	10	Peak Volume			71	66	137													
07:45			8	6	14	19:45			7	3	10	Peak Hour Factor			0.657	0.589	0.623													
08:00			10	5	15	20:00			4	5	9	Peak Period	12:00	to	00:00															
08:15			5	11	16	20:15			3	6	9	Volume			276	302	578													
08:30			16	11	27	20:30			2	5	7	Peak Hour			14:30	15:15	14:45													
08:45			21	15	36	20:45			2	3	5	Peak Volume			58	69	118													
09:00			27	28	55	21:00			3	2	5	Peak Hour Factor			0.690	0.663	0.797													
09:15			7	12	19	21:15			1	3	4	Peak Period	07:00	to	09:00															
09:30			7	5	12	21:30			4	1	5	Volume			74	66	140													
09:45			6	4	10	21:45			5	4	9	Peak Hour			8:00	8:00	8:00													
10:00			3	5	8	22:00			1	1	2	Peak Volume			52	42	94													
10:15			4	9	13	22:15			2	0	2	Peak Hour Factor			0.619	0.700	0.653													
10:30			2	8	10	22:30			4	0	4	Peak Period	16:00	to	18:00															
10:45			7	6	13	22:45			2	2	4	Volume			74	88	162													
11:00			0	4	4	23:00			1	2	3	Peak Hour			16:15	16:00	16:00													
11:15			9	5	14	23:15			2	0	2	Peak Volume			43	51	93													
11:30			5	3	8	23:30			1	3	4	Peak Hour Factor			0.717	0.981	0.830													
11:45			6	8	14	23:45			3	0	3																			
TOTALS	0	0	173	182	355	TOTALS	0	0	276	302	578																			
SPLIT %	0%	0%	49%	51%	38%	SPLIT %	0%	0%	48%	52%	62%																			



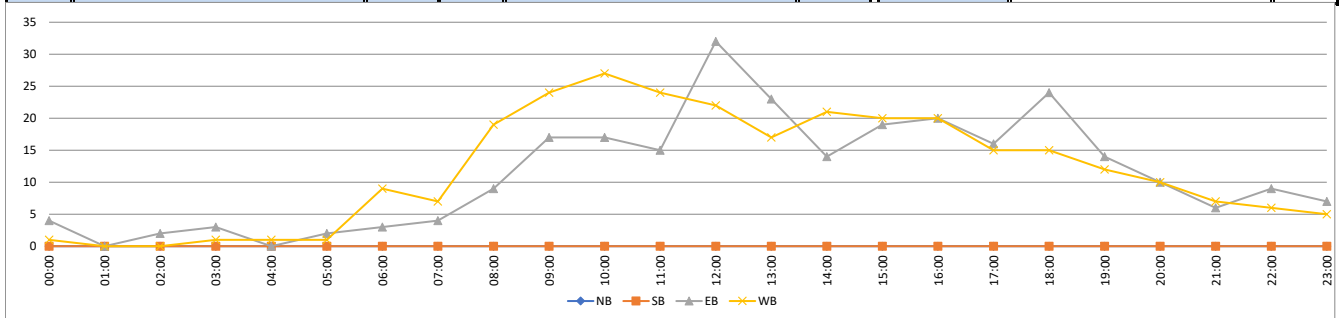
VOLUME

Ridge Rd Bet. Lastner Ln & Research Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_004

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					0	0	270	284	554								
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00			2	0	2	12:00			10	4	14	00:00 01:00			4	1	5
00:15			1	0	1	12:15			7	9	16	01:00 02:00			0	0	0
00:30			1	1	2	12:30			9	4	13	02:00 03:00			2	0	2
00:45			0	0	0	12:45			6	5	11	03:00 04:00			3	1	4
01:00			0	0	0	13:00			7	5	12	04:00 05:00			0	1	1
01:15			0	0	0	13:15			3	5	8	05:00 06:00			2	1	3
01:30			0	0	0	13:30			8	3	11	06:00 07:00			3	9	12
01:45			0	0	0	13:45			5	4	9	07:00 08:00			4	7	11
02:00			1	0	1	14:00			5	5	10	08:00 09:00			9	19	28
02:15			0	0	0	14:15			2	6	8	09:00 10:00			17	24	41
02:30			0	0	0	14:30			4	4	8	10:00 11:00			17	27	44
02:45			1	0	1	14:45			3	6	9	11:00 12:00			15	24	39
03:00			0	0	0	15:00			4	5	9	12:00 13:00			32	22	54
03:15			2	0	2	15:15			8	6	14	13:00 14:00			23	17	40
03:30			0	0	0	15:30			3	4	7	14:00 15:00			14	21	35
03:45			1	1	2	15:45			4	5	9	15:00 16:00			19	20	39
04:00			0	0	0	16:00			5	7	12	16:00 17:00			20	20	40
04:15			0	0	0	16:15			5	5	10	17:00 18:00			16	15	31
04:30			0	0	0	16:30			3	1	4	18:00 19:00			24	15	39
04:45			0	1	1	16:45			7	7	14	19:00 20:00			14	12	26
05:00			0	0	0	17:00			5	7	12	20:00 21:00			10	10	20
05:15			1	0	1	17:15			3	1	4	21:00 22:00			6	7	13
05:30			0	0	0	17:30			6	5	11	22:00 23:00			9	6	15
05:45			1	1	2	17:45			2	2	4	23:00 00:00			7	5	12
06:00			0	0	0	18:00			2	6	8	STATISTICS					
06:15			0	2	2	18:15			10	2	12		NB	SB	EB	WB	TOTAL
06:30			0	3	3	18:30			8	4	12	Peak Period	00:00 to 12:00				
06:45			3	4	7	18:45			4	3	7	Volume			76	114	190
07:00			1	4	5	19:00			3	4	7	Peak Hour	9:00 to 9:45				
07:15			2	1	3	19:15			6	5	11	Peak Volume			17	28	45
07:30			1	0	1	19:30			3	3	6	Peak Hour Factor			0.607	0.875	0.865
07:45			0	2	2	19:45			2	0	2	Peak Period	12:00 to 00:00				
08:00			4	8	12	20:00			2	5	7	Volume			194	170	364
08:15			4	0	4	20:15			0	3	3	Peak Hour	12:00 to 12:15				12:00
08:30			0	8	8	20:30			5	1	6	Peak Volume			32	23	54
08:45			1	3	4	20:45			3	1	4	Peak Hour Factor			0.800	0.639	0.844
09:00			7	8	15	21:00			2	2	4	Peak Period	07:00 to 09:00				
09:15			3	3	6	21:15			0	3	3	Volume			13	26	39
09:30			3	6	9	21:30			1	0	1	Peak Hour	7:30 to 8:00				8:00
09:45			4	7	11	21:45			3	2	5	Peak Volume			9	19	28
10:00			5	7	12	22:00			3	2	5	Peak Hour Factor			0.563	0.594	0.583
10:15			3	6	9	22:15			1	1	2	Peak Period	16:00 to 18:00				
10:30			5	8	13	22:30			3	1	4	Volume			36	35	71
10:45			4	6	10	22:45			2	2	4	Peak Hour	16:45 to 16:00				16:45
11:00			4	6	10	23:00			4	2	6	Peak Volume			21	20	41
11:15			2	5	7	23:15			2	1	3	Peak Hour Factor			0.750	0.714	0.732
11:30			1	8	9	23:30			0	1	1						
11:45			8	5	13	23:45			1	1	2						
TOTALS	0	0	76	114	190	TOTALS	0	0	194	170	364						
SPLIT %	0%	0%	40%	60%	34%	SPLIT %	0%	0%	53%	47%	66%						



SPEED

Ridge Rd Bet. Lastnr Ln & Research Rd

Day: Thursday

City: Greenbelt

Date: 11/16/2023

Project #: MD23_280047_004

Time	EASTBOUND												Total	WESTBOUND												Total	TOTALS												Total					
	5	15	20	25	30	35	40	45	50	55	60	65		70	70	99	5	15	20	25	30	35	40	45	50		55	60	65	70	70	99	5	15	20	25	30	35		40	45	50	55	60
00:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
01:00	0	0	1	0	0	2	0	0	0	0	0	0	0	0	3	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	3	0	0	2	1	3	0	0	0	0	0	0	0	6
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	
04:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	
05:00	0	0	1	1	1	0	1	0	0	0	0	0	0	0	4	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	1	2	0	1	0	0	0	0	0	0	5	
06:00	0	0	0	2	4	3	1	0	0	0	0	0	0	0	10	0	1	6	10	2	0	0	0	0	0	0	0	0	0	19	0	1	8	14	5	1	0	0	0	0	0	0	29	
07:00	0	0	2	10	6	3	1	0	0	0	0	0	0	0	22	0	3	10	13	4	1	0	0	0	0	0	0	0	31	0	3	12	23	10	4	1	0	0	0	0	0	53		
08:00	1	2	11	31	15	7	0	0	0	0	0	0	0	67	0	1	14	23	10	1	1	0	0	0	0	0	0	50	1	3	25	54	25	8	1	0	0	0	0	0	0	117		
09:00	1	1	4	15	9	5	1	0	0	0	0	0	0	36	1	2	14	23	10	0	0	0	0	0	0	0	0	50	2	3	18	38	19	5	1	0	0	0	0	0	0	86		
10:00	0	0	1	6	2	0	0	0	0	0	0	0	0	9	0	4	5	10	3	1	0	0	0	0	0	0	0	23	0	4	6	16	5	1	0	0	0	0	0	0	0	32		
11:00	1	2	5	2	4	2	0	0	0	0	0	0	0	16	1	3	2	9	5	0	0	0	0	0	0	0	0	20	2	5	7	11	9	2	0	0	0	0	0	0	36			
12:00	3	0	6	3	4	4	0	0	0	0	0	0	0	16	1	1	10	8	3	0	0	0	0	0	0	0	0	23	4	1	16	11	7	0	0	0	0	0	0	0	39			
13:00	0	0	5	8	7	1	0	0	0	0	0	0	0	21	1	1	3	3	3	0	0	0	0	0	0	0	0	11	1	1	8	11	10	1	0	0	0	0	0	0	32			
14:00	2	4	8	9	3	1	0	0	0	0	0	0	0	27	4	3	10	9	2	1	0	0	0	0	0	0	0	29	6	7	18	18	5	2	0	0	0	0	0	0	56			
15:00	0	6	14	22	10	3	0	0	0	0	0	0	0	55	7	10	30	17	9	0	0	0	0	0	0	0	0	73	7	16	44	39	19	3	0	0	0	0	0	128				
16:00	0	5	6	12	4	1	0	0	0	0	0	0	0	28	1	3	17	21	4	0	0	0	0	0	0	0	0	46	1	8	23	33	8	1	0	0	0	0	0	0	74			
17:00	2	7	10	15	5	1	0	0	0	0	0	0	0	40	0	7	17	4	5	0	0	0	0	0	0	0	0	33	2	14	27	19	10	1	0	0	0	0	0	0	73			
18:00	1	4	8	12	1	0	1	0	0	0	0	0	0	27	0	4	15	10	1	0	0	0	0	0	0	0	0	30	1	8	23	22	2	0	1	0	0	0	0	0	57			
19:00	0	2	11	5	1	1	0	0	0	0	0	0	0	20	1	1	7	2	0	0	0	0	0	0	0	0	0	11	1	3	18	7	1	1	0	0	0	0	0	0	31			
20:00	0	2	6	5	1	0	0	0	0	0	0	0	0	14	1	3	6	3	1	0	0	0	0	0	0	0	0	14	1	5	12	8	2	0	0	0	0	0	0	0	28			
21:00	0	2	2	2	0	0	0	0	0	0	0	0	0	6	0	0	2	2	0	0	0	0	0	0	0	0	0	4	0	2	4	4	0	0	0	0	0	0	0	0	10			
22:00	0	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	1	1	0	0	0	0	0	0	0	0	2		
23:00	0	0	0	1	5	0	0	0	0	0	0	0	0	6	0	0	0	3	0	0	0	0	0	0	0	0	0	3	0	0	1	8	0	0	0	0	0	0	0	0	0	9		
Totals	11	39	103	170	76	27	3	0	0	0	0	0	0	429	18	48	172	172	63	4	1	0	0	0	0	0	478	29	87	275	342	139	31	4	0	0	0	0	0	0	907			
% of Totals	3%	9%	24%	40%	18%	6%	1%							100%	4%	10%	36%	36%	13%	1%	0%					100%	3%	10%	30%	38%	15%	3%	0%								100%			

Time	EASTBOUND												Total	WESTBOUND												Total														
	5	15	20	25	30	35	40	45	50	55	60	65		70	70	99	5	15	20	25	30	35	40	45	50		55	60	65	70	70	99								
00:00 - 12:00	3	7	26	71	40	19	2	0	0	0	0	0	0	168	2	15	55	90	34	3	1	0	0	0	0	0	200	5	22	81	161	74	22	3	0	0	0	0	0	368
%	1%	2%	6%	17%	9%	4%	0%	0%	0%	0%	0%	0%	0%	39%	0%	3%	13%	21%	8%	1%	0%	0%	0%	0%	0%	0%	47%	1%	5%	19%	38%	17%	5%	1%	0%	0%	0%	0%	0%	86%
Peak Hour	11:15	7:15	8:00	8:15	8:30	8:00	6:45	0:00	0:00	0:00	0:00	0:00	0:00	8:30	8:30	10:30	8:45	8:45	8:30	6:45	7:45	0:00	0:00	0:00	0:00	8:15	11:15	10:30	8:00	8:15	8:30	8:00	8:15	0:00	0:00	0:00	0:00	0:00	0:00	8:30
Peak Volume	3	2	11	33	21	7	1	0	0	0	0	0	0	69	1	5	18	34	13	1	1	0	0	0	0	67	4	6	25	66	34	8	2	0	0	0	0	0	136	
12:00 - 24:00	8	32	77	99	36	8	1	0	0	0	0	0	0	261	16	33	117	82	29	1	0	0	0	0	0	278	24	65	194	181	65	9	1	0	0	0	0	0	539	
%	2%	7%	18%	23%	8%	2%	0%	0%	0%	0%	0%	0%	0%	61%	4%	8%	27%	19%	7%	0%	0%	0%	0%	0%	65%	6%	15%	45%	42%	15%	2%	0%	0%	0%	0%	0%	0%	0%	126%	
Peak Hour	12:00	15:30	14:30	14:45	14:45	14:30	17:45	12:00	12:00	12:00	12:00	12:00	12:00	14:45	14:30	14:45	15:00	15:45	15:00	13:45	12:00	12:00	12:00	12:00	12:00	15:00	14:30	14:45	15:00	15:00	14:45	14:30	17:45	12:00	12:00	12:00	12:00	12:00	15:00	
Peak Volume	3	7	16	23	11	4	1	0	0	0	0	0	0	59	8	12	30	21	9	1	0	0	0	0	73	8	18	44	39	19	5	1	0	0	0	0	0	128		
07:00 - 09:00	1	2	13	41	21	10	1	0	0	0	0	0	0	89	0	4	24	36	14	2	1	0	0	0	0	81	1	6	37	77	35	12	2	0	0	0	0	0	170	
%	0%	0%	3%	10%	5%	2%	0%	0%	0%	0%	0%	0%	0%	21%	0%	1%	6%	8%	3%	0%	0%	0%	0%	0%	19%	0%	1%	9%	18%	8%	3%	0%	0%	0%	0%	0%	0%	0%	40%	
Peak Hour	8:00	7:15	8:00	8:00	8:00	8:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	8:00	7:00	7:00	8:00	8:00	7:00	7:45	7:00	7:00	7:00	7:00	8:00	8:00	7:15	8:00	8:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	8:00	
Peak Volume	1	2	11	31	15	7	1	0	0	0	0	0	0	67	0	3	14	23	10	1	1	0	0	0	50	1	4	25	54	25	8	1	0	0	0	0	0	0	117	
16:00 - 18:00	2	12	16	27	9	2	0	0	0	0	0	0	0	68	1	10	34	25	9	0	0	0	0	0	79	3	22	50	52	18	2	0	0	0	0	0	0	0	147	
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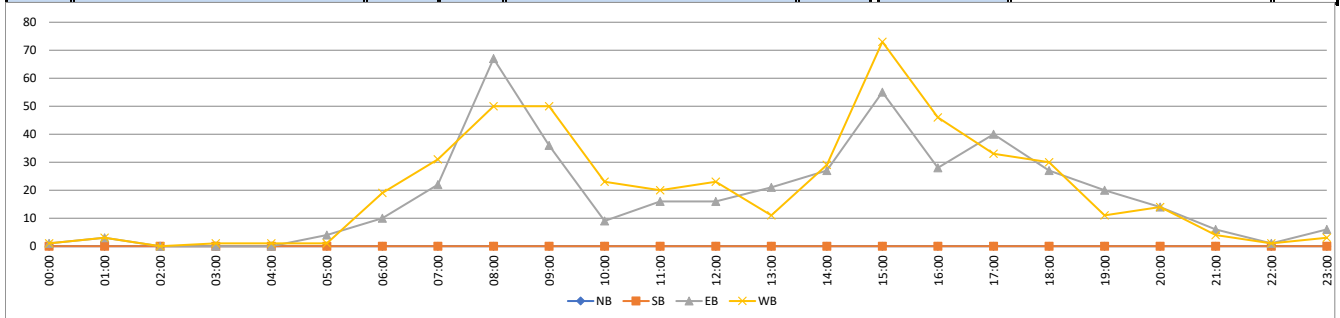
VOLUME

Ridge Rd Bet. Lastner Ln & Research Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_004

DAILY TOTALS						NB					SB					EB					WB					Total																																																																																																																																
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06:00			2	4	6	18:00			7	4	11	<table border="1"> <thead> <tr> <th colspan="6">STATISTICS</th> </tr> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="5">00:00 to 12:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>168</td><td>200</td><td>368</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>8:30</td><td>8:15</td><td>8:30</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>69</td><td>67</td><td>136</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.616</td><td>0.644</td><td>0.680</td> </tr> <tr> <td>Peak Period</td><td colspan="5">12:00 to 00:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>261</td><td>278</td><td>539</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>14:45</td><td>15:00</td><td>15:00</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>59</td><td>73</td><td>128</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.702</td><td>0.760</td><td>0.780</td> </tr> <tr> <td>Peak Period</td><td colspan="5">07:00 to 09:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>89</td><td>81</td><td>170</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>8:00</td><td>8:00</td><td>8:00</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>67</td><td>50</td><td>117</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.598</td><td>0.568</td><td>0.585</td> </tr> <tr> <td>Peak Period</td><td colspan="5">16:00 to 18:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>68</td><td>79</td><td>147</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>17:00</td><td>16:15</td><td>16:30</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>40</td><td>47</td><td>80</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.833</td><td>0.783</td><td>0.909</td> </tr> </tbody> </table>						STATISTICS							NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume			168	200	368	Peak Hour			8:30	8:15	8:30	Peak Volume			69	67	136	Peak Hour Factor			0.616	0.644	0.680	Peak Period	12:00 to 00:00					Volume			261	278	539	Peak Hour			14:45	15:00	15:00	Peak Volume			59	73	128	Peak Hour Factor			0.702	0.760	0.780	Peak Period	07:00 to 09:00					Volume			89	81	170	Peak Hour			8:00	8:00	8:00	Peak Volume			67	50	117	Peak Hour Factor			0.598	0.568	0.585	Peak Period	16:00 to 18:00					Volume			68	79	147	Peak Hour			17:00	16:15	16:30	Peak Volume			40	47	80	Peak Hour Factor			0.833	0.783	0.909					
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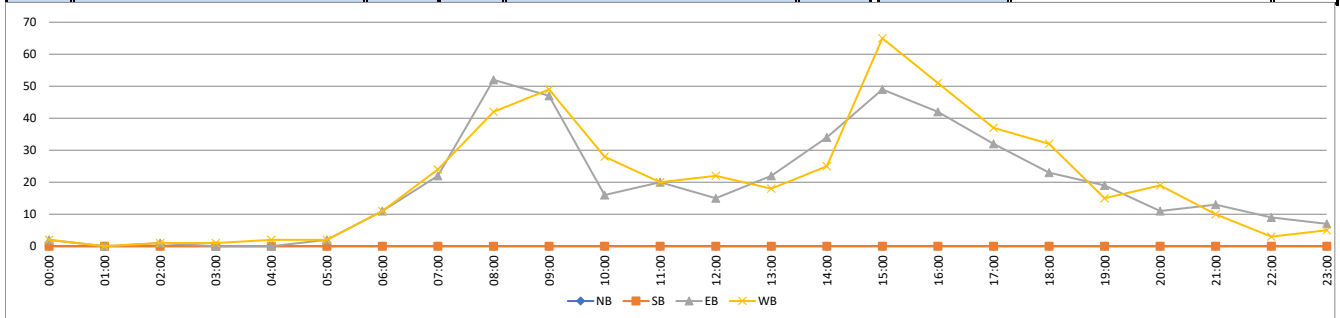
VOLUME

Ridge Rd Bet. Lastner Ln & Research Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_004

DAILY TOTALS						NB					SB					EB					WB					Total				
						0					0					449					484					933				
DAILY TOTALS																														
15-Minutes Interval												Hourly Intervals																		
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL													
00:00			0	0	0	12:00			3	11	14	00:00	01:00			2	2	4												
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00:30			2	0	2	12:30			4	2	6	02:00	03:00			1	1	2												
00:45			0	0	0	12:45			6	3	9	03:00	04:00			0	1	1												
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06:45			4	6	10	18:45			5	5	10	Volume					355													
07:00			4	5	9	19:00			5	3	8	Peak Hour	8:30 8:30				8:30													
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07:45			8	6	14	19:45			7	3	10	Peak Period	12:00 to 00:00																	
08:00			10	5	15	20:00			4	5	9	Volume					578													
08:15			5	11	16	20:15			3	6	9	Peak Hour	14:30 15:15				14:45													
08:30			16	11	27	20:30			2	5	7	Peak Volume					118													
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09:00			27	28	55	21:00			3	2	5	Peak Period	07:00 to 09:00																	
09:15			7	12	19	21:15			1	3	4	Volume					140													
09:30			7	5	12	21:30			4	1	5	Peak Hour	8:00 8:00				8:00													
09:45			6	4	10	21:45			5	4	9	Peak Volume					94													
10:00			3	5	8	22:00			1	1	2	Peak Hour Factor					0.653													
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11:00			0	4	4	23:00			1	2	3	Peak Volume					93													
11:15			9	5	14	23:15			2	0	2	Peak Hour Factor					0.830													
11:30			5	3	8	23:30			1	3	4																			
11:45			6	8	14	23:45			3	0	3																			
TOTALS	0	0	173	182	355	TOTALS	0	0	276	302	578																			
SPLIT %	0%	0%	49%	51%	38%	SPLIT %	0%	0%	48%	52%	62%																			



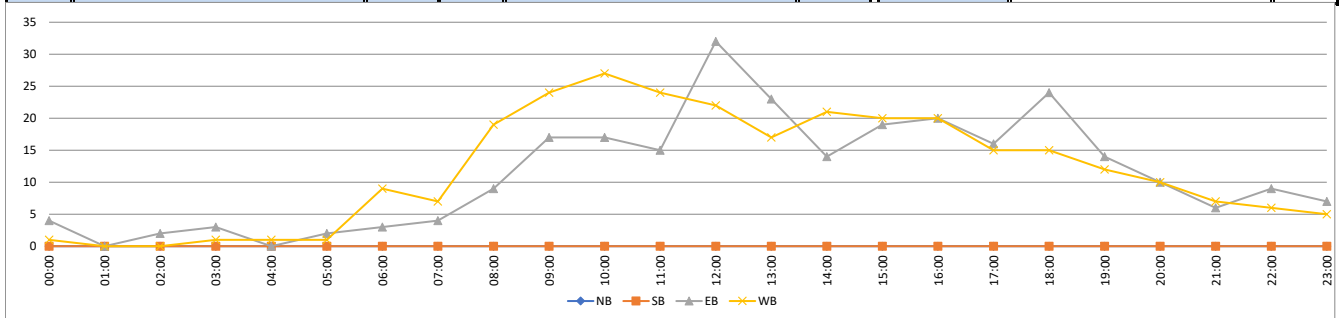
VOLUME

Ridge Rd Bet. Lastner Ln & Research Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_004

DAILY TOTALS						NB					SB					EB					WB					Total																																																																																																																																
						0					0					270					284					554																																																																																																																																
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06:00			0	0	0	18:00			2	6	8	<table border="1"> <thead> <tr> <th colspan="6">STATISTICS</th> </tr> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="5">00:00 to 12:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>76</td><td>114</td><td>190</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>9:00</td><td>9:45</td><td>9:45</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>17</td><td>28</td><td>45</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.607</td><td>0.875</td><td>0.865</td> </tr> <tr> <td>Peak Period</td><td colspan="5">12:00 to 00:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>194</td><td>170</td><td>364</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>12:00</td><td>12:15</td><td>12:00</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>32</td><td>23</td><td>54</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.800</td><td>0.639</td><td>0.844</td> </tr> <tr> <td>Peak Period</td><td colspan="5">07:00 to 09:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>13</td><td>26</td><td>39</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>7:30</td><td>8:00</td><td>8:00</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>9</td><td>19</td><td>28</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.563</td><td>0.594</td><td>0.583</td> </tr> <tr> <td>Peak Period</td><td colspan="5">16:00 to 18:00</td> </tr> <tr> <td>Volume</td><td colspan="2"></td><td>36</td><td>35</td><td>71</td> </tr> <tr> <td>Peak Hour</td><td colspan="2"></td><td>16:45</td><td>16:00</td><td>16:45</td> </tr> <tr> <td>Peak Volume</td><td colspan="2"></td><td>21</td><td>20</td><td>41</td> </tr> <tr> <td>Peak Hour Factor</td><td colspan="2"></td><td>0.750</td><td>0.714</td><td>0.732</td> </tr> </tbody> </table>						STATISTICS							NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume			76	114	190	Peak Hour			9:00	9:45	9:45	Peak Volume			17	28	45	Peak Hour Factor			0.607	0.875	0.865	Peak Period	12:00 to 00:00					Volume			194	170	364	Peak Hour			12:00	12:15	12:00	Peak Volume			32	23	54	Peak Hour Factor			0.800	0.639	0.844	Peak Period	07:00 to 09:00					Volume			13	26	39	Peak Hour			7:30	8:00	8:00	Peak Volume			9	19	28	Peak Hour Factor			0.563	0.594	0.583	Peak Period	16:00 to 18:00					Volume			36	35	71	Peak Hour			16:45	16:00	16:45	Peak Volume			21	20	41	Peak Hour Factor			0.750	0.714	0.732					
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SPLIT %	0%	0%	40%	60%	34%	SPLIT %	0%	0%	53%	47%	66%																																																																																																																																															



Prepared by National Data & Surveying Services
CLASSIFICATION
 Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Thursday
 Date: 11/16/2023

City: Greenbelt
 Project #: MD23_280047_005

Time	NORTHBOUND													Total	SOUTHBOUND													Total	TOTALS													Total			
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13				
00:00	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
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06:00	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	7	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
07:00	0	9	2	0	0	0	0	0	0	0	0	0	0	11	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	14	2	0	0	0	0	0	0	0	0	0	0	0	0	16	
08:00	0	21	1	0	0	0	0	0	0	0	0	0	0	22	0	8	1	0	0	0	0	0	0	0	0	0	0	9	0	29	2	0	0	0	0	0	0	0	0	0	0	0	31		
09:00	0	16	0	0	1	1	0	0	0	0	0	0	0	18	0	12	0	0	0	0	0	0	0	0	0	0	12	0	28	0	0	1	1	0	0	0	0	0	0	0	0	0	30		
10:00	0	6	1	0	0	0	0	0	0	0	0	0	0	7	0	8	2	0	1	0	0	0	0	0	0	0	11	0	14	3	0	1	0	0	0	0	0	0	0	0	0	0	18		
11:00	0	14	3	0	0	1	0	0	0	0	0	0	0	18	0	9	1	0	0	0	0	0	0	0	0	0	10	0	23	4	0	0	1	0	0	0	0	0	0	0	0	0	28		
12:00	0	10	2	0	0	1	0	0	0	0	0	0	0	13	0	10	1	0	0	0	0	0	0	0	0	0	11	0	20	3	0	0	1	0	0	0	0	0	0	0	0	0	24		
13:00	0	9	0	0	1	1	0	0	0	0	0	0	0	11	0	15	0	1	0	0	0	0	0	0	0	0	16	0	24	0	2	1	0	0	0	0	0	0	0	0	0	0	27		
14:00	0	12	1	0	0	0	0	0	0	0	0	0	0	13	0	22	2	0	0	0	0	0	0	0	0	0	24	0	34	3	0	0	0	0	0	0	0	0	0	0	0	37			
15:00	1	8	3	0	1	1	0	0	0	0	0	0	0	14	0	21	1	0	0	0	0	0	0	0	0	22	1	29	4	0	1	1	0	0	0	0	0	0	0	0	0	36			
16:00	0	10	4	0	0	1	1	0	0	0	0	0	0	16	0	14	2	0	0	0	0	0	0	0	0	16	0	24	6	0	1	1	0	0	0	0	0	0	0	0	0	0	32		
17:00	1	12	0	0	1	0	0	0	0	0	0	0	0	14	0	24	1	0	1	0	0	0	0	0	0	26	1	36	1	0	2	0	0	0	0	0	0	0	0	0	0	0	40		
18:00	0	12	0	0	0	0	0	0	0	0	0	0	0	12	0	15	1	0	1	0	0	0	0	0	0	17	0	27	1	0	1	0	0	0	0	0	0	0	0	0	0	0	29		
19:00	0	6	0	0	0	0	0	0	0	0	0	0	0	6	0	11	1	0	0	0	0	0	0	0	0	12	0	17	1	0	0	0	0	0	0	0	0	0	0	0	0	0	18		
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Totals	2	162	19	0	4	6	1	0	0	0	0	0	0	194	0	208	13	0	5	0	0	0	0	0	0	226	2	370	32	0	9	6	1	0	0	0	0	0	0	0	0	0	420		
% of Total	1%	84%	10%	0%	2%	3%	1%	0%	0%	0%	0%	0%	0%	100%	0%	92%	6%	0%	2%	0%	0%	0%	0%	0%	100%	0%	88%	8%	0%	2%	1%	1%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	100%		

CLASSIFICATION DEFINITIONS

#1 Motorcycles	#2 Passenger Cars	#3 2-Axle, 4-Tire Single Unit	#4 Buses	#5 2-Axle, 6-Tire Single Units	#6 3-Axle Single Units	#7 >=4-Axle Single Units
#8 <=4-Axle Single Trailers	#9 5-Axle Single Trailers	#10 >=6-Axle Single Trailers	#11 <=5-Axle Multi-Trailers	#12 6-Axle Multi-Trailers	#13 >=7-Axle Multi-Trailers	

STATISTICS

Time	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total
00:00 - 12:00	0	72	8	0	1	2	0	0	0	0	0	0	0	83	0	45	4	0	1	0	0	0	0	0	0	0	0	50	0	117	12	0	2	2	0	0	0	0	0	0	0	133
Peak Hour	0:00	8:15	10:30	0:00	8:30	11:15	0:00	0:00	0:00	0:00	0:00	0:00	0:00	8:15	0:00	8:30	10:15	0:00	10:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	8:30	0:00	28%	3%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	8:30
Peak Volume	0	22	3	0	1	2	0	0	0	0	0	0	0	24	0	14	3	0	1	0	0	0	0	0	0	0	15	0	35	6	0	1	2	0	0	0	0	0	0	0	39	
12:00 - 24:00	2	90	11	0	3	4	1	0	0	0	0	0	0	111	0	163	9	0	4	0	0	0	0	0	0	0	176	2	253	20	0	7	4	1	0	0	0	0	0	0	287	
Peak Hour	14:30	16:45	14:45	12:00	12:30	12:00	16:00	12:00	12:00	12:00	12:00	12:00	12:00	16:30	12:00	13:30	14:00	12:00	12:30	12:00	12:00	12:00	12:00	12:00	12:00	12:00	13:45	14:30	17:15	14:45	12:00	12:30	12:00	16:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	15:15
Peak Volume	1	15	4	0	1	1	1	0	0	0	0	0	0	18	0	26	2	0	1	0	0	0	0	0	0	0	27	1	38	6	0	2	1	1	0	0	0	0	0	0	43	
07:00 - 09:00	0	30	3	0	0	0	0	0	0	0	0	0	0	33	0	13	1	0	0	0	0	0	0	0	0	0	14	0	43	4	0	0	0	0	0	0	0	0	0	0	47	
Peak Hour	7:00	7:45	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	8:00	7:45	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	8:00	7:00	8:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	8:00	
Peak Volume	0	21	2	0	0	0	0	0	0	0	0	0	0	22	0	8	1	0	0	0	0	0	0	0	0	0	9	0	29	2	0	0	0	0	0	0	0	0	0	0	31	
16:00 - 18:00	1	22	4	0	1	1	0	0	0	0	0	0	0	30	0	38	3	0	1	0	0	0	0	0	0	0	42	1	60	7	0	2	1	1	0	0	0	0	0	0	72	
Peak Hour	17:00	16:45	16:00	16:00	16:30	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:30	16:00	17:00	16:00	16:00	16:15	16:00	16:00	16:00	16:00	16:00	16:00	17:00	17:00	17:00	16:00	16:00	16:30	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	17%	
Peak Volume	1	15	4	0	1	1	1	0	0	0	0	0	0	18	0	24	2	0	1	0	0	0																				

CLASSIFICATION
Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_005

Time	NORTHBOUND													Total	SOUTHBOUND													Total	TOTALS													Total	
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13		
12:00	0	4	1	0	0	1	0	0	0	0	0	0	0	6	0	2	0	0	0	0	0	0	0	0	0	2	0	6	1	0	0	1	0	0	0	0	0	0	0	0	0	0	8
12:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
12:30	0	2	1	0	0	0	0	0	0	0	0	0	0	3	0	2	0	0	0	0	0	0	0	0	2	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5
12:45	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	4	1	0	0	0	0	0	0	0	5	0	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9
13:00	0	2	0	0	0	1	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	3	0	5	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	6
13:15	0	2	0	0	1	0	0	0	0	0	0	0	0	3	0	1	0	0	1	0	0	0	0	2	0	3	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	5	
13:30	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	5	0	0	0	0	0	0	0	5	0	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9
13:45	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	6	0	0	0	0	0	0	0	6	0	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
14:00	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	6	0	0	0	0	0	0	0	6	0	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	
14:15	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	9	0	0	0	0	0	0	0	9	0	11	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	
14:30	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	5	1	0	0	0	0	0	0	6	0	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
14:45	0	3	1	0	0	0	0	0	0	0	0	0	0	4	0	2	1	0	0	0	0	0	3	0	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
15:00	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	1	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2		
15:15	1	3	1	0	0	0	0	0	0	0	0	0	0	5	0	5	0	0	0	0	0	0	5	1	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10		
15:30	0	2	1	0	0	1	0	0	0	0	0	0	0	4	0	4	1	0	0	0	0	0	5	0	6	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	9		
15:45	0	3	0	0	1	0	0	0	0	0	0	0	0	4	0	11	0	0	0	0	0	0	0	11	0	14	0	0	1	0	0	0	0	0	0	0	0	0	0	0	15		
16:00	0	3	1	0	0	0	0	0	0	0	0	0	0	4	0	4	1	0	0	0	0	0	5	0	7	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
16:15	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	4	0	0	0	0	0	0	4	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5		
16:30	0	3	2	0	0	1	0	0	0	0	0	0	0	6	0	3	0	0	0	0	0	0	3	0	6	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	9		
16:45	0	4	0	0	0	0	1	0	0	0	0	0	0	5	0	3	1	0	0	0	0	0	4	0	7	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	9		
17:00	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	4	1	0	1	0	0	0	6	0	7	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	9		
17:15	0	3	0	0	1	0	0	0	0	0	0	0	0	4	0	6	0	0	0	0	0	0	6	0	9	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	10		
17:30	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	7	0	0	0	0	0	0	7	0	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12		
17:45	1	1	0	0	0	0	0	0	0	0	0	0	0	2	0	7	0	0	0	0	0	0	7	1	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
18:00	0	6	0	0	0	0	0	0	0	0	0	0	0	6	0	3	0	0	0	0	0	0	3	0	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
18:15	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	6	1	0	1	0	0	0	8	0	9	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	11		
18:30	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	3	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6		
18:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3		
19:00	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	0	1	0	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3		
19:15	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	2	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4		
19:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5		
19:45	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	4	0	0	0	0	0	0	4	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6		
20:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5		
20:15	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	3	0	0	0	0	0	0	3	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4		
20:30	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	3	0	0	0	0	0	0	3	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4		
20:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1		
21:00	0	6	0	0	0	0	0	0	0	0	0	0	0	6	0	3	0	0	0	0	0	0	3	0	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
21:15	0	2	0	0	0	0	0	0	0	0	0	0	0	2	0	1	0	0	0	0	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3		
21:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2		
21:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3		
22:00	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	4	0	1	0	0	0	0	5	0	4	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	6		
22:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1			
22:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1		
22:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
23:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2	0	2	0	0	0															

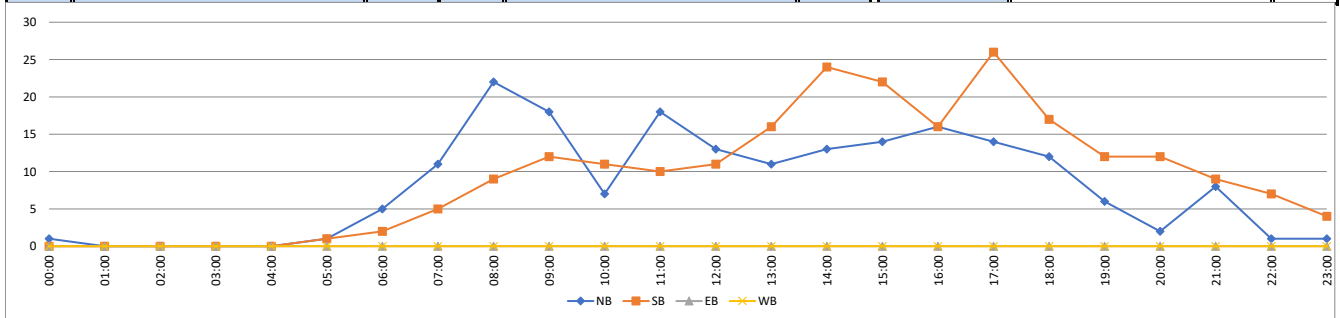
VOLUME

Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_005

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS																																																																																																																																									
					194	226	0	0	420																																																																																																																																										
15-Minutes Interval											Hourly Intervals																																																																																																																																								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																		
00:00	0	0			0	12:00	6	2			8	00:00	01:00	1	0			1																																																																																																																																	
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06:00	1	2			3	18:00	6	3			9	<table border="1"> <thead> <tr> <th colspan="5">STATISTICS</th> </tr> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="4">00:00 to 12:00</td><td></td> </tr> <tr> <td>Volume</td><td>83</td><td>50</td><td></td><td></td><td>133</td> </tr> <tr> <td>Peak Hour</td><td colspan="4">8:15 8:30</td><td>8:30</td> </tr> <tr> <td>Peak Volume</td><td>24</td><td>15</td><td></td><td></td><td>39</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.600</td><td>0.750</td><td></td><td></td><td>0.696</td> </tr> <tr> <td>Peak Period</td><td colspan="4">12:00 to 00:00</td><td></td> </tr> <tr> <td>Volume</td><td>111</td><td>176</td><td></td><td></td><td>287</td> </tr> <tr> <td>Peak Hour</td><td colspan="4">16:30 13:45</td><td>15:15</td> </tr> <tr> <td>Peak Volume</td><td>18</td><td>27</td><td></td><td></td><td>43</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.750</td><td>0.750</td><td></td><td></td><td>0.717</td> </tr> <tr> <td>Peak Period</td><td colspan="4">07:00 to 09:00</td><td></td> </tr> <tr> <td>Volume</td><td>33</td><td>14</td><td></td><td></td><td>47</td> </tr> <tr> <td>Peak Hour</td><td colspan="4">8:00 8:00</td><td>8:00</td> </tr> <tr> <td>Peak Volume</td><td>22</td><td>9</td><td></td><td></td><td>31</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.550</td><td>0.450</td><td></td><td></td><td>0.554</td> </tr> <tr> <td>Peak Period</td><td colspan="4">16:00 to 18:00</td><td></td> </tr> <tr> <td>Volume</td><td>30</td><td>42</td><td></td><td></td><td>72</td> </tr> <tr> <td>Peak Hour</td><td colspan="4">16:30 17:00</td><td>16:45</td> </tr> <tr> <td>Peak Volume</td><td>18</td><td>26</td><td></td><td></td><td>40</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.750</td><td>0.929</td><td></td><td></td><td>0.833</td> </tr> </tbody> </table>					STATISTICS						NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume	83	50			133	Peak Hour	8:15 8:30				8:30	Peak Volume	24	15			39	Peak Hour Factor	0.600	0.750			0.696	Peak Period	12:00 to 00:00					Volume	111	176			287	Peak Hour	16:30 13:45				15:15	Peak Volume	18	27			43	Peak Hour Factor	0.750	0.750			0.717	Peak Period	07:00 to 09:00					Volume	33	14			47	Peak Hour	8:00 8:00				8:00	Peak Volume	22	9			31	Peak Hour Factor	0.550	0.450			0.554	Peak Period	16:00 to 18:00					Volume	30	42			72	Peak Hour	16:30 17:00				16:45	Peak Volume	18	26			40	Peak Hour Factor	0.750	0.929			0.833
STATISTICS																																																																																																																																																			
	NB	SB	EB	WB	TOTAL																																																																																																																																														
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Volume	83	50			133																																																																																																																																														
Peak Hour	8:15 8:30				8:30																																																																																																																																														
Peak Volume	24	15			39																																																																																																																																														
Peak Hour Factor	0.600	0.750			0.696																																																																																																																																														
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Volume	111	176			287																																																																																																																																														
Peak Hour	16:30 13:45				15:15																																																																																																																																														
Peak Volume	18	27			43																																																																																																																																														
Peak Hour Factor	0.750	0.750			0.717																																																																																																																																														
Peak Period	07:00 to 09:00																																																																																																																																																		
Volume	33	14			47																																																																																																																																														
Peak Hour	8:00 8:00				8:00																																																																																																																																														
Peak Volume	22	9			31																																																																																																																																														
Peak Hour Factor	0.550	0.450			0.554																																																																																																																																														
Peak Period	16:00 to 18:00																																																																																																																																																		
Volume	30	42			72																																																																																																																																														
Peak Hour	16:30 17:00				16:45																																																																																																																																														
Peak Volume	18	26			40																																																																																																																																														
Peak Hour Factor	0.750	0.929			0.833																																																																																																																																														
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TOTALS	83	50	0	0	133	TOTALS	111	176	0	0	287																																																																																																																																								
SPLIT %	62%	38%	0%	0%	32%	SPLIT %	39%	61%	0%	0%	68%																																																																																																																																								



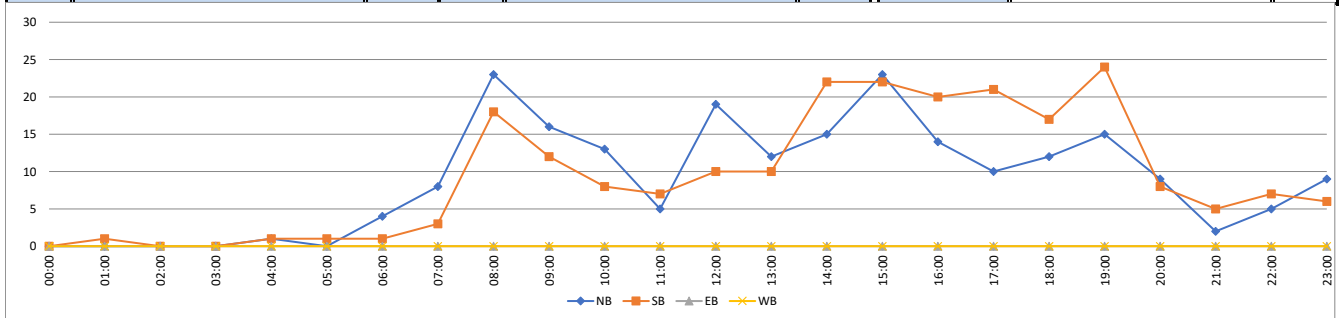
VOLUME

Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_005

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS																																																																																																																																															
					215	224	0	0	439																																																																																																																																																
15-Minutes Interval											Hourly Intervals																																																																																																																																														
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																								
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06:00	1	0			1	18:00	4	7			11	<table border="1"> <thead> <tr> <th colspan="6">STATISTICS</th> </tr> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="5">00:00 to 12:00</td> </tr> <tr> <td>Volume</td><td>70</td><td>52</td><td></td><td></td><td>122</td> </tr> <tr> <td>Peak Hour</td><td colspan="5">8:00 8:15</td> </tr> <tr> <td>Peak Volume</td><td>23</td><td>25</td><td></td><td></td><td>43</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.821</td><td>0.625</td><td></td><td></td><td>0.717</td> </tr> <tr> <td>Peak Period</td><td colspan="5">12:00 to 00:00</td> </tr> <tr> <td>Volume</td><td>145</td><td>172</td><td></td><td></td><td>317</td> </tr> <tr> <td>Peak Hour</td><td colspan="5">14:45 15:15</td> </tr> <tr> <td>Peak Volume</td><td>23</td><td>26</td><td></td><td></td><td>45</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.821</td><td>0.929</td><td></td><td></td><td>0.865</td> </tr> <tr> <td>Peak Period</td><td colspan="5">07:00 to 09:00</td> </tr> <tr> <td>Volume</td><td>31</td><td>21</td><td></td><td></td><td>52</td> </tr> <tr> <td>Peak Hour</td><td colspan="5">8:00 8:00</td> </tr> <tr> <td>Peak Volume</td><td>23</td><td>18</td><td></td><td></td><td>41</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.821</td><td>0.450</td><td></td><td></td><td>0.683</td> </tr> <tr> <td>Peak Period</td><td colspan="5">16:00 to 18:00</td> </tr> <tr> <td>Volume</td><td>24</td><td>41</td><td></td><td></td><td>65</td> </tr> <tr> <td>Peak Hour</td><td colspan="5">16:15 16:15</td> </tr> <tr> <td>Peak Volume</td><td>15</td><td>21</td><td></td><td></td><td>36</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.750</td><td>0.656</td><td></td><td></td><td>0.818</td> </tr> </tbody> </table>					STATISTICS							NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume	70	52			122	Peak Hour	8:00 8:15					Peak Volume	23	25			43	Peak Hour Factor	0.821	0.625			0.717	Peak Period	12:00 to 00:00					Volume	145	172			317	Peak Hour	14:45 15:15					Peak Volume	23	26			45	Peak Hour Factor	0.821	0.929			0.865	Peak Period	07:00 to 09:00					Volume	31	21			52	Peak Hour	8:00 8:00					Peak Volume	23	18			41	Peak Hour Factor	0.821	0.450			0.683	Peak Period	16:00 to 18:00					Volume	24	41			65	Peak Hour	16:15 16:15					Peak Volume	15	21			36	Peak Hour Factor	0.750	0.656			0.818					
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TOTALS	70	52	0	0	122	TOTALS	145	172	0	0	317																																																																																																																																														
SPLIT %	57%	43%	0%	0%	28%	SPLIT %	46%	54%	0%	0%	72%																																																																																																																																														



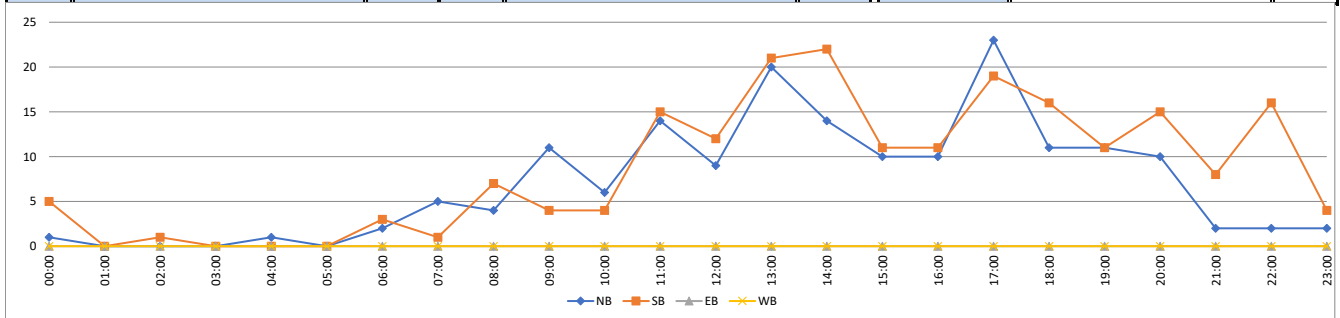
VOLUME

Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_005

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS						
						168	206	0	0	374							
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	1			1	12:00	2	3			5	00:00	01:00	1	5		6
00:15	0	2			2	12:15	3	2			5	01:00	02:00	0	0		0
00:30	1	2			3	12:30	2	4			6	02:00	03:00	0	1		1
00:45	0	0			0	12:45	2	3			5	03:00	04:00	0	0		0
01:00	0	0			0	13:00	4	6			10	04:00	05:00	1	0		1
01:15	0	0			0	13:15	8	3			11	05:00	06:00	0	0		0
01:30	0	0			0	13:30	6	3			9	06:00	07:00	2	3		5
01:45	0	0			0	13:45	2	9			11	07:00	08:00	5	1		6
02:00	0	0			0	14:00	6	5			11	08:00	09:00	4	7		11
02:15	0	0			0	14:15	4	5			9	09:00	10:00	11	4		15
02:30	0	0			0	14:30	2	7			9	10:00	11:00	6	4		10
02:45	0	1			1	14:45	2	5			7	11:00	12:00	14	15		29
03:00	0	0			0	15:00	2	0			2	12:00	13:00	9	12		21
03:15	0	0			0	15:15	5	3			8	13:00	14:00	20	21		41
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04:00	0	0			0	16:00	2	2			4	16:00	17:00	10	11		21
04:15	0	0			0	16:15	3	4			7	17:00	18:00	23	19		42
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05:45	0	0			0	17:45	4	5			9	23:00	00:00	2	4		6
06:00	1	0			1	18:00	2	4			6	STATISTICS					
06:15	0	1			1	18:15	4	2		6							
06:30	0	0			0	18:30	3	1		4	Peak Period	00:00 to 12:00					
06:45	1	2			3	18:45	2	9		11	Volume	44 40				84	
07:00	1	0			1	19:00	2	1		3	Peak Hour	11:00 11:00				11:00	
07:15	2	0			2	19:15	4	2		6	Peak Volume	14 15				29	
07:30	1	0			1	19:30	3	2		5	Peak Hour Factor	0.583 0.750				0.659	
07:45	1	1			2	19:45	2	6		8	Peak Period	12:00 to 00:00					
08:00	0	1			1	20:00	1	3		4	Volume	124 166				290	
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08:30	2	2			4	20:30	6	3		9	Peak Volume	23 26				42	
08:45	1	4			5	20:45	0	3		3	Peak Hour Factor	0.639 0.722				0.955	
09:00	3	3			6	21:00	0	1		1	Peak Period	07:00 to 09:00					
09:15	2	1			3	21:15	1	4		5	Volume	9 8				17	
09:30	5	0			5	21:30	1	1		2	Peak Hour	7:00 8:00				8:00	
09:45	1	0			1	21:45	0	2		2	Peak Volume	5 7				11	
10:00	3	1			4	22:00	0	7		7	Peak Hour Factor	0.625 0.438				0.550	
10:15	1	1			2	22:15	1	4		5	Peak Period	16:00 to 18:00					
10:30	1	2			3	22:30	1	3		4	Volume	33 30				63	
10:45	1	0			1	22:45	0	2		2	Peak Hour	16:45 17:00				17:00	
11:00	3	2			5	23:00	1	2		3	Peak Volume	23 19				42	
11:15	3	4			7	23:15	1	1		2	Peak Hour Factor	0.639 0.792				0.875	
11:30	6	5			11	23:30	0	1		1							
11:45	2	4			6	23:45	0	0		0							
TOTALS	44	40	0	0	84	TOTALS	124	166	0	0	290						
SPLIT %	52%	48%	0%	0%	22%	SPLIT %	43%	57%	0%	0%	78%						



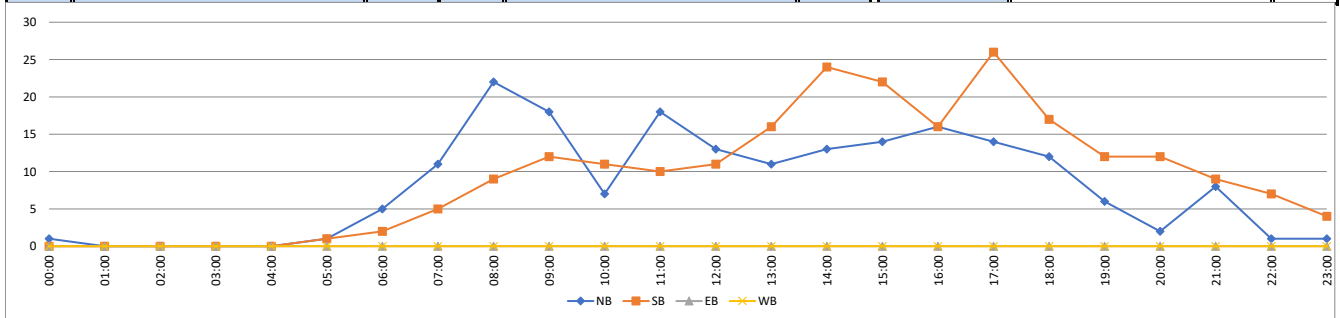
VOLUME

Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_005

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS																																																																																																																																										
					194	226	0	0	420																																																																																																																																											
15-Minutes Interval											Hourly Intervals																																																																																																																																									
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																			
00:00	0	0			0	12:00	6	2			8	00:00	01:00	1	0			1																																																																																																																																		
00:15	1	0			1	12:15	0	2			2	01:00	02:00	0	0			0																																																																																																																																		
00:30	0	0			0	12:30	3	2			5	02:00	03:00	0	0			0																																																																																																																																		
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01:30	0	0			0	13:30	4	5			9	06:00	07:00	5	2			7																																																																																																																																		
01:45	0	0			0	13:45	1	6			7	07:00	08:00	11	5			16																																																																																																																																		
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06:00	1	2			3	18:00	6	3			9	<table border="1"> <thead> <tr> <th colspan="6">STATISTICS</th> </tr> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="4">00:00 to 12:00</td><td></td> </tr> <tr> <td>Volume</td><td>83</td><td>50</td><td></td><td></td><td>133</td> </tr> <tr> <td>Peak Hour</td><td colspan="4">8:15 8:30</td><td>8:30</td> </tr> <tr> <td>Peak Volume</td><td>24</td><td>15</td><td></td><td></td><td>39</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.600</td><td>0.750</td><td></td><td></td><td>0.696</td> </tr> <tr> <td>Peak Period</td><td colspan="4">12:00 to 00:00</td><td></td> </tr> <tr> <td>Volume</td><td>111</td><td>176</td><td></td><td></td><td>287</td> </tr> <tr> <td>Peak Hour</td><td colspan="4">16:30 13:45</td><td>15:15</td> </tr> <tr> <td>Peak Volume</td><td>18</td><td>27</td><td></td><td></td><td>43</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.750</td><td>0.750</td><td></td><td></td><td>0.717</td> </tr> <tr> <td>Peak Period</td><td colspan="4">07:00 to 09:00</td><td></td> </tr> <tr> <td>Volume</td><td>33</td><td>14</td><td></td><td></td><td>47</td> </tr> <tr> <td>Peak Hour</td><td colspan="4">8:00 8:00</td><td>8:00</td> </tr> <tr> <td>Peak Volume</td><td>22</td><td>9</td><td></td><td></td><td>31</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.550</td><td>0.450</td><td></td><td></td><td>0.554</td> </tr> <tr> <td>Peak Period</td><td colspan="4">16:00 to 18:00</td><td></td> </tr> <tr> <td>Volume</td><td>30</td><td>42</td><td></td><td></td><td>72</td> </tr> <tr> <td>Peak Hour</td><td colspan="4">16:30 17:00</td><td>16:45</td> </tr> <tr> <td>Peak Volume</td><td>18</td><td>26</td><td></td><td></td><td>40</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.750</td><td>0.929</td><td></td><td></td><td>0.833</td> </tr> </tbody> </table>					STATISTICS							NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume	83	50			133	Peak Hour	8:15 8:30				8:30	Peak Volume	24	15			39	Peak Hour Factor	0.600	0.750			0.696	Peak Period	12:00 to 00:00					Volume	111	176			287	Peak Hour	16:30 13:45				15:15	Peak Volume	18	27			43	Peak Hour Factor	0.750	0.750			0.717	Peak Period	07:00 to 09:00					Volume	33	14			47	Peak Hour	8:00 8:00				8:00	Peak Volume	22	9			31	Peak Hour Factor	0.550	0.450			0.554	Peak Period	16:00 to 18:00					Volume	30	42			72	Peak Hour	16:30 17:00				16:45	Peak Volume	18	26			40	Peak Hour Factor	0.750	0.929			0.833
STATISTICS																																																																																																																																																				
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Peak Hour Factor	0.750	0.929			0.833																																																																																																																																															
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TOTALS	83	50	0	0	133	TOTALS	111	176	0	0	287																																																																																																																																									
SPLIT %	62%	38%	0%	0%	32%	SPLIT %	39%	61%	0%	0%	68%																																																																																																																																									



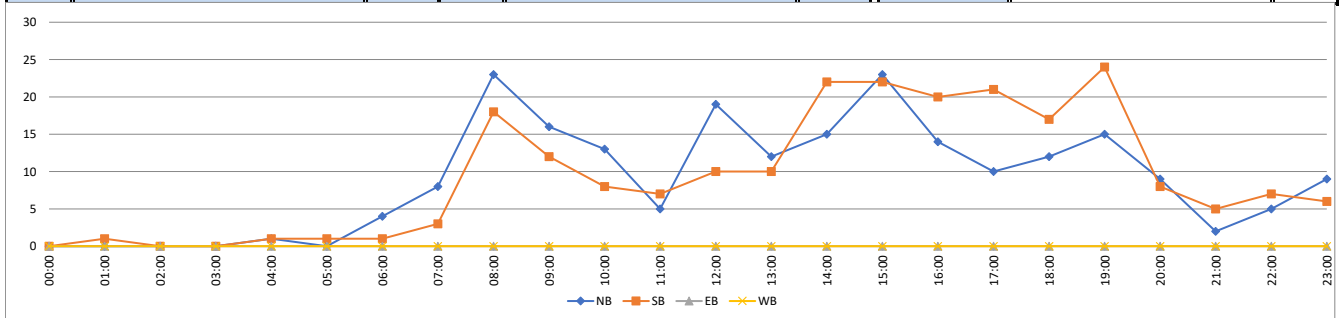
VOLUME

Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_005

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					215	224	0	0	439								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	0			0	12:00	5	1			6	00:00	0	0			0
00:15	0	0			0	12:15	5	1			6	01:00	0	1			1
00:30	0	0			0	12:30	3	6			9	02:00	0	0			0
00:45	0	0			0	12:45	6	2			8	03:00	0	0			0
01:00	0	0			0	13:00	4	1			5	04:00	1	1			2
01:15	0	0			0	13:15	0	1			1	05:00	0	1			1
01:30	0	0			0	13:30	5	4			9	06:00	4	1			5
01:45	0	1			1	13:45	3	4			7	07:00	8	3			11
02:00	0	0			0	14:00	3	4			7	08:00	23	18			41
02:15	0	0			0	14:15	5	7			12	09:00	16	12			28
02:30	0	0			0	14:30	4	5			9	10:00	13	8			21
02:45	0	0			0	14:45	3	6			9	11:00	5	7			12
03:00	0	0			0	15:00	7	3			10	12:00	19	10			29
03:15	0	0			0	15:15	7	5			12	13:00	12	10			22
03:30	0	0			0	15:30	6	7			13	14:00	15	22			37
03:45	0	0			0	15:45	3	7			10	15:00	23	22			45
04:00	0	0			0	16:00	2	7			9	16:00	14	20			34
04:15	1	1			2	16:15	3	3			6	17:00	10	21			31
04:30	0	0			0	16:30	5	5			10	18:00	12	17			29
04:45	0	0			0	16:45	4	5			9	19:00	15	24			39
05:00	0	0			0	17:00	3	8			11	20:00	9	8			17
05:15	0	0			0	17:15	2	2			4	21:00	2	5			7
05:30	0	0			0	17:30	3	5			8	22:00	5	7			12
05:45	0	1			1	17:45	2	6			8	23:00	9	6			15
06:00	1	0			1	18:00	4	7			11	STATISTICS					
06:15	2	1			3	18:15	2	2			4		NB	SB	EB	WB	TOTAL
06:30	0	0			0	18:30	2	2			4	Peak Period	00:00 to 12:00				
06:45	1	0			1	18:45	4	6			10	Volume	70	52			122
07:00	3	1			4	19:00	2	6			8	Peak Hour	8:00	8:15			8:15
07:15	0	0			0	19:15	6	2			8	Peak Volume	23	25			43
07:30	3	2			5	19:30	3	8			11	Peak Hour Factor	0.821	0.625			0.717
07:45	2	0			2	19:45	4	8			12	Peak Period	12:00 to 00:00				
08:00	7	2			9	20:00	3	3			6	Volume	145	172			317
08:15	5	3			8	20:15	3	0			3	Peak Hour	14:45	15:15			15:00
08:30	6	3			9	20:30	2	4			6	Peak Volume	23	26			45
08:45	5	10			15	20:45	1	1			2	Peak Hour Factor	0.821	0.929			0.865
09:00	2	9			11	21:00	1	1			2	Peak Period	07:00 to 09:00				
09:15	5	0			5	21:15	1	1			2	Volume	31	21			52
09:30	3	2			5	21:30	0	1			1	Peak Hour	8:00	8:00			8:00
09:45	6	1			7	21:45	0	2			2	Peak Volume	23	18			41
10:00	4	2			6	22:00	3	2			5	Peak Hour Factor	0.821	0.450			0.683
10:15	5	2			7	22:15	1	4			5	Peak Period	16:00 to 18:00				
10:30	3	1			4	22:30	0	1			1	Volume	24	41			65
10:45	1	3			4	22:45	1	0			1	Peak Hour	16:15	16:15			16:15
11:00	0	0			0	23:00	2	3			5	Peak Volume	15	21			36
11:15	2	1			3	23:15	2	2			4	Peak Hour Factor	0.750	0.656			0.818
11:30	1	3			4	23:30	3	0			3						
11:45	2	3			5	23:45	2	1			3						
TOTALS	70	52	0	0	122	TOTALS	145	172	0	0	317						
SPLIT %	57%	43%	0%	0%	28%	SPLIT %	46%	54%	0%	0%	72%						



SPEED

Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Saturday

City: Greenbelt

Date: 11/18/2023

Project #: MD23_280047_005

Time	NORTHBOUND														Total	SOUTHBOUND														Total	TOTALS														Total				
	5	15	20	25	30	35	40	45	50	55	60	65	70	99		5	15	20	25	30	35	40	45	50	55	60	65	70	99		5	15	20	25	30	35	40	45	50	55	60	65	70	99					
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99								
12:00	0	1	0	1	0	0	0	0	0	0	0	0	0	2	0	0	0	3	0	0	0	0	0	0	0	0	0	3	0	1	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5		
12:15	0	1	0	2	0	0	0	0	0	0	0	0	0	3	0	1	1	0	0	0	0	0	0	0	0	0	0	2	0	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	
12:30	0	0	0	1	1	0	0	0	0	0	0	0	0	2	0	1	2	1	0	0	0	0	0	0	0	0	0	4	0	1	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
12:45	0	1	1	0	0	0	0	0	0	0	0	0	0	2	0	1	2	0	0	0	0	0	0	0	0	0	0	3	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5		
13:00	0	0	2	2	0	0	0	0	0	0	0	0	0	4	0	2	4	0	0	0	0	0	0	0	0	0	0	6	0	2	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10		
13:15	0	1	6	1	0	0	0	0	0	0	0	0	0	8	0	2	1	0	0	0	0	0	0	0	0	0	3	0	3	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11		
13:30	0	0	0	5	1	0	0	0	0	0	0	0	0	6	0	1	2	0	0	0	0	0	0	0	0	0	3	0	1	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
13:45	0	0	1	1	0	0	0	0	0	0	0	0	0	2	2	5	0	2	0	0	0	0	0	0	0	0	9	2	5	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	
14:00	1	1	3	1	0	0	0	0	0	0	0	0	0	6	0	3	2	0	0	0	0	0	0	0	0	0	5	1	4	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	
14:15	0	1	1	1	1	0	0	0	0	0	0	0	0	4	0	4	1	0	0	0	0	0	0	0	0	0	5	0	5	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	
14:30	0	1	1	0	0	0	0	0	0	0	0	0	0	2	0	3	4	0	0	0	0	0	0	0	0	0	7	0	4	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	
14:45	0	1	1	0	0	0	0	0	0	0	0	0	0	2	1	2	1	1	0	0	0	0	0	0	0	0	5	1	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
15:00	1	1	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
15:15	2	0	2	1	0	0	0	0	0	0	0	0	0	5	1	1	0	1	0	0	0	0	0	0	0	0	3	3	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	
15:30	0	0	1	1	0	0	0	0	0	0	0	0	0	2	1	2	1	0	0	0	0	0	0	0	0	0	4	1	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
15:45	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	3	1	0	0	0	0	0	0	0	0	4	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	
16:00	0	0	1	0	0	1	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	2	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
16:15	0	0	2	0	0	1	0	0	0	0	0	0	0	3	0	2	1	1	0	0	0	0	0	0	0	4	0	2	3	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	
16:30	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	0	2	0	0	0	0	0	0	0	3	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
16:45	1	0	3	0	0	0	0	0	0	0	0	0	0	4	0	2	0	0	0	0	0	0	0	0	0	0	2	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
17:00	0	0	5	1	0	0	0	0	0	0	0	0	0	6	0	0	4	1	0	0	0	0	0	0	0	0	5	0	0	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	
17:15	0	2	1	1	0	0	0	0	0	0	0	0	0	4	1	1	1	3	0	0	0	0	0	0	0	6	1	3	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	
17:30	0	1	4	4	0	0	0	0	0	0	0	0	0	9	0	1	2	0	0	0	0	0	0	0	0	3	0	2	6	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	
17:45	0	1	2	1	0	0	0	0	0	0	0	0	0	4	1	2	1	1	0	0	0	0	0	0	0	5	1	3	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	
18:00	0	1	1	0	0	0	0	0	0	0	0	0	0	2	0	3	1	0	0	0	0	0	0	0	0	4	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
18:15	0	3	1	0	0	0	0	0	0	0	0	0	0	4	0	1	1	0	0	0	0	0	0	0	0	2	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
18:30	0	0	2	1	0	0	0	0	0	0	0	0	0	3	0	0	1	0	0	0	0	0	0	0	0	1	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
18:45	0	1	0	1	0	0	0	0	0	0	0	0	0	2	1	4	4	0	0	0	0	0	0	0	0	9	1	5	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11
19:00	0	0	2	0	0	0	0	0	0	0	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	1	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
19:15	0	1	3	0	0	0	0	0	0	0	0	0	0	4	0	1	1	0	0	0	0	0	0	0	0	2	0	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	
19:30	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	0	1	1	0	0	0	0	0	0	0	2	0	3	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	
19:45	0	1	0	1	0	0	0	0	0	0	0	0	0	2	1	1	2	2	0	0	0	0	0	0	0	6	1	2	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8
20:00	0	0	1	0	0	0	0	0	0	0	0	0	0	1	1	1	1	0	0	0	0	0	0	0	0	3	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	
20:15	0	1	2	0	0	0	0	0	0	0	0	0	0	3	1	1	4	0	0	0	0	0	0	0	0	6	1	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9
20:30	0	0	2	4	0	0	0	0	0	0	0	0	0	6	0	0	2	1	0	0	0	0	0	0	0	3	0	0	4	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	
20:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	2	0	0	0	0	0	0	0	3	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3		
21:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
21:15	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	3	0	1	0	0	0	0																										

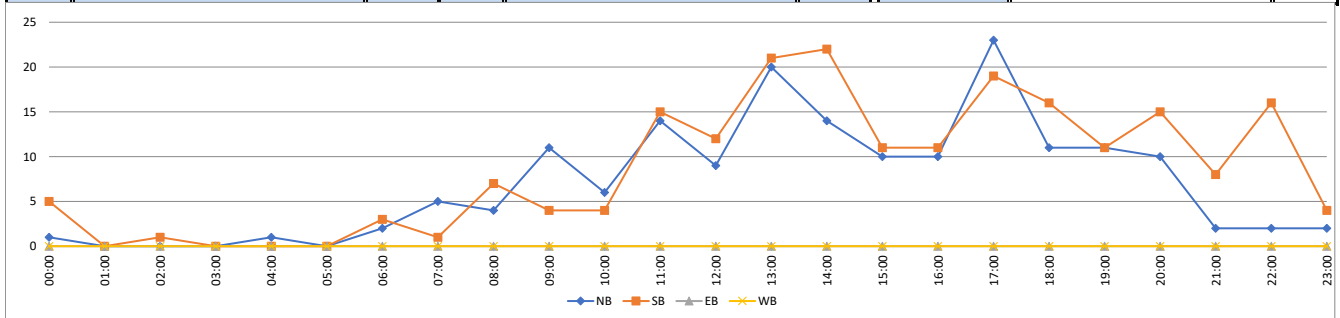
VOLUME

Lakeside Dr Bet. Olivewood Ct & Pinecrest Ct

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_005

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS						
						168	206	0	0	374							
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	1			1	12:00	2	3			5	00:00	01:00	1	5		6
00:15	0	2			2	12:15	3	2			5	01:00	02:00	0	0		0
00:30	1	2			3	12:30	2	4			6	02:00	03:00	0	1		1
00:45	0	0			0	12:45	2	3			5	03:00	04:00	0	0		0
01:00	0	0			0	13:00	4	6			10	04:00	05:00	1	0		1
01:15	0	0			0	13:15	8	3			11	05:00	06:00	0	0		0
01:30	0	0			0	13:30	6	3			9	06:00	07:00	2	3		5
01:45	0	0			0	13:45	2	9			11	07:00	08:00	5	1		6
02:00	0	0			0	14:00	6	5			11	08:00	09:00	4	7		11
02:15	0	0			0	14:15	4	5			9	09:00	10:00	11	4		15
02:30	0	0			0	14:30	2	7			9	10:00	11:00	6	4		10
02:45	0	1			1	14:45	2	5			7	11:00	12:00	14	15		29
03:00	0	0			0	15:00	2	0			2	12:00	13:00	9	12		21
03:15	0	0			0	15:15	5	3			8	13:00	14:00	20	21		41
03:30	0	0			0	15:30	2	4			6	14:00	15:00	14	22		36
03:45	0	0			0	15:45	1	4			5	15:00	16:00	10	11		21
04:00	0	0			0	16:00	2	2			4	16:00	17:00	10	11		21
04:15	0	0			0	16:15	3	4			7	17:00	18:00	23	19		42
04:30	1	0			1	16:30	1	3			4	18:00	19:00	11	16		27
04:45	0	0			0	16:45	4	2			6	19:00	20:00	11	11		22
05:00	0	0			0	17:00	6	5			11	20:00	21:00	10	15		25
05:15	0	0			0	17:15	4	6			10	21:00	22:00	2	8		10
05:30	0	0			0	17:30	9	3			12	22:00	23:00	2	16		18
05:45	0	0			0	17:45	4	5			9	23:00	00:00	2	4		6
06:00	1	0			1	18:00	2	4			6	STATISTICS					
06:15	0	1			1	18:15	4	2		6							
06:30	0	0			0	18:30	3	1		4	Peak Period	00:00 to 12:00					
06:45	1	2			3	18:45	2	9		11	Volume	44 40				84	
07:00	1	0			1	19:00	2	1		3	Peak Hour	11:00 11:00				11:00	
07:15	2	0			2	19:15	4	2		6	Peak Volume	14 15				29	
07:30	1	0			1	19:30	3	2		5	Peak Hour Factor	0.583 0.750				0.659	
07:45	1	1			2	19:45	2	6		8	Peak Period	12:00 to 00:00					
08:00	0	1			1	20:00	1	3		4	Volume	124 166				290	
08:15	1	0			1	20:15	3	6		9	Peak Hour	16:45 13:45				13:15	
08:30	2	2			4	20:30	6	3		9	Peak Volume	23 26				42	
08:45	1	4			5	20:45	0	3		3	Peak Hour Factor	0.639 0.722				0.955	
09:00	3	3			6	21:00	0	1		1	Peak Period	07:00 to 09:00					
09:15	2	1			3	21:15	1	4		5	Volume	9 8				17	
09:30	5	0			5	21:30	1	1		2	Peak Hour	7:00 8:00				8:00	
09:45	1	0			1	21:45	0	2		2	Peak Volume	5 7				11	
10:00	3	1			4	22:00	0	7		7	Peak Hour Factor	0.625 0.438				0.550	
10:15	1	1			2	22:15	1	4		5	Peak Period	16:00 to 18:00					
10:30	1	2			3	22:30	1	3		4	Volume	33 30				63	
10:45	1	0			1	22:45	0	2		2	Peak Hour	16:45 17:00				17:00	
11:00	3	2			5	23:00	1	2		3	Peak Volume	23 19				42	
11:15	3	4			7	23:15	1	1		2	Peak Hour Factor	0.639 0.792				0.875	
11:30	6	5			11	23:30	0	1		1							
11:45	2	4			6	23:45	0	0		0							
TOTALS	44	40	0	0	84	TOTALS	124	166	0	0	290						
SPLIT %	52%	48%	0%	0%	22%	SPLIT %	43%	57%	0%	0%	78%						



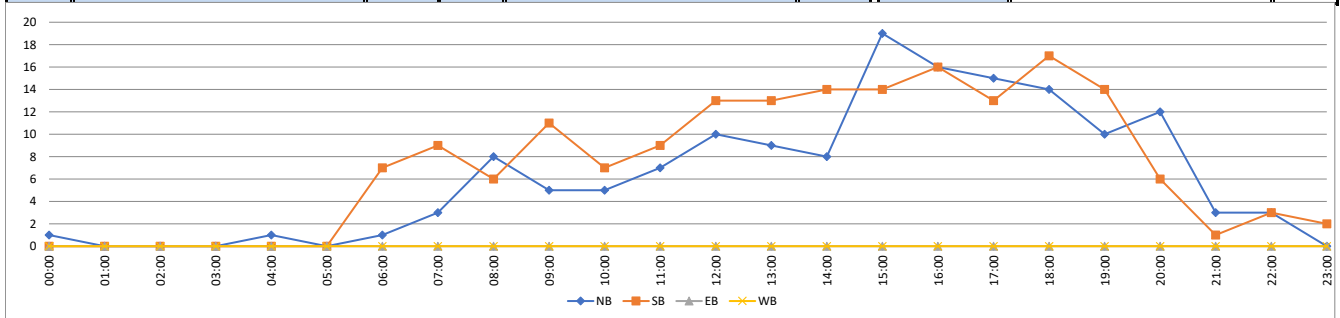
VOLUME

Lakeside Dr N/O Crescent Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_006

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					150	175	0	0	325									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	0	0			0	12:00	3	2			5	00:00	01:00	1	0			1
00:15	1	0			1	12:15	2	3			5	01:00	02:00	0	0			0
00:30	0	0			0	12:30	3	4			7	02:00	03:00	0	0			0
00:45	0	0			0	12:45	2	4			6	03:00	04:00	0	0			0
01:00	0	0			0	13:00	3	5			8	04:00	05:00	1	0			1
01:15	0	0			0	13:15	2	1			3	05:00	06:00	0	0			0
01:30	0	0			0	13:30	3	5			8	06:00	07:00	1	7			8
01:45	0	0			0	13:45	1	2			3	07:00	08:00	3	9			12
02:00	0	0			0	14:00	1	4			5	08:00	09:00	8	6			14
02:15	0	0			0	14:15	3	3			6	09:00	10:00	5	11			16
02:30	0	0			0	14:30	4	4			8	10:00	11:00	5	7			12
02:45	0	0			0	14:45	0	3			3	11:00	12:00	7	9			16
03:00	0	0			0	15:00	5	1			6	12:00	13:00	10	13			23
03:15	0	0			0	15:15	4	5			9	13:00	14:00	9	13			22
03:30	0	0			0	15:30	5	3			8	14:00	15:00	8	14			22
03:45	0	0			0	15:45	5	5			10	15:00	16:00	19	14			33
04:00	0	0			0	16:00	2	5			7	16:00	17:00	16	16			32
04:15	1	0			1	16:15	2	4			6	17:00	18:00	15	13			28
04:30	0	0			0	16:30	6	2			8	18:00	19:00	14	17			31
04:45	0	0			0	16:45	6	5			11	19:00	20:00	10	14			24
05:00	0	0			0	17:00	4	4			8	20:00	21:00	12	6			18
05:15	0	0			0	17:15	2	2			4	21:00	22:00	3	1			4
05:30	0	0			0	17:30	4	6			10	22:00	23:00	3	3			6
05:45	0	0			0	17:45	5	1			6	23:00	00:00	0	2			2
06:00	0	2			2	18:00	4	4			8	STATISTICS						
06:15	0	1			1	18:15	5	3			8						NB	SB
06:30	1	0			1	18:30	1	6			7	Peak Period	00:00	to	12:00			
06:45	0	4			4	18:45	4	4			8	Volume	31	49		80		
07:00	0	2			2	19:00	2	4			6	Peak Hour	8:30	8:45		8:30		
07:15	2	2			4	19:15	2	5			7	Peak Volume	10	12		21		
07:30	1	3			4	19:30	3	1			4	Peak Hour Factor	0.625	0.750		0.875		
07:45	0	2			2	19:45	3	4			7	Peak Period	12:00	to	00:00			
08:00	0	2			2	20:00	1	2			3	Volume	119	126		245		
08:15	1	0			1	20:15	2	1			3	Peak Hour	15:00	18:30		15:15		
08:30	4	2			6	20:30	4	2			6	Peak Volume	19	19		34		
08:45	3	2			5	20:45	5	1			6	Peak Hour Factor	0.950	0.792		0.850		
09:00	1	4			5	21:00	2	0			2	Peak Period	07:00	to	09:00			
09:15	2	3			5	21:15	1	0			1	Volume	11	15		26		
09:30	1	3			4	21:30	0	1			1	Peak Hour	8:00	7:00		8:00		
09:45	1	1			2	21:45	0	0			0	Peak Volume	8	9		14		
10:00	2	1			3	22:00	2	1			3	Peak Hour Factor	0.500	0.750		0.583		
10:15	0	0			0	22:15	1	1			2	Peak Period	16:00	to	18:00			
10:30	1	5			6	22:30	0	1			1	Volume	31	29		60		
10:45	2	1			3	22:45	0	0			0	Peak Hour	16:15	16:45		16:15		
11:00	1	3			4	23:00	0	1			1	Peak Volume	18	17		33		
11:15	3	0			3	23:15	0	1			1	Peak Hour Factor	0.750	0.708		0.750		
11:30	1	4			5	23:30	0	0			0							
11:45	2	2			4	23:45	0	0			0							
TOTALS	31	49	0	0	80	TOTALS	119	126	0	0	245							
SPLIT %	39%	61%	0%	0%	25%	SPLIT %	49%	51%	0%	0%	75%							



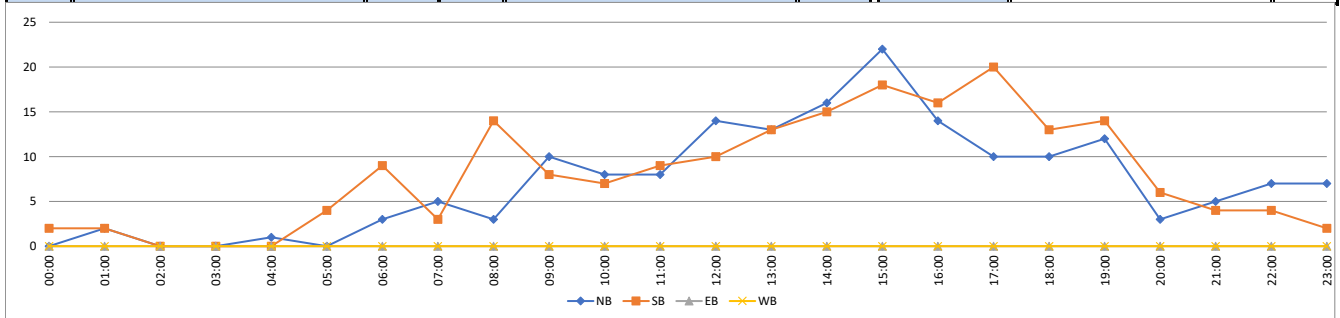
VOLUME

Lakeside Dr N/O Crescent Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_006

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					173	193	0	0	366									
15-Minutes Interval											Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	0	0			0	12:00	1	0			1	00:00	01:00	0	2			2
00:15	0	0			0	12:15	4	3			7	01:00	02:00	2	2			4
00:30	0	1			1	12:30	3	2			5	02:00	03:00	0	0			0
00:45	0	1			1	12:45	6	5			11	03:00	04:00	0	0			0
01:00	0	1			1	13:00	2	4			6	04:00	05:00	1	0			1
01:15	1	1			2	13:15	3	5			8	05:00	06:00	0	4			4
01:30	1	0			1	13:30	5	1			6	06:00	07:00	3	9			12
01:45	0	0			0	13:45	3	3			6	07:00	08:00	5	3			8
02:00	0	0			0	14:00	6	1			7	08:00	09:00	3	14			17
02:15	0	0			0	14:15	5	7			12	09:00	10:00	10	8			18
02:30	0	0			0	14:30	3	4			7	10:00	11:00	8	7			15
02:45	0	0			0	14:45	2	3			5	11:00	12:00	8	9			17
03:00	0	0			0	15:00	6	6			12	12:00	13:00	14	10			24
03:15	0	0			0	15:15	7	3			10	13:00	14:00	13	13			26
03:30	0	0			0	15:30	4	5			9	14:00	15:00	16	15			31
03:45	0	0			0	15:45	5	4			9	15:00	16:00	22	18			40
04:00	0	0			0	16:00	2	5			7	16:00	17:00	14	16			30
04:15	0	0			0	16:15	5	2			7	17:00	18:00	10	20			30
04:30	1	0			1	16:30	3	2			5	18:00	19:00	10	13			23
04:45	0	0			0	16:45	4	7			11	19:00	20:00	12	14			26
05:00	0	1			1	17:00	4	4			8	20:00	21:00	3	6			9
05:15	0	2			2	17:15	2	5			7	21:00	22:00	5	4			9
05:30	0	0			0	17:30	2	8			10	22:00	23:00	7	4			11
05:45	0	1			1	17:45	2	3			5	23:00	00:00	7	2			9
06:00	0	3			3	18:00	3	3			6	STATISTICS						
06:15	0	0			0	18:15	2	2			4						NB	SB
06:30	1	2			3	18:30	4	5			9	Peak Period	00:00 to 12:00					
06:45	2	4			6	18:45	1	3			4	Volume	40 58			98		
07:00	2	1			3	19:00	3	7			10	Peak Hour	8:30 8:00			8:30		
07:15	1	0			1	19:15	3	1			4	Peak Volume	10 14			20		
07:30	2	0			2	19:30	1	2			3	Peak Hour Factor	0.500 0.583			0.625		
07:45	0	2			2	19:45	5	4			9	Peak Period	12:00 to 00:00					
08:00	0	2			2	20:00	2	2			4	Volume	133 135			268		
08:15	1	6			7	20:15	0	0			0	Peak Hour	15:00 16:45			15:00		
08:30	2	2			4	20:30	0	2			2	Peak Volume	22 24			40		
08:45	0	4			4	20:45	1	2			3	Peak Hour Factor	0.786 0.750			0.833		
09:00	3	1			4	21:00	1	2			3	Peak Period	07:00 to 09:00					
09:15	5	3			8	21:15	1	0			1	Volume	8 17			25		
09:30	0	2			2	21:30	1	0			1	Peak Hour	7:00 8:00			8:00		
09:45	2	2			4	21:45	2	2			4	Peak Volume	5 14			17		
10:00	3	3			6	22:00	0	1			1	Peak Hour Factor	0.625 0.583			0.607		
10:15	2	3			5	22:15	3	0			3	Peak Period	16:00 to 18:00					
10:30	2	0			2	22:30	2	2			4	Volume	24 36			60		
10:45	1	1			2	22:45	2	1			3	Peak Hour	16:15 16:45			16:45		
11:00	1	1			2	23:00	0	1			1	Peak Volume	16 24			36		
11:15	2	0			2	23:15	1	1			2	Peak Hour Factor	0.800 0.750			0.818		
11:30	1	6			7	23:30	3	0			3							
11:45	4	2			6	23:45	3	0			3							
TOTALS	40	58	0	0	98	TOTALS	133	135	0	0	268							
SPLIT %	41%	59%	0%	0%	27%	SPLIT %	50%	50%	0%	0%	73%							



CLASSIFICATION
Lakeside Dr N/O Crescent Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_006

HOURLY BREAKDOWN table with columns for Time, NORTHBOUND, SOUTHBOUND, and TOTALS. Rows include hourly counts from 00:00 to 23:00 and a Totals row.

CLASSIFICATION DEFINITIONS section showing illustrations and labels for vehicle types: #1 Motorcycles, #2 Passenger Cars, #3 2-Axle, 4-Tire Single Unit, #4 Buses, #5 2-Axle, 6-Tire Single Units, #6 3-Axle Single Units, #7 >=4-Axle Single Units, #8 <=4-Axle Single Trailers, #9 5-Axle Single Trailers, #10 >=6-Axle Single Trailers, #11 <=5-Axle Multi-Trailers, #12 6-Axle Multi-Trailers, #13 >=7-Axle Multi-Trailers.

STATISTICS table showing peak hour and peak volume data for various time intervals (e.g., 00:00-12:00, 12:00-24:00, 07:00-09:00, 16:00-18:00) across all vehicle classifications.

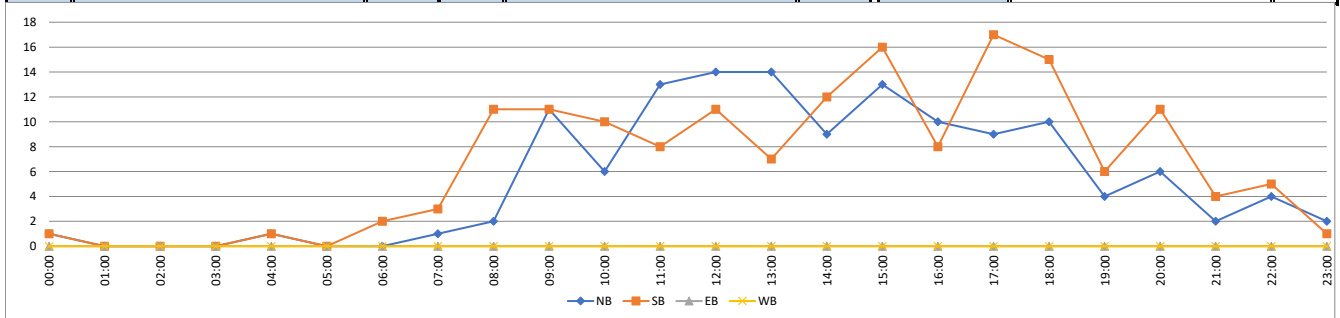
VOLUME

Lakeside Dr N/O Crescent Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_006

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					132	160	0	0	292									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	1	0			1	12:00	1	4			5	00:00	01:00	1	1			2
00:15	0	1			1	12:15	2	3			5	01:00	02:00	0	0			0
00:30	0	0			0	12:30	3	2			5	02:00	03:00	0	0			0
00:45	0	0			0	12:45	8	2			10	03:00	04:00	0	0			0
01:00	0	0			0	13:00	5	3			8	04:00	05:00	1	1			2
01:15	0	0			0	13:15	4	2			6	05:00	06:00	0	0			0
01:30	0	0			0	13:30	2	1			3	06:00	07:00	0	2			2
01:45	0	0			0	13:45	3	1			4	07:00	08:00	1	3			4
02:00	0	0			0	14:00	5	2			7	08:00	09:00	2	11			13
02:15	0	0			0	14:15	2	5			7	09:00	10:00	11	11			22
02:30	0	0			0	14:30	1	4			5	10:00	11:00	6	10			16
02:45	0	0			0	14:45	1	1			2	11:00	12:00	13	8			21
03:00	0	0			0	15:00	5	4			9	12:00	13:00	14	11			25
03:15	0	0			0	15:15	3	3			6	13:00	14:00	14	7			21
03:30	0	0			0	15:30	4	2			6	14:00	15:00	9	12			21
03:45	0	0			0	15:45	1	7			8	15:00	16:00	13	16			29
04:00	0	0			0	16:00	2	3			5	16:00	17:00	10	8			18
04:15	0	0			0	16:15	3	1			4	17:00	18:00	9	17			26
04:30	1	1			2	16:30	2	3			5	18:00	19:00	10	15			25
04:45	0	0			0	16:45	3	1			4	19:00	20:00	4	6			10
05:00	0	0			0	17:00	4	5			9	20:00	21:00	6	11			17
05:15	0	0			0	17:15	1	7			8	21:00	22:00	2	4			6
05:30	0	0			0	17:30	3	2			5	22:00	23:00	4	5			9
05:45	0	0			0	17:45	1	3			4	23:00	00:00	2	1			3
06:00	0	0			0	18:00	2	2			4	STATISTICS						
06:15	0	0			0	18:15	4	3			7		NB	SB	EB	WB	TOTAL	
06:30	0	1			1	18:30	1	6			7	Peak Period	00:00 to 12:00					
06:45	0	1			1	18:45	3	4			7	Volume	35	47			82	
07:00	0	1			1	19:00	2	2			4	Peak Hour	11:00 9:45				8:45	
07:15	0	1			1	19:15	0	3			3	Peak Volume	13	16			23	
07:30	0	0			0	19:30	1	0			1	Peak Hour Factor	0.813	0.571			0.639	
07:45	1	1			2	19:45	1	1			2	Peak Period	12:00 to 00:00					
08:00	0	0			0	20:00	2	2			4	Volume	97	113			210	
08:15	0	2			2	20:15	1	8			9	Peak Hour	12:30 17:00				12:30	
08:30	0	2			2	20:30	3	0			3	Peak Volume	20	17			29	
08:45	2	7			9	20:45	0	1			1	Peak Hour Factor	0.625	0.607			0.725	
09:00	3	3			6	21:00	2	1			3	Peak Period	07:00 to 09:00					
09:15	4	0			4	21:15	0	1			1	Volume	3	14			17	
09:30	3	1			4	21:30	0	2			2	Peak Hour	8:00 8:00				8:00	
09:45	1	7			8	21:45	0	0			0	Peak Volume	2	11			13	
10:00	0	4			4	22:00	1	2			3	Peak Hour Factor	0.250	0.393			0.361	
10:15	3	2			5	22:15	0	1			1	Peak Period	16:00 to 18:00					
10:30	1	3			4	22:30	2	1			3	Volume	19	25			44	
10:45	2	1			3	22:45	1	1			2	Peak Hour	16:15 17:00				16:30	
11:00	2	1			3	23:00	0	0			0	Peak Volume	12	17			26	
11:15	3	3			6	23:15	1	0			1	Peak Hour Factor	0.750	0.607			0.722	
11:30	4	4			8	23:30	1	1			2							
11:45	4	0			4	23:45	0	0			0							
TOTALS	35	47	0	0	82	TOTALS	97	113	0	0	210							
SPLIT %	43%	57%	0%	0%	28%	SPLIT %	46%	54%	0%	0%	72%							



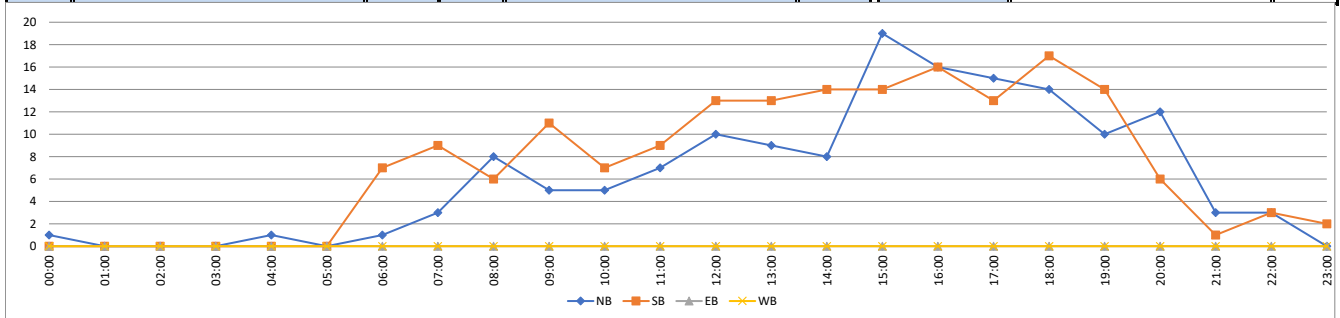
VOLUME

Lakeside Dr N/O Crescent Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_006

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					150	175	0	0	325									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	0	0			0	12:00	3	2			5	00:00	01:00	1	0			1
00:15	1	0			1	12:15	2	3			5	01:00	02:00	0	0			0
00:30	0	0			0	12:30	3	4			7	02:00	03:00	0	0			0
00:45	0	0			0	12:45	2	4			6	03:00	04:00	0	0			0
01:00	0	0			0	13:00	3	5			8	04:00	05:00	1	0			1
01:15	0	0			0	13:15	2	1			3	05:00	06:00	0	0			0
01:30	0	0			0	13:30	3	5			8	06:00	07:00	1	7			8
01:45	0	0			0	13:45	1	2			3	07:00	08:00	3	9			12
02:00	0	0			0	14:00	1	4			5	08:00	09:00	8	6			14
02:15	0	0			0	14:15	3	3			6	09:00	10:00	5	11			16
02:30	0	0			0	14:30	4	4			8	10:00	11:00	5	7			12
02:45	0	0			0	14:45	0	3			3	11:00	12:00	7	9			16
03:00	0	0			0	15:00	5	1			6	12:00	13:00	10	13			23
03:15	0	0			0	15:15	4	5			9	13:00	14:00	9	13			22
03:30	0	0			0	15:30	5	3			8	14:00	15:00	8	14			22
03:45	0	0			0	15:45	5	5			10	15:00	16:00	19	14			33
04:00	0	0			0	16:00	2	5			7	16:00	17:00	16	16			32
04:15	1	0			1	16:15	2	4			6	17:00	18:00	15	13			28
04:30	0	0			0	16:30	6	2			8	18:00	19:00	14	17			31
04:45	0	0			0	16:45	6	5			11	19:00	20:00	10	14			24
05:00	0	0			0	17:00	4	4			8	20:00	21:00	12	6			18
05:15	0	0			0	17:15	2	2			4	21:00	22:00	3	1			4
05:30	0	0			0	17:30	4	6			10	22:00	23:00	3	3			6
05:45	0	0			0	17:45	5	1			6	23:00	00:00	0	2			2
06:00	0	2			2	18:00	4	4			8	STATISTICS						
06:15	0	1			1	18:15	5	3			8						NB	SB
06:30	1	0			1	18:30	1	6			7	Peak Period	00:00	to	12:00			
06:45	0	4			4	18:45	4	4			8	Volume	31	49		80		
07:00	0	2			2	19:00	2	4			6	Peak Hour	8:30	8:45		8:30		
07:15	2	2			4	19:15	2	5			7	Peak Volume	10	12		21		
07:30	1	3			4	19:30	3	1			4	Peak Hour Factor	0.625	0.750		0.875		
07:45	0	2			2	19:45	3	4			7	Peak Period	12:00	to	00:00			
08:00	0	2			2	20:00	1	2			3	Volume	119	126		245		
08:15	1	0			1	20:15	2	1			3	Peak Hour	15:00	18:30		15:15		
08:30	4	2			6	20:30	4	2			6	Peak Volume	19	19		34		
08:45	3	2			5	20:45	5	1			6	Peak Hour Factor	0.950	0.792		0.850		
09:00	1	4			5	21:00	2	0			2	Peak Period	07:00	to	09:00			
09:15	2	3			5	21:15	1	0			1	Volume	11	15		26		
09:30	1	3			4	21:30	0	1			1	Peak Hour	8:00	7:00		8:00		
09:45	1	1			2	21:45	0	0			0	Peak Volume	8	9		14		
10:00	2	1			3	22:00	2	1			3	Peak Hour Factor	0.500	0.750		0.583		
10:15	0	0			0	22:15	1	1			2	Peak Period	16:00	to	18:00			
10:30	1	5			6	22:30	0	1			1	Volume	31	29		60		
10:45	2	1			3	22:45	0	0			0	Peak Hour	16:15	16:45		16:15		
11:00	1	3			4	23:00	0	1			1	Peak Volume	18	17		33		
11:15	3	0			3	23:15	0	1			1	Peak Hour Factor	0.750	0.708		0.750		
11:30	1	4			5	23:30	0	0			0							
11:45	2	2			4	23:45	0	0			0							
TOTALS	31	49	0	0	80	TOTALS	119	126	0	0	245							
SPLIT %	39%	61%	0%	0%	25%	SPLIT %	49%	51%	0%	0%	75%							



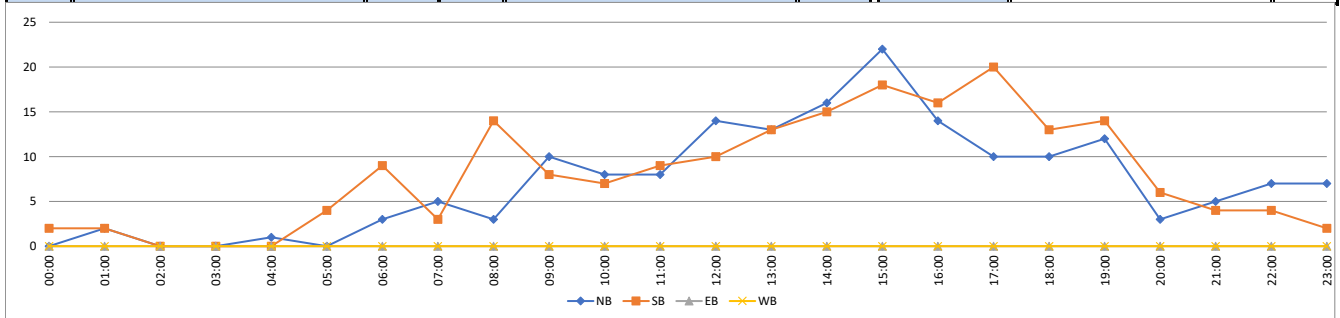
VOLUME

Lakeside Dr N/O Crescent Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_006

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS																																																																																																																																															
					173	193	0	0	366																																																																																																																																																
15-Minutes Interval											Hourly Intervals																																																																																																																																														
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL																																																																																																																																								
00:00	0	0			0	12:00	1	0			1	00:00	01:00	0	2		2																																																																																																																																								
00:15	0	0			0	12:15	4	3			7	01:00	02:00	2	2		4																																																																																																																																								
00:30	0	1			1	12:30	3	2			5	02:00	03:00	0	0		0																																																																																																																																								
00:45	0	1			1	12:45	6	5			11	03:00	04:00	0	0		0																																																																																																																																								
01:00	0	1			1	13:00	2	4			6	04:00	05:00	1	0		1																																																																																																																																								
01:15	1	1			2	13:15	3	5			8	05:00	06:00	0	4		4																																																																																																																																								
01:30	1	0			1	13:30	5	1			6	06:00	07:00	3	9		12																																																																																																																																								
01:45	0	0			0	13:45	3	3			6	07:00	08:00	5	3		8																																																																																																																																								
02:00	0	0			0	14:00	6	1			7	08:00	09:00	3	14		17																																																																																																																																								
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02:30	0	0			0	14:30	3	4			7	10:00	11:00	8	7		15																																																																																																																																								
02:45	0	0			0	14:45	2	3			5	11:00	12:00	8	9		17																																																																																																																																								
03:00	0	0			0	15:00	6	6			12	12:00	13:00	14	10		24																																																																																																																																								
03:15	0	0			0	15:15	7	3			10	13:00	14:00	13	13		26																																																																																																																																								
03:30	0	0			0	15:30	4	5			9	14:00	15:00	16	15		31																																																																																																																																								
03:45	0	0			0	15:45	5	4			9	15:00	16:00	22	18		40																																																																																																																																								
04:00	0	0			0	16:00	2	5			7	16:00	17:00	14	16		30																																																																																																																																								
04:15	0	0			0	16:15	5	2			7	17:00	18:00	10	20		30																																																																																																																																								
04:30	1	0			1	16:30	3	2			5	18:00	19:00	10	13		23																																																																																																																																								
04:45	0	0			0	16:45	4	7			11	19:00	20:00	12	14		26																																																																																																																																								
05:00	0	1			1	17:00	4	4			8	20:00	21:00	3	6		9																																																																																																																																								
05:15	0	2			2	17:15	2	5			7	21:00	22:00	5	4		9																																																																																																																																								
05:30	0	0			0	17:30	2	8			10	22:00	23:00	7	4		11																																																																																																																																								
05:45	0	1			1	17:45	2	3			5	23:00	00:00	7	2		9																																																																																																																																								
06:00	0	3			3	18:00	3	3			6	<table border="1"> <thead> <tr> <th colspan="6">STATISTICS</th> </tr> <tr> <th></th><th>NB</th><th>SB</th><th>EB</th><th>WB</th><th>TOTAL</th> </tr> </thead> <tbody> <tr> <td>Peak Period</td><td colspan="5">00:00 to 12:00</td> </tr> <tr> <td>Volume</td><td>40</td><td>58</td><td></td><td></td><td>98</td> </tr> <tr> <td>Peak Hour</td><td colspan="5">8:30 8:00</td> </tr> <tr> <td>Peak Volume</td><td>10</td><td>14</td><td></td><td></td><td>20</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.500</td><td>0.583</td><td></td><td></td><td>0.625</td> </tr> <tr> <td>Peak Period</td><td colspan="5">12:00 to 00:00</td> </tr> <tr> <td>Volume</td><td>133</td><td>135</td><td></td><td></td><td>268</td> </tr> <tr> <td>Peak Hour</td><td colspan="5">15:00 16:45</td> </tr> <tr> <td>Peak Volume</td><td>22</td><td>24</td><td></td><td></td><td>40</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.786</td><td>0.750</td><td></td><td></td><td>0.833</td> </tr> <tr> <td>Peak Period</td><td colspan="5">07:00 to 09:00</td> </tr> <tr> <td>Volume</td><td>8</td><td>17</td><td></td><td></td><td>25</td> </tr> <tr> <td>Peak Hour</td><td colspan="5">7:00 8:00</td> </tr> <tr> <td>Peak Volume</td><td>5</td><td>14</td><td></td><td></td><td>17</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.625</td><td>0.583</td><td></td><td></td><td>0.607</td> </tr> <tr> <td>Peak Period</td><td colspan="5">16:00 to 18:00</td> </tr> <tr> <td>Volume</td><td>24</td><td>36</td><td></td><td></td><td>60</td> </tr> <tr> <td>Peak Hour</td><td colspan="5">16:15 16:45</td> </tr> <tr> <td>Peak Volume</td><td>16</td><td>24</td><td></td><td></td><td>36</td> </tr> <tr> <td>Peak Hour Factor</td><td>0.800</td><td>0.750</td><td></td><td></td><td>0.818</td> </tr> </tbody> </table>					STATISTICS							NB	SB	EB	WB	TOTAL	Peak Period	00:00 to 12:00					Volume	40	58			98	Peak Hour	8:30 8:00					Peak Volume	10	14			20	Peak Hour Factor	0.500	0.583			0.625	Peak Period	12:00 to 00:00					Volume	133	135			268	Peak Hour	15:00 16:45					Peak Volume	22	24			40	Peak Hour Factor	0.786	0.750			0.833	Peak Period	07:00 to 09:00					Volume	8	17			25	Peak Hour	7:00 8:00					Peak Volume	5	14			17	Peak Hour Factor	0.625	0.583			0.607	Peak Period	16:00 to 18:00					Volume	24	36			60	Peak Hour	16:15 16:45					Peak Volume	16	24			36	Peak Hour Factor	0.800	0.750			0.818					
STATISTICS																																																																																																																																																									
	NB	SB	EB	WB	TOTAL																																																																																																																																																				
Peak Period	00:00 to 12:00																																																																																																																																																								
Volume	40	58			98																																																																																																																																																				
Peak Hour	8:30 8:00																																																																																																																																																								
Peak Volume	10	14			20																																																																																																																																																				
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Peak Period	12:00 to 00:00																																																																																																																																																								
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Peak Hour	15:00 16:45																																																																																																																																																								
Peak Volume	22	24			40																																																																																																																																																				
Peak Hour Factor	0.786	0.750			0.833																																																																																																																																																				
Peak Period	07:00 to 09:00																																																																																																																																																								
Volume	8	17			25																																																																																																																																																				
Peak Hour	7:00 8:00																																																																																																																																																								
Peak Volume	5	14			17																																																																																																																																																				
Peak Hour Factor	0.625	0.583			0.607																																																																																																																																																				
Peak Period	16:00 to 18:00																																																																																																																																																								
Volume	24	36			60																																																																																																																																																				
Peak Hour	16:15 16:45																																																																																																																																																								
Peak Volume	16	24			36																																																																																																																																																				
Peak Hour Factor	0.800	0.750			0.818																																																																																																																																																				
06:15	0	0			0	18:15	2	2			4																																																																																																																																														
06:30	1	2			3	18:30	4	5			9																																																																																																																																														
06:45	2	4			6	18:45	1	3			4																																																																																																																																														
07:00	2	1			3	19:00	3	7			10																																																																																																																																														
07:15	1	0			1	19:15	3	1			4																																																																																																																																														
07:30	2	0			2	19:30	1	2			3																																																																																																																																														
07:45	0	2			2	19:45	5	4			9																																																																																																																																														
08:00	0	2			2	20:00	2	2			4																																																																																																																																														
08:15	1	6			7	20:15	0	0			0																																																																																																																																														
08:30	2	2			4	20:30	0	2			2																																																																																																																																														
08:45	0	4			4	20:45	1	2			3																																																																																																																																														
09:00	3	1			4	21:00	1	2			3																																																																																																																																														
09:15	5	3			8	21:15	1	0			1																																																																																																																																														
09:30	0	2			2	21:30	1	0			1																																																																																																																																														
09:45	2	2			4	21:45	2	2			4																																																																																																																																														
10:00	3	3			6	22:00	0	1			1																																																																																																																																														
10:15	2	3			5	22:15	3	0			3																																																																																																																																														
10:30	2	0			2	22:30	2	2			4																																																																																																																																														
10:45	1	1			2	22:45	2	1			3																																																																																																																																														
11:00	1	1			2	23:00	0	1			1																																																																																																																																														
11:15	2	0			2	23:15	1	1			2																																																																																																																																														
11:30	1	6			7	23:30	3	0			3																																																																																																																																														
11:45	4	2			6	23:45	3	0			3																																																																																																																																														
TOTALS	40	58	0	0	98	TOTALS	133	135	0	0	268																																																																																																																																														
SPLIT %	41%	59%	0%	0%	27%	SPLIT %	50%	50%	0%	0%	73%																																																																																																																																														



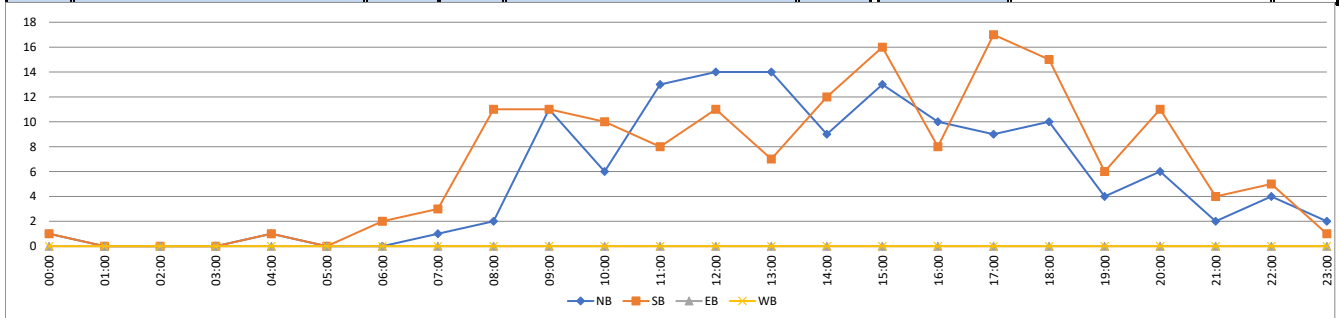
VOLUME

Lakeside Dr N/O Crescent Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_006

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					132	160	0	0	292									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	1	0			1	12:00	1	4			5	00:00	01:00	1	1			2
00:15	0	1			1	12:15	2	3			5	01:00	02:00	0	0			0
00:30	0	0			0	12:30	3	2			5	02:00	03:00	0	0			0
00:45	0	0			0	12:45	8	2			10	03:00	04:00	0	0			0
01:00	0	0			0	13:00	5	3			8	04:00	05:00	1	1			2
01:15	0	0			0	13:15	4	2			6	05:00	06:00	0	0			0
01:30	0	0			0	13:30	2	1			3	06:00	07:00	0	2			2
01:45	0	0			0	13:45	3	1			4	07:00	08:00	1	3			4
02:00	0	0			0	14:00	5	2			7	08:00	09:00	2	11			13
02:15	0	0			0	14:15	2	5			7	09:00	10:00	11	11			22
02:30	0	0			0	14:30	1	4			5	10:00	11:00	6	10			16
02:45	0	0			0	14:45	1	1			2	11:00	12:00	13	8			21
03:00	0	0			0	15:00	5	4			9	12:00	13:00	14	11			25
03:15	0	0			0	15:15	3	3			6	13:00	14:00	14	7			21
03:30	0	0			0	15:30	4	2			6	14:00	15:00	9	12			21
03:45	0	0			0	15:45	1	7			8	15:00	16:00	13	16			29
04:00	0	0			0	16:00	2	3			5	16:00	17:00	10	8			18
04:15	0	0			0	16:15	3	1			4	17:00	18:00	9	17			26
04:30	1	1			2	16:30	2	3			5	18:00	19:00	10	15			25
04:45	0	0			0	16:45	3	1			4	19:00	20:00	4	6			10
05:00	0	0			0	17:00	4	5			9	20:00	21:00	6	11			17
05:15	0	0			0	17:15	1	7			8	21:00	22:00	2	4			6
05:30	0	0			0	17:30	3	2			5	22:00	23:00	4	5			9
05:45	0	0			0	17:45	1	3			4	23:00	00:00	2	1			3
06:00	0	0			0	18:00	2	2			4	STATISTICS						
06:15	0	0			0	18:15	4	3			7		NB	SB	EB	WB	TOTAL	
06:30	0	1			1	18:30	1	6			7	Peak Period	00:00 to 12:00					
06:45	0	1			1	18:45	3	4			7	Volume	35	47			82	
07:00	0	1			1	19:00	2	2			4	Peak Hour	11:00 9:45				8:45	
07:15	0	1			1	19:15	0	3			3	Peak Volume	13	16			23	
07:30	0	0			0	19:30	1	0			1	Peak Hour Factor	0.813	0.571			0.639	
07:45	1	1			2	19:45	1	1			2	Peak Period	12:00 to 00:00					
08:00	0	0			0	20:00	2	2			4	Volume	97	113			210	
08:15	0	2			2	20:15	1	8			9	Peak Hour	12:30 17:00				12:30	
08:30	0	2			2	20:30	3	0			3	Peak Volume	20	17			29	
08:45	2	7			9	20:45	0	1			1	Peak Hour Factor	0.625	0.607			0.725	
09:00	3	3			6	21:00	2	1			3	Peak Period	07:00 to 09:00					
09:15	4	0			4	21:15	0	1			1	Volume	3	14			17	
09:30	3	1			4	21:30	0	2			2	Peak Hour	8:00 8:00				8:00	
09:45	1	7			8	21:45	0	0			0	Peak Volume	2	11			13	
10:00	0	4			4	22:00	1	2			3	Peak Hour Factor	0.250	0.393			0.361	
10:15	3	2			5	22:15	0	1			1	Peak Period	16:00 to 18:00					
10:30	1	3			4	22:30	2	1			3	Volume	19	25			44	
10:45	2	1			3	22:45	1	1			2	Peak Hour	16:15 17:00				16:30	
11:00	2	1			3	23:00	0	0			0	Peak Volume	12	17			26	
11:15	3	3			6	23:15	1	0			1	Peak Hour Factor	0.750	0.607			0.722	
11:30	4	4			8	23:30	1	1			2							
11:45	4	0			4	23:45	0	0			0							
TOTALS	35	47	0	0	82	TOTALS	97	113	0	0	210							
SPLIT %	43%	57%	0%	0%	28%	SPLIT %	46%	54%	0%	0%	72%							



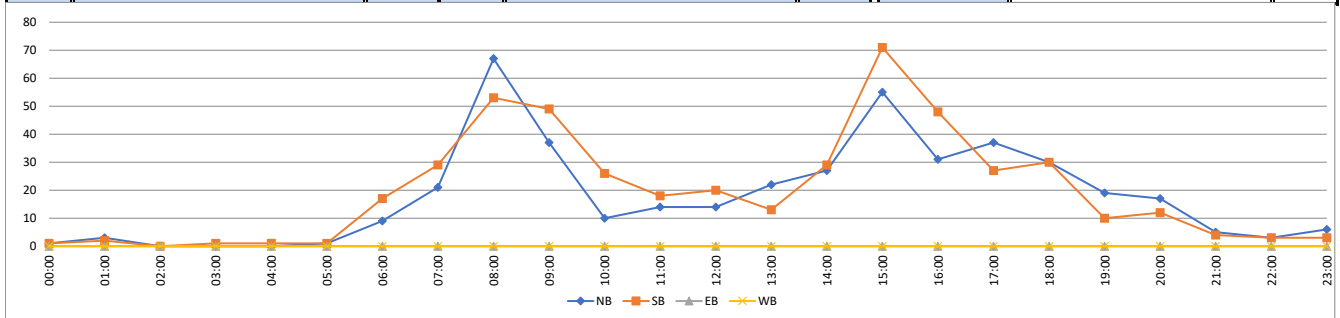
VOLUME

Lastner Ln S/O Ridge Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_007

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					429	468	0	0	897								
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	0			0	12:00	3	5			8	00:00	01:00	1	1		2
00:15	0	0			0	12:15	4	4			8	01:00	02:00	3	2		5
00:30	1	1			2	12:30	1	1			2	02:00	03:00	0	0		0
00:45	0	0			0	12:45	6	10			16	03:00	04:00	0	1		1
01:00	1	0			1	13:00	9	3			12	04:00	05:00	0	1		1
01:15	0	2			2	13:15	4	3			7	05:00	06:00	1	1		2
01:30	0	0			0	13:30	3	4			7	06:00	07:00	9	17		26
01:45	2	0			2	13:45	6	3			9	07:00	08:00	21	29		50
02:00	0	0			0	14:00	4	5			9	08:00	09:00	67	53		120
02:15	0	0			0	14:15	9	5			14	09:00	10:00	37	49		86
02:30	0	0			0	14:30	4	15			19	10:00	11:00	10	26		36
02:45	0	0			0	14:45	10	4			14	11:00	12:00	14	18		32
03:00	0	0			0	15:00	22	13			35	12:00	13:00	14	20		34
03:15	0	0			0	15:15	16	23			39	13:00	14:00	22	13		35
03:30	0	1			1	15:30	10	20			30	14:00	15:00	27	29		56
03:45	0	0			0	15:45	7	15			22	15:00	16:00	55	71		126
04:00	0	0			0	16:00	10	8			18	16:00	17:00	31	48		79
04:15	0	0			0	16:15	6	14			20	17:00	18:00	37	27		64
04:30	0	0			0	16:30	10	16			26	18:00	19:00	30	30		60
04:45	0	1			1	16:45	5	10			15	19:00	20:00	19	10		29
05:00	0	0			0	17:00	11	6			17	20:00	21:00	17	12		29
05:15	0	0			0	17:15	11	9			20	21:00	22:00	5	4		9
05:30	1	0			1	17:30	8	8			16	22:00	23:00	3	3		6
05:45	0	1			1	17:45	7	4			11	23:00	00:00	6	3		9
06:00	1	3			4	18:00	9	4			13	STATISTICS					
06:15	1	5			6	18:15	6	11			17		NB	SB	EB	WB	TOTAL
06:30	5	3			8	18:30	10	8			18	Peak Period	00:00 to 12:00				
06:45	2	6			8	18:45	5	7			12	Volume	163	198			361
07:00	4	8			12	19:00	6	2			8	Peak Hour	8:30	8:15			8:15
07:15	4	4			8	19:15	7	2			9	Peak Volume	70	69			138
07:30	7	8			15	19:30	2	2			4	Peak Hour Factor	0.673	0.639			0.719
07:45	6	9			15	19:45	4	4			8	Peak Period	12:00 to 00:00				
08:00	15	11			26	20:00	5	2			7	Volume	266	270			536
08:15	8	10			18	20:15	4	2			6	Peak Hour	14:45	15:00			15:00
08:30	18	10			28	20:30	3	4			7	Peak Volume	58	71			126
08:45	26	22			48	20:45	5	4			9	Peak Hour Factor	0.659	0.772			0.808
09:00	17	27			44	21:00	0	0			0	Peak Period	07:00 to 09:00				
09:15	9	8			17	21:15	0	0			0	Volume	88	82			170
09:30	5	9			14	21:30	3	2			5	Peak Hour	8:00	8:00			8:00
09:45	6	5			11	21:45	2	2			4	Peak Volume	67	53			120
10:00	1	10			11	22:00	1	0			1	Peak Hour Factor	0.644	0.602			0.625
10:15	2	9			11	22:15	0	1			1	Peak Period	16:00 to 18:00				
10:30	3	4			7	22:30	1	1			2	Volume	68	75			143
10:45	4	3			7	22:45	1	1			2	Peak Hour	16:30	16:00			16:00
11:00	2	7			9	23:00	1	0			1	Peak Volume	37	48			79
11:15	5	4			9	23:15	1	0			1	Peak Hour Factor	0.841	0.750			0.760
11:30	4	4			8	23:30	1	2			3						
11:45	3	3			6	23:45	3	1			4						
TOTALS	163	198	0	0	361	TOTALS	266	270	0	0	536						
SPLIT %	45%	55%	0%	0%	40%	SPLIT %	50%	50%	0%	0%	60%						



CLASSIFICATION

Lastner Ln S/O Ridge Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_007

Table with columns: Time, NORTHBOUND (#1-#13, Total), SOUTHBOUND (#1-#13, Total), TOTALS (#1-#13, Total). Rows include 15-minute breakdowns from 12:00 to 23:45 and summary rows for Totals and % of Totals.

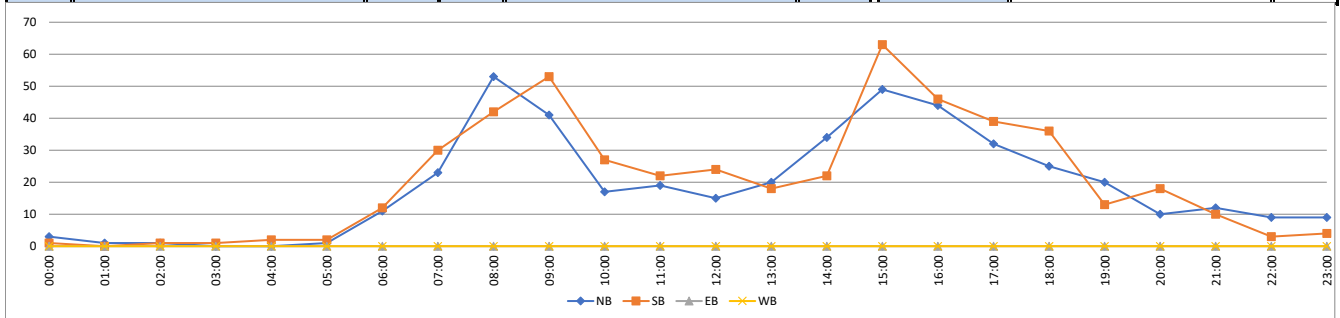
VOLUME

Lastner Ln S/O Ridge Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_007

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					449	489	0	0	938								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	1	0			1	12:00	3	12			15	00:00	01:00	3	1		4
00:15	0	1			1	12:15	2	6			8	01:00	02:00	1	0		1
00:30	2	0			2	12:30	4	2			6	02:00	03:00	1	1		2
00:45	0	0			0	12:45	6	4			10	03:00	04:00	0	1		1
01:00	0	0			0	13:00	10	5			15	04:00	05:00	0	2		2
01:15	1	0			1	13:15	2	5			7	05:00	06:00	1	2		3
01:30	0	0			0	13:30	5	4			9	06:00	07:00	11	12		23
01:45	0	0			0	13:45	3	4			7	07:00	08:00	23	30		53
02:00	0	0			0	14:00	5	5			10	08:00	09:00	53	42		95
02:15	0	1			1	14:15	7	5			12	09:00	10:00	41	53		94
02:30	0	0			0	14:30	8	7			15	10:00	11:00	17	27		44
02:45	1	0			1	14:45	14	5			19	11:00	12:00	19	22		41
03:00	0	0			0	15:00	20	8			28	12:00	13:00	15	24		39
03:15	0	0			0	15:15	16	20			36	13:00	14:00	20	18		38
03:30	0	1			1	15:30	6	24			30	14:00	15:00	34	22		56
03:45	0	0			0	15:45	7	11			18	15:00	16:00	49	63		112
04:00	0	0			0	16:00	11	13			24	16:00	17:00	44	46		90
04:15	0	0			0	16:15	15	11			26	17:00	18:00	32	39		71
04:30	0	1			1	16:30	9	13			22	18:00	19:00	25	36		61
04:45	0	1			1	16:45	9	9			18	19:00	20:00	20	13		33
05:00	0	0			0	17:00	11	11			22	20:00	21:00	10	18		28
05:15	0	1			1	17:15	6	14			20	21:00	22:00	12	10		22
05:30	0	0			0	17:30	4	7			11	22:00	23:00	9	3		12
05:45	1	1			2	17:45	11	7			18	23:00	00:00	9	4		13
06:00	3	2			5	18:00	8	8			16	STATISTICS					
06:15	1	2			3	18:15	6	9			15		NB	SB	EB	WB	TOTAL
06:30	3	3			6	18:30	6	13			19	Peak Period	00:00 to 12:00				
06:45	4	5			9	18:45	5	6			11	Volume	170	193			363
07:00	6	7			13	19:00	5	2			7	Peak Hour	8:15	8:15			8:15
07:15	5	8			13	19:15	2	4			6	Peak Volume	67	66			133
07:30	5	8			13	19:30	5	4			9	Peak Hour Factor	0.698	0.589			0.639
07:45	7	7			14	19:45	8	3			11	Peak Period	12:00 to 00:00				
08:00	10	4			14	20:00	4	6			10	Volume	279	296			575
08:15	6	11			17	20:15	2	5			7	Peak Hour	14:30	15:15			14:45
08:30	15	12			27	20:30	2	4			6	Peak Volume	58	68			113
08:45	22	15			37	20:45	2	3			5	Peak Hour Factor	0.725	0.708			0.785
09:00	24	28			52	21:00	2	2			4	Peak Period	07:00 to 09:00				
09:15	6	11			17	21:15	3	3			6	Volume	76	72			148
09:30	6	6			12	21:30	3	1			4	Peak Hour	8:00	8:00			8:00
09:45	5	8			13	21:45	4	4			8	Peak Volume	53	42			95
10:00	3	4			7	22:00	2	0			2	Peak Hour Factor	0.602	0.700			0.642
10:15	4	6			10	22:15	3	1			4	Peak Period	16:00 to 18:00				
10:30	3	12			15	22:30	3	0			3	Volume	76	85			161
10:45	7	5			12	22:45	1	2			3	Peak Hour	16:00	16:30			16:00
11:00	0	5			5	23:00	2	2			4	Peak Volume	44	47			90
11:15	8	5			13	23:15	3	0			3	Peak Hour Factor	0.733	0.839			0.865
11:30	5	4			9	23:30	1	2			3						
11:45	6	8			14	23:45	3	0			3						
TOTALS	170	193	0	0	363	TOTALS	279	296	0	0	575						
SPLIT %	47%	53%	0%	0%	39%	SPLIT %	49%	51%	0%	0%	61%						



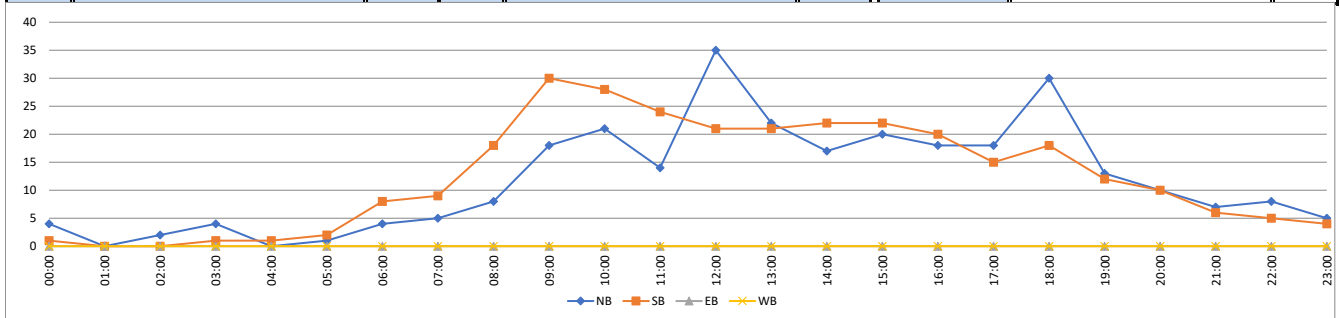
VOLUME

Lastner Ln S/O Ridge Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_007

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					284	298	0	0	582								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	2	0			2	12:00	11	4			15	00:00	01:00	4	1		5
00:15	1	0			1	12:15	8	6			14	01:00	02:00	0	0		0
00:30	1	1			2	12:30	9	5			14	02:00	03:00	2	0		2
00:45	0	0			0	12:45	7	6			13	03:00	04:00	4	1		5
01:00	0	0			0	13:00	6	7			13	04:00	05:00	0	1		1
01:15	0	0			0	13:15	3	7			10	05:00	06:00	1	2		3
01:30	0	0			0	13:30	9	4			13	06:00	07:00	4	8		12
01:45	0	0			0	13:45	4	3			7	07:00	08:00	5	9		14
02:00	1	0			1	14:00	6	6			12	08:00	09:00	8	18		26
02:15	0	0			0	14:15	4	8			12	09:00	10:00	18	30		48
02:30	0	0			0	14:30	2	3			5	10:00	11:00	21	28		49
02:45	1	0			1	14:45	5	5			10	11:00	12:00	14	24		38
03:00	0	0			0	15:00	5	5			10	12:00	13:00	35	21		56
03:15	3	0			3	15:15	8	6			14	13:00	14:00	22	21		43
03:30	0	0			0	15:30	3	6			9	14:00	15:00	17	22		39
03:45	1	1			2	15:45	4	5			9	15:00	16:00	20	22		42
04:00	0	0			0	16:00	5	7			12	16:00	17:00	18	20		38
04:15	0	0			0	16:15	4	5			9	17:00	18:00	18	15		33
04:30	0	0			0	16:30	3	2			5	18:00	19:00	30	18		48
04:45	0	1			1	16:45	6	6			12	19:00	20:00	13	12		25
05:00	0	0			0	17:00	5	6			11	20:00	21:00	10	10		20
05:15	1	0			1	17:15	3	2			5	21:00	22:00	7	6		13
05:30	0	0			0	17:30	7	5			12	22:00	23:00	8	5		13
05:45	0	2			2	17:45	3	2			5	23:00	00:00	5	4		9
06:00	0	0			0	18:00	6	8			14	STATISTICS					
06:15	1	1			2	18:15	11	2			13		NB	SB	EB	WB	TOTAL
06:30	0	3			3	18:30	6	4			10	Peak Period	00:00 to 12:00				
06:45	3	4			7	18:45	7	4			11	Volume	81	122			203
07:00	1	4			5	19:00	4	5			9	Peak Hour	10:00 9:45				9:45
07:15	3	3			6	19:15	4	5			9	Peak Volume	21	31			49
07:30	1	0			1	19:30	3	2			5	Peak Hour Factor	0.750	0.861			0.766
07:45	0	2			2	19:45	2	0			2	Peak Period	12:00 to 00:00				
08:00	3	7			10	20:00	2	4			6	Volume	203	176			379
08:15	4	0			4	20:15	0	2			2	Peak Hour	12:00 12:30				12:00
08:30	0	8			8	20:30	6	2			8	Peak Volume	35	25			56
08:45	1	3			4	20:45	2	2			4	Peak Hour Factor	0.795	0.893			0.933
09:00	7	7			14	21:00	2	2			4	Peak Period	07:00 to 09:00				
09:15	3	6			9	21:15	1	3			4	Volume	13	27			40
09:30	5	8			13	21:30	1	0			1	Peak Hour	7:30 8:00				8:00
09:45	3	9			12	21:45	3	1			4	Peak Volume	8	18			26
10:00	6	4			10	22:00	3	2			5	Peak Hour Factor	0.500	0.563			0.650
10:15	2	9			11	22:15	2	1			3	Peak Period	16:00 to 18:00				
10:30	7	9			16	22:30	2	1			3	Volume	36	35			71
10:45	6	6			12	22:45	1	1			2	Peak Hour	16:45 16:00				16:45
11:00	4	6			10	23:00	3	1			4	Peak Volume	21	20			40
11:15	2	6			8	23:15	1	1			2	Peak Hour Factor	0.750	0.714			0.833
11:30	1	7			8	23:30	0	1			1						
11:45	7	5			12	23:45	1	1			2						
TOTALS	81	122	0	0	203	TOTALS	203	176	0	0	379						
SPLIT %	40%	60%	0%	0%	35%	SPLIT %	54%	46%	0%	0%	65%						



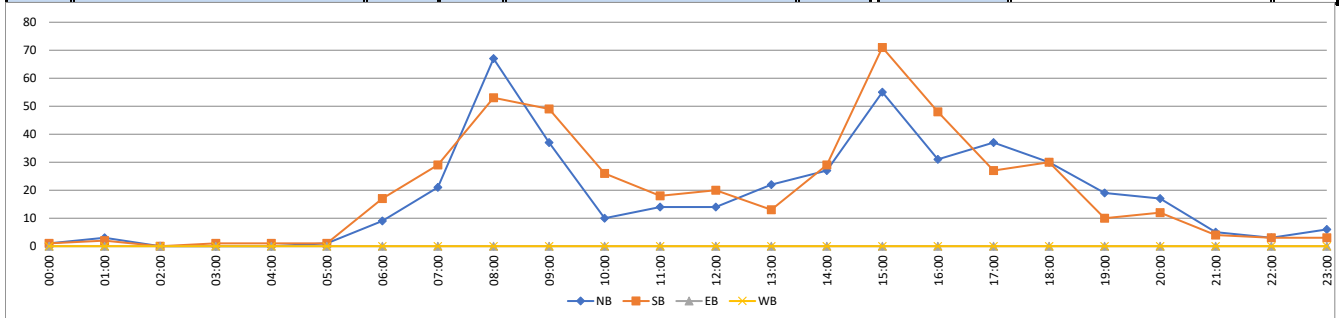
VOLUME

Lastner Ln S/O Ridge Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_007

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					429	468	0	0	897								
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	0			0	12:00	3	5			8	00:00	01:00	1	1		2
00:15	0	0			0	12:15	4	4			8	01:00	02:00	3	2		5
00:30	1	1			2	12:30	1	1			2	02:00	03:00	0	0		0
00:45	0	0			0	12:45	6	10			16	03:00	04:00	0	1		1
01:00	1	0			1	13:00	9	3			12	04:00	05:00	0	1		1
01:15	0	2			2	13:15	4	3			7	05:00	06:00	1	1		2
01:30	0	0			0	13:30	3	4			7	06:00	07:00	9	17		26
01:45	2	0			2	13:45	6	3			9	07:00	08:00	21	29		50
02:00	0	0			0	14:00	4	5			9	08:00	09:00	67	53		120
02:15	0	0			0	14:15	9	5			14	09:00	10:00	37	49		86
02:30	0	0			0	14:30	4	15			19	10:00	11:00	10	26		36
02:45	0	0			0	14:45	10	4			14	11:00	12:00	14	18		32
03:00	0	0			0	15:00	22	13			35	12:00	13:00	14	20		34
03:15	0	0			0	15:15	16	23			39	13:00	14:00	22	13		35
03:30	0	1			1	15:30	10	20			30	14:00	15:00	27	29		56
03:45	0	0			0	15:45	7	15			22	15:00	16:00	55	71		126
04:00	0	0			0	16:00	10	8			18	16:00	17:00	31	48		79
04:15	0	0			0	16:15	6	14			20	17:00	18:00	37	27		64
04:30	0	0			0	16:30	10	16			26	18:00	19:00	30	30		60
04:45	0	1			1	16:45	5	10			15	19:00	20:00	19	10		29
05:00	0	0			0	17:00	11	6			17	20:00	21:00	17	12		29
05:15	0	0			0	17:15	11	9			20	21:00	22:00	5	4		9
05:30	1	0			1	17:30	8	8			16	22:00	23:00	3	3		6
05:45	0	1			1	17:45	7	4			11	23:00	00:00	6	3		9
06:00	1	3			4	18:00	9	4			13	STATISTICS					
06:15	1	5			6	18:15	6	11			17		NB	SB	EB	WB	TOTAL
06:30	5	3			8	18:30	10	8			18	Peak Period	00:00 to 12:00				
06:45	2	6			8	18:45	5	7			12	Volume	163	198			361
07:00	4	8			12	19:00	6	2			8	Peak Hour	8:30 8:15				8:15
07:15	4	4			8	19:15	7	2			9	Volume	70	69			138
07:30	7	8			15	19:30	2	2			4	Peak Hour Factor	0.673	0.639			0.719
07:45	6	9			15	19:45	4	4			8	Peak Period	12:00 to 00:00				
08:00	15	11			26	20:00	5	2			7	Volume	266	270			536
08:15	8	10			18	20:15	4	2			6	Peak Hour	14:45 15:00				15:00
08:30	18	10			28	20:30	3	4			7	Volume	58	71			126
08:45	26	22			48	20:45	5	4			9	Peak Hour Factor	0.659	0.772			0.808
09:00	17	27			44	21:00	0	0			0	Peak Period	07:00 to 09:00				
09:15	9	8			17	21:15	0	0			0	Volume	88	82			170
09:30	5	9			14	21:30	3	2			5	Peak Hour	8:00 8:00				8:00
09:45	6	5			11	21:45	2	2			4	Volume	67	53			120
10:00	1	10			11	22:00	1	0			1	Peak Hour Factor	0.644	0.602			0.625
10:15	2	9			11	22:15	0	1			1	Peak Period	16:00 to 18:00				
10:30	3	4			7	22:30	1	1			2	Volume	68	75			143
10:45	4	3			7	22:45	1	1			2	Peak Hour	16:30 16:00				16:00
11:00	2	7			9	23:00	1	0			1	Volume	37	48			79
11:15	5	4			9	23:15	1	0			1	Peak Hour Factor	0.841	0.750			0.760
11:30	4	4			8	23:30	1	2			3						
11:45	3	3			6	23:45	3	1			4						
TOTALS	163	198	0	0	361	TOTALS	266	270	0	0	536						
SPLIT %	45%	55%	0%	0%	40%	SPLIT %	50%	50%	0%	0%	60%						



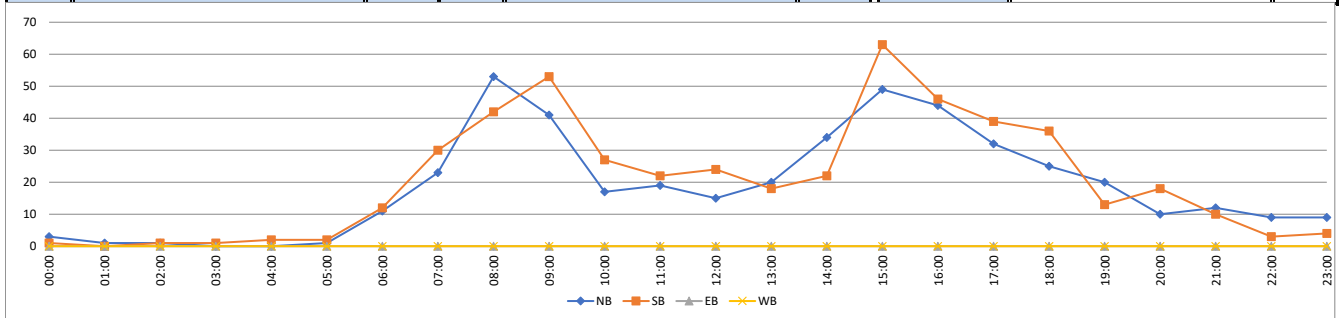
VOLUME

Lastner Ln S/O Ridge Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_007

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					449	489	0	0	938								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	1	0			1	12:00	3	12			15	00:00	01:00	3	1		4
00:15	0	1			1	12:15	2	6			8	01:00	02:00	1	0		1
00:30	2	0			2	12:30	4	2			6	02:00	03:00	1	1		2
00:45	0	0			0	12:45	6	4			10	03:00	04:00	0	1		1
01:00	0	0			0	13:00	10	5			15	04:00	05:00	0	2		2
01:15	1	0			1	13:15	2	5			7	05:00	06:00	1	2		3
01:30	0	0			0	13:30	5	4			9	06:00	07:00	11	12		23
01:45	0	0			0	13:45	3	4			7	07:00	08:00	23	30		53
02:00	0	0			0	14:00	5	5			10	08:00	09:00	53	42		95
02:15	0	1			1	14:15	7	5			12	09:00	10:00	41	53		94
02:30	0	0			0	14:30	8	7			15	10:00	11:00	17	27		44
02:45	1	0			1	14:45	14	5			19	11:00	12:00	19	22		41
03:00	0	0			0	15:00	20	8			28	12:00	13:00	15	24		39
03:15	0	0			0	15:15	16	20			36	13:00	14:00	20	18		38
03:30	0	1			1	15:30	6	24			30	14:00	15:00	34	22		56
03:45	0	0			0	15:45	7	11			18	15:00	16:00	49	63		112
04:00	0	0			0	16:00	11	13			24	16:00	17:00	44	46		90
04:15	0	0			0	16:15	15	11			26	17:00	18:00	32	39		71
04:30	0	1			1	16:30	9	13			22	18:00	19:00	25	36		61
04:45	0	1			1	16:45	9	9			18	19:00	20:00	20	13		33
05:00	0	0			0	17:00	11	11			22	20:00	21:00	10	18		28
05:15	0	1			1	17:15	6	14			20	21:00	22:00	12	10		22
05:30	0	0			0	17:30	4	7			11	22:00	23:00	9	3		12
05:45	1	1			2	17:45	11	7			18	23:00	00:00	9	4		13
06:00	3	2			5	18:00	8	8			16	STATISTICS					
06:15	1	2			3	18:15	6	9			15		NB	SB	EB	WB	TOTAL
06:30	3	3			6	18:30	6	13			19	Peak Period	00:00 to 12:00				
06:45	4	5			9	18:45	5	6			11	Volume	170	193			363
07:00	6	7			13	19:00	5	2			7	Peak Hour	8:15	8:15			8:15
07:15	5	8			13	19:15	2	4			6	Peak Volume	67	66			133
07:30	5	8			13	19:30	5	4			9	Peak Hour Factor	0.698	0.589			0.639
07:45	7	7			14	19:45	8	3			11	Peak Period	12:00 to 00:00				
08:00	10	4			14	20:00	4	6			10	Volume	279	296			575
08:15	6	11			17	20:15	2	5			7	Peak Hour	14:30	15:15			14:45
08:30	15	12			27	20:30	2	4			6	Peak Volume	58	68			113
08:45	22	15			37	20:45	2	3			5	Peak Hour Factor	0.725	0.708			0.785
09:00	24	28			52	21:00	2	2			4	Peak Period	07:00 to 09:00				
09:15	6	11			17	21:15	3	3			6	Volume	76	72			148
09:30	6	6			12	21:30	3	1			4	Peak Hour	8:00	8:00			8:00
09:45	5	8			13	21:45	4	4			8	Peak Volume	53	42			95
10:00	3	4			7	22:00	2	0			2	Peak Hour Factor	0.602	0.700			0.642
10:15	4	6			10	22:15	3	1			4	Peak Period	16:00 to 18:00				
10:30	3	12			15	22:30	3	0			3	Volume	76	85			161
10:45	7	5			12	22:45	1	2			3	Peak Hour	16:00	16:30			16:00
11:00	0	5			5	23:00	2	2			4	Peak Volume	44	47			90
11:15	8	5			13	23:15	3	0			3	Peak Hour Factor	0.733	0.839			0.865
11:30	5	4			9	23:30	1	2			3						
11:45	6	8			14	23:45	3	0			3						
TOTALS	170	193	0	0	363	TOTALS	279	296	0	0	575						
SPLIT %	47%	53%	0%	0%	39%	SPLIT %	49%	51%	0%	0%	61%						



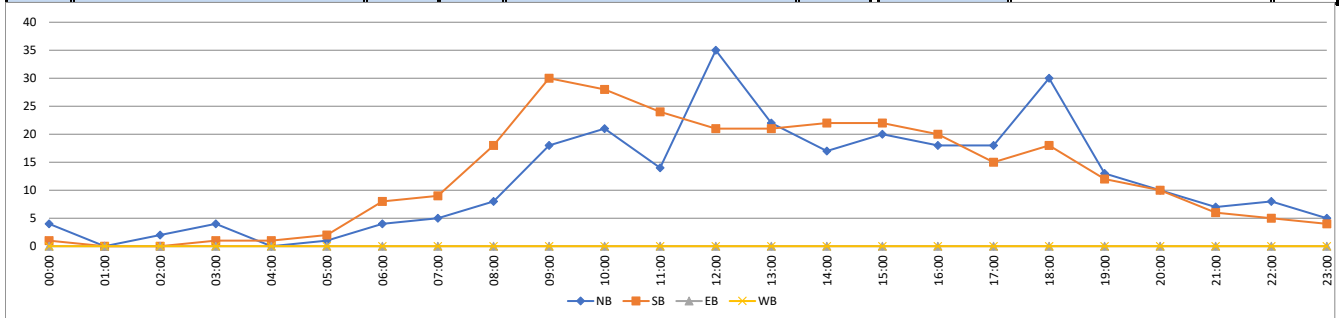
VOLUME

Lastner Ln S/O Ridge Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_007

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					284	298	0	0	582								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	2	0			2	12:00	11	4			15	00:00	01:00	4	1		5
00:15	1	0			1	12:15	8	6			14	01:00	02:00	0	0		0
00:30	1	1			2	12:30	9	5			14	02:00	03:00	2	0		2
00:45	0	0			0	12:45	7	6			13	03:00	04:00	4	1		5
01:00	0	0			0	13:00	6	7			13	04:00	05:00	0	1		1
01:15	0	0			0	13:15	3	7			10	05:00	06:00	1	2		3
01:30	0	0			0	13:30	9	4			13	06:00	07:00	4	8		12
01:45	0	0			0	13:45	4	3			7	07:00	08:00	5	9		14
02:00	1	0			1	14:00	6	6			12	08:00	09:00	8	18		26
02:15	0	0			0	14:15	4	8			12	09:00	10:00	18	30		48
02:30	0	0			0	14:30	2	3			5	10:00	11:00	21	28		49
02:45	1	0			1	14:45	5	5			10	11:00	12:00	14	24		38
03:00	0	0			0	15:00	5	5			10	12:00	13:00	35	21		56
03:15	3	0			3	15:15	8	6			14	13:00	14:00	22	21		43
03:30	0	0			0	15:30	3	6			9	14:00	15:00	17	22		39
03:45	1	1			2	15:45	4	5			9	15:00	16:00	20	22		42
04:00	0	0			0	16:00	5	7			12	16:00	17:00	18	20		38
04:15	0	0			0	16:15	4	5			9	17:00	18:00	18	15		33
04:30	0	0			0	16:30	3	2			5	18:00	19:00	30	18		48
04:45	0	1			1	16:45	6	6			12	19:00	20:00	13	12		25
05:00	0	0			0	17:00	5	6			11	20:00	21:00	10	10		20
05:15	1	0			1	17:15	3	2			5	21:00	22:00	7	6		13
05:30	0	0			0	17:30	7	5			12	22:00	23:00	8	5		13
05:45	0	2			2	17:45	3	2			5	23:00	00:00	5	4		9
06:00	0	0			0	18:00	6	8			14	STATISTICS					
06:15	1	1			2	18:15	11	2			13		NB	SB	EB	WB	TOTAL
06:30	0	3			3	18:30	6	4			10	Peak Period	00:00 to 12:00				
06:45	3	4			7	18:45	7	4			11	Volume	81	122			203
07:00	1	4			5	19:00	4	5			9	Peak Hour	10:00 9:45				9:45
07:15	3	3			6	19:15	4	5			9	Peak Volume	21	31			49
07:30	1	0			1	19:30	3	2			5	Peak Hour Factor	0.750	0.861			0.766
07:45	0	2			2	19:45	2	0			2	Peak Period	12:00 to 00:00				
08:00	3	7			10	20:00	2	4			6	Volume	203	176			379
08:15	4	0			4	20:15	0	2			2	Peak Hour	12:00 12:30				12:00
08:30	0	8			8	20:30	6	2			8	Peak Volume	35	25			56
08:45	1	3			4	20:45	2	2			4	Peak Hour Factor	0.795	0.893			0.933
09:00	7	7			14	21:00	2	2			4	Peak Period	07:00 to 09:00				
09:15	3	6			9	21:15	1	3			4	Volume	13	27			40
09:30	5	8			13	21:30	1	0			1	Peak Hour	7:30 8:00				8:00
09:45	3	9			12	21:45	3	1			4	Peak Volume	8	18			26
10:00	6	4			10	22:00	3	2			5	Peak Hour Factor	0.500	0.563			0.650
10:15	2	9			11	22:15	2	1			3	Peak Period	16:00 to 18:00				
10:30	7	9			16	22:30	2	1			3	Volume	36	35			71
10:45	6	6			12	22:45	1	1			2	Peak Hour	16:45 16:00				16:45
11:00	4	6			10	23:00	3	1			4	Peak Volume	21	20			40
11:15	2	6			8	23:15	1	1			2	Peak Hour Factor	0.750	0.714			0.833
11:30	1	7			8	23:30	0	1			1						
11:45	7	5			12	23:45	1	1			2						
TOTALS	81	122	0	0	203	TOTALS	203	176	0	0	379						
SPLIT %	40%	60%	0%	0%	35%	SPLIT %	54%	46%	0%	0%	65%						



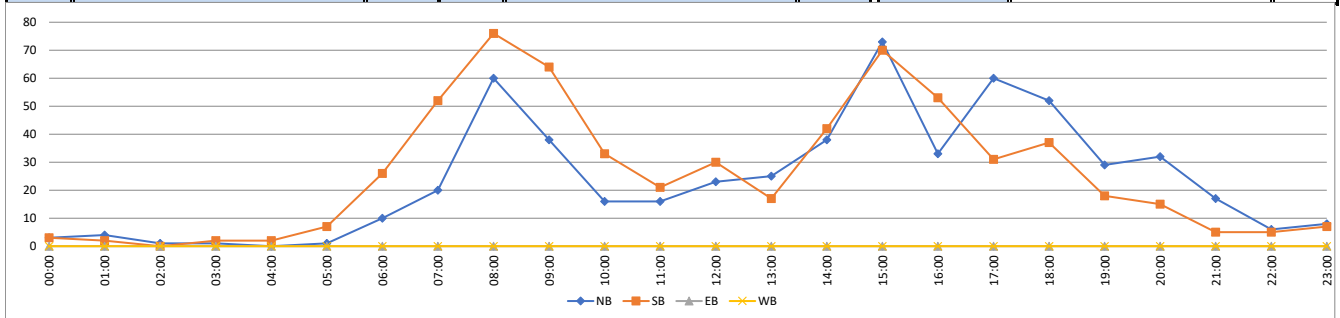
VOLUME

Lastner Ln Bet. Rosewood Dr & Ivy Ln

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_008

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					566	618	0	0	1,184								
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	2			2	12:00	4	7			11	00:00	01:00	3	3		6
00:15	2	0			2	12:15	8	8			16	01:00	02:00	4	2		6
00:30	1	1			2	12:30	4	6			10	02:00	03:00	1	0		1
00:45	0	0			0	12:45	7	9			16	03:00	04:00	1	2		3
01:00	2	0			2	13:00	11	4			15	04:00	05:00	0	2		2
01:15	0	2			2	13:15	5	5			10	05:00	06:00	1	7		8
01:30	0	0			0	13:30	4	6			10	06:00	07:00	10	26		36
01:45	2	0			2	13:45	5	2			7	07:00	08:00	20	52		72
02:00	1	0			1	14:00	10	9			19	08:00	09:00	60	76		136
02:15	0	0			0	14:15	10	6			16	09:00	10:00	38	64		102
02:30	0	0			0	14:30	4	18			22	10:00	11:00	16	33		49
02:45	0	0			0	14:45	14	9			23	11:00	12:00	16	21		37
03:00	0	1			1	15:00	25	12			37	12:00	13:00	23	30		53
03:15	0	0			0	15:15	16	25			41	13:00	14:00	25	17		42
03:30	0	1			1	15:30	18	21			39	14:00	15:00	38	42		80
03:45	1	0			1	15:45	14	12			26	15:00	16:00	73	70		143
04:00	0	0			0	16:00	8	10			18	16:00	17:00	33	53		86
04:15	0	1			1	16:15	6	14			20	17:00	18:00	60	31		91
04:30	0	0			0	16:30	8	20			28	18:00	19:00	52	37		89
04:45	0	1			1	16:45	11	9			20	19:00	20:00	29	18		47
05:00	0	3			3	17:00	16	5			21	20:00	21:00	32	15		47
05:15	0	1			1	17:15	19	11			30	21:00	22:00	17	5		22
05:30	0	2			2	17:30	12	10			22	22:00	23:00	6	5		11
05:45	1	1			2	17:45	13	5			18	23:00	00:00	8	7		15
06:00	2	3			5	18:00	15	10			25	STATISTICS					
06:15	1	6			7	18:15	12	14			26		NB	SB	EB	WB	TOTAL
06:30	2	6			8	18:30	17	8			25	Peak Period	00:00 to 12:00				
06:45	5	11			16	18:45	8	5			13	Volume	170	288			458
07:00	4	16			20	19:00	7	7			14	Peak Hour	8:30	8:15			8:30
07:15	4	7			11	19:15	9	2			11	Peak Volume	67	86			153
07:30	6	17			23	19:30	4	5			9	Peak Hour Factor	0.698	0.827			0.797
07:45	6	12			18	19:45	9	4			13	Peak Period	12:00 to 00:00				
08:00	13	16			29	20:00	12	1			13	Volume	396	330			726
08:15	8	15			23	20:15	8	3			11	Peak Hour	14:45	15:00			15:00
08:30	15	21			36	20:30	8	5			13	Peak Volume	73	70			143
08:45	24	24			48	20:45	4	6			10	Peak Hour Factor	0.730	0.700			0.872
09:00	16	26			42	21:00	3	1			4	Peak Period	07:00 to 09:00				
09:15	12	15			27	21:15	6	0			6	Volume	80	128			208
09:30	4	14			18	21:30	5	2			7	Peak Hour	8:00	8:00			8:00
09:45	6	9			15	21:45	3	2			5	Peak Volume	60	76			136
10:00	3	13			16	22:00	1	1			2	Peak Hour Factor	0.625	0.792			0.708
10:15	5	10			15	22:15	0	1			1	Peak Period	16:00 to 18:00				
10:30	4	5			9	22:30	4	2			6	Volume	93	84			177
10:45	4	5			9	22:45	1	1			2	Peak Hour	17:00	16:00			16:30
11:00	1	5			6	23:00	2	3			5	Peak Volume	60	53			99
11:15	6	5			11	23:15	1	0			1	Peak Hour Factor	0.789	0.663			0.825
11:30	7	6			13	23:30	2	2			4						
11:45	2	5			7	23:45	3	2			5						
TOTALS	170	288	0	0	458	TOTALS	396	330	0	0	726						
SPLIT %	37%	63%	0%	0%	39%	SPLIT %	55%	45%	0%	0%	61%						



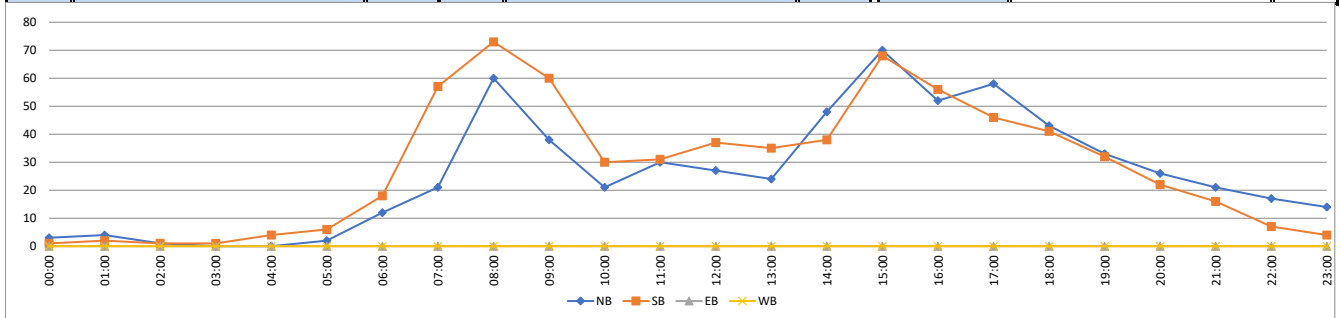
VOLUME

Lastner Ln Bet. Rosewood Dr & Ivy Ln

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_008

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					625	686	0	0	1,311								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	1	0			1	12:00	4	14			18	00:00	01:00	3	1		4
00:15	0	1			1	12:15	6	9			15	01:00	02:00	4	2		6
00:30	2	0			2	12:30	8	8			16	02:00	03:00	1	1		2
00:45	0	0			0	12:45	9	6			15	03:00	04:00	0	1		1
01:00	1	1			2	13:00	6	9			15	04:00	05:00	0	4		4
01:15	3	0			3	13:15	4	8			12	05:00	06:00	2	6		8
01:30	0	0			0	13:30	6	6			12	06:00	07:00	12	18		30
01:45	0	1			1	13:45	8	12			20	07:00	08:00	21	57		78
02:00	0	0			0	14:00	10	11			21	08:00	09:00	60	73		133
02:15	0	1			1	14:15	10	6			16	09:00	10:00	38	60		98
02:30	0	0			0	14:30	11	13			24	10:00	11:00	21	30		51
02:45	1	0			1	14:45	17	8			25	11:00	12:00	30	31		61
03:00	0	0			0	15:00	17	13			30	12:00	13:00	27	37		64
03:15	0	0			0	15:15	25	23			48	13:00	14:00	24	35		59
03:30	0	1			1	15:30	13	23			36	14:00	15:00	48	38		86
03:45	0	0			0	15:45	15	9			24	15:00	16:00	70	68		138
04:00	0	0			0	16:00	17	17			34	16:00	17:00	52	56		108
04:15	0	1			1	16:15	15	9			24	17:00	18:00	58	46		104
04:30	0	1			1	16:30	11	17			28	18:00	19:00	43	41		84
04:45	0	2			2	16:45	9	13			22	19:00	20:00	33	32		65
05:00	0	1			1	17:00	14	12			26	20:00	21:00	26	22		48
05:15	0	1			1	17:15	17	14			31	21:00	22:00	21	16		37
05:30	0	0			0	17:30	8	9			17	22:00	23:00	17	7		24
05:45	2	4			6	17:45	19	11			30	23:00	00:00	14	4		18
06:00	2	4			6	18:00	17	11			28	STATISTICS					
06:15	1	2			3	18:15	11	12			23		NB	SB	EB	WB	TOTAL
06:30	5	6			11	18:30	8	9			17	Peak Period	00:00	to	12:00		
06:45	4	6			10	18:45	7	9			16	Volume	192	284			476
07:00	5	14			19	19:00	9	6			15	Peak Hour	8:15	8:15			8:15
07:15	6	13			19	19:15	6	6			12	Peak Volume	66	88			154
07:30	2	19			21	19:30	9	12			21	Peak Hour Factor	0.825	0.880			0.917
07:45	8	11			19	19:45	9	8			17	Peak Period	12:00	to	00:00		
08:00	11	10			21	20:00	9	7			16	Volume	433	402			835
08:15	13	22			35	20:15	6	5			11	Peak Hour	14:45	15:15			15:15
08:30	16	19			35	20:30	5	6			11	Peak Volume	72	72			142
08:45	20	22			42	20:45	6	4			10	Peak Hour Factor	0.720	0.783			0.740
09:00	17	25			42	21:00	4	7			11	Peak Period	07:00	to	09:00		
09:15	7	17			24	21:15	6	3			9	Volume	81	130			211
09:30	8	8			16	21:30	8	1			9	Peak Hour	8:00	8:00			8:00
09:45	6	10			16	21:45	3	5			8	Peak Volume	60	73			133
10:00	4	6			10	22:00	5	0			5	Peak Hour Factor	0.750	0.830			0.792
10:15	7	9			16	22:15	5	2			7	Peak Period	16:00	to	18:00		
10:30	4	11			15	22:30	5	1			6	Volume	110	102			212
10:45	6	4			10	22:45	2	4			6	Peak Hour	17:00	16:00			16:00
11:00	5	8			13	23:00	2	1			3	Peak Volume	58	56			108
11:15	10	7			17	23:15	6	0			6	Peak Hour Factor	0.763	0.824			0.794
11:30	7	9			16	23:30	3	2			5						
11:45	8	7			15	23:45	3	1			4						
TOTALS	192	284	0	0	476	TOTALS	433	402	0	0	835						
SPLIT %	40%	60%	0%	0%	36%	SPLIT %	52%	48%	0%	0%	64%						



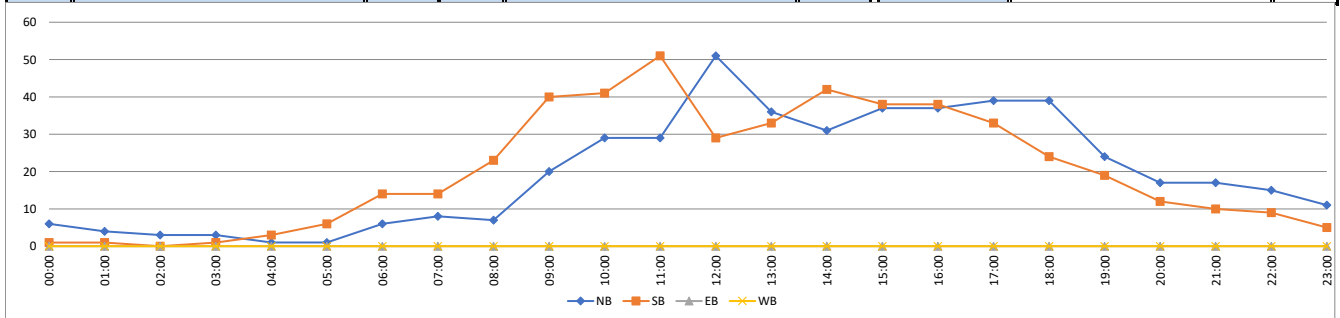
VOLUME

Lastner Ln Bet. Rosewood Dr & Ivy Ln

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_008

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					471	487	0	0	958									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	2	0			2	12:00	16	6			22	00:00	01:00	6	1			7
00:15	2	0			2	12:15	12	10			22	01:00	02:00	4	1			5
00:30	1	1			2	12:30	14	6			20	02:00	03:00	3	0			3
00:45	1	0			1	12:45	9	7			16	03:00	04:00	3	1			4
01:00	2	0			2	13:00	10	11			21	04:00	05:00	1	3			4
01:15	0	0			0	13:15	7	13			20	05:00	06:00	1	6			7
01:30	0	0			0	13:30	11	4			15	06:00	07:00	6	14			20
01:45	2	1			3	13:45	8	5			13	07:00	08:00	8	14			22
02:00	1	0			1	14:00	10	13			23	08:00	09:00	7	23			30
02:15	1	0			1	14:15	6	12			18	09:00	10:00	20	40			60
02:30	0	0			0	14:30	7	6			13	10:00	11:00	29	41			70
02:45	1	0			1	14:45	8	11			19	11:00	12:00	29	51			80
03:00	0	0			0	15:00	6	7			13	12:00	13:00	51	29			80
03:15	2	0			2	15:15	15	14			29	13:00	14:00	36	33			69
03:30	0	0			0	15:30	6	8			14	14:00	15:00	31	42			73
03:45	1	1			2	15:45	10	9			19	15:00	16:00	37	38			75
04:00	1	0			1	16:00	10	10			20	16:00	17:00	37	38			75
04:15	0	0			0	16:15	8	12			20	17:00	18:00	39	33			72
04:30	0	2			2	16:30	10	8			18	18:00	19:00	39	24			63
04:45	0	1			1	16:45	9	8			17	19:00	20:00	24	19			43
05:00	0	1			1	17:00	12	11			23	20:00	21:00	17	12			29
05:15	1	2			3	17:15	12	12			24	21:00	22:00	17	10			27
05:30	0	0			0	17:30	8	4			12	22:00	23:00	15	9			24
05:45	0	3			3	17:45	7	6			13	23:00	00:00	11	5			16
06:00	0	0			0	18:00	15	7			22	STATISTICS						
06:15	2	3			5	18:15	11	3			14		NB	SB	EB	WB	TOTAL	
06:30	1	4			5	18:30	5	7			12	Peak Period	00:00 to 12:00					
06:45	3	7			10	18:45	8	7			15	Volume	117	195			312	
07:00	3	6			9	19:00	9	9			18	Peak Hour	10:30	10:45			10:30	
07:15	2	5			7	19:15	6	8			14	Peak Volume	35	52			86	
07:30	2	1			3	19:30	5	2			7	Peak Hour Factor	0.729	0.813			0.796	
07:45	1	2			3	19:45	4	0			4	Peak Period	12:00 to 00:00					
08:00	2	6			8	20:00	7	5			12	Volume	354	292			646	
08:15	2	4			6	20:15	3	4			7	Peak Hour	12:00	14:00			15:15	
08:30	2	9			11	20:30	6	3			9	Peak Volume	51	42			82	
08:45	1	4			5	20:45	1	0			1	Peak Hour Factor	0.797	0.808			0.707	
09:00	8	12			20	21:00	7	3			10	Peak Period	07:00 to 09:00					
09:15	4	7			11	21:15	5	5			10	Volume	15	37			52	
09:30	6	12			18	21:30	1	2			3	Peak Hour	7:00	8:00			8:00	
09:45	2	9			11	21:45	4	0			4	Peak Volume	8	23			30	
10:00	7	7			14	22:00	6	5			11	Peak Hour Factor	0.667	0.639			0.682	
10:15	3	9			12	22:15	2	1			3	Peak Period	16:00 to 18:00					
10:30	12	15			27	22:30	5	2			7	Volume	76	71			147	
10:45	7	10			17	22:45	2	1			3	Peak Hour	16:30	16:15			16:30	
11:00	9	14			23	23:00	4	1			5	Peak Volume	43	39			82	
11:15	7	12			19	23:15	4	2			6	Peak Hour Factor	0.896	0.813			0.854	
11:30	5	16			21	23:30	2	1			3							
11:45	8	9			17	23:45	1	1			2							
TOTALS	117	195	0	0	312	TOTALS	354	292	0	0	646							
SPLIT %	38%	63%	0%	0%	33%	SPLIT %	55%	45%	0%	0%	67%							



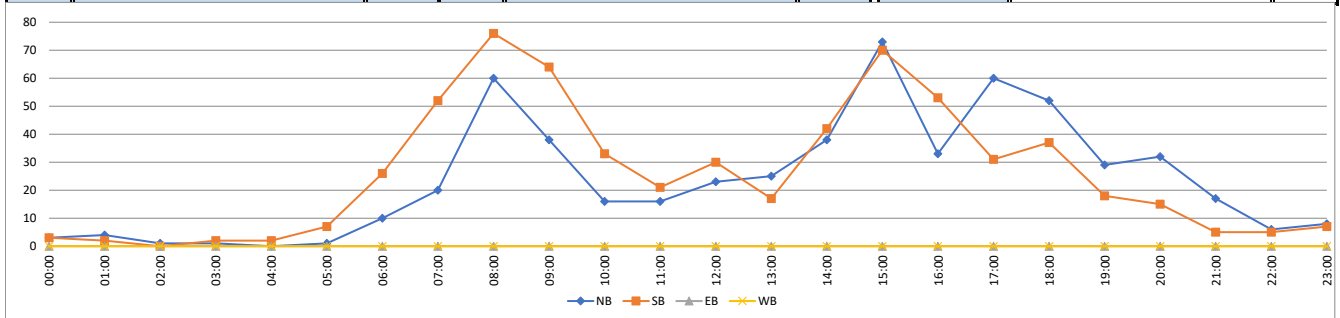
VOLUME

Lastner Ln Bet. Rosewood Dr & Ivy Ln

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_008

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					566	618	0	0	1,184								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	2			2	12:00	4	7			11	00:00	01:00	3	3		6
00:15	2	0			2	12:15	8	8			16	01:00	02:00	4	2		6
00:30	1	1			2	12:30	4	6			10	02:00	03:00	1	0		1
00:45	0	0			0	12:45	7	9			16	03:00	04:00	1	2		3
01:00	2	0			2	13:00	11	4			15	04:00	05:00	0	2		2
01:15	0	2			2	13:15	5	5			10	05:00	06:00	1	7		8
01:30	0	0			0	13:30	4	6			10	06:00	07:00	10	26		36
01:45	2	0			2	13:45	5	2			7	07:00	08:00	20	52		72
02:00	1	0			1	14:00	10	9			19	08:00	09:00	60	76		136
02:15	0	0			0	14:15	10	6			16	09:00	10:00	38	64		102
02:30	0	0			0	14:30	4	18			22	10:00	11:00	16	33		49
02:45	0	0			0	14:45	14	9			23	11:00	12:00	16	21		37
03:00	0	1			1	15:00	25	12			37	12:00	13:00	23	30		53
03:15	0	0			0	15:15	16	25			41	13:00	14:00	25	17		42
03:30	0	1			1	15:30	18	21			39	14:00	15:00	38	42		80
03:45	1	0			1	15:45	14	12			26	15:00	16:00	73	70		143
04:00	0	0			0	16:00	8	10			18	16:00	17:00	33	53		86
04:15	0	1			1	16:15	6	14			20	17:00	18:00	60	31		91
04:30	0	0			0	16:30	8	20			28	18:00	19:00	52	37		89
04:45	0	1			1	16:45	11	9			20	19:00	20:00	29	18		47
05:00	0	3			3	17:00	16	5			21	20:00	21:00	32	15		47
05:15	0	1			1	17:15	19	11			30	21:00	22:00	17	5		22
05:30	0	2			2	17:30	12	10			22	22:00	23:00	6	5		11
05:45	1	1			2	17:45	13	5			18	23:00	00:00	8	7		15
06:00	2	3			5	18:00	15	10			25	STATISTICS					
06:15	1	6			7	18:15	12	14			26		NB	SB	EB	WB	TOTAL
06:30	2	6			8	18:30	17	8			25	Peak Period	00:00	to	12:00		
06:45	5	11			16	18:45	8	5			13	Volume	170	288			458
07:00	4	16			20	19:00	7	7			14	Peak Hour	8:30	8:15			8:30
07:15	4	7			11	19:15	9	2			11	Peak Volume	67	86			153
07:30	6	17			23	19:30	4	5			9	Peak Hour Factor	0.698	0.827			0.797
07:45	6	12			18	19:45	9	4			13	Peak Period	12:00	to	00:00		
08:00	13	16			29	20:00	12	1			13	Volume	396	330			726
08:15	8	15			23	20:15	8	3			11	Peak Hour	14:45	15:00			15:00
08:30	15	21			36	20:30	8	5			13	Peak Volume	73	70			143
08:45	24	24			48	20:45	4	6			10	Peak Hour Factor	0.730	0.700			0.872
09:00	16	26			42	21:00	3	1			4	Peak Period	07:00	to	09:00		
09:15	12	15			27	21:15	6	0			6	Volume	80	128			208
09:30	4	14			18	21:30	5	2			7	Peak Hour	8:00	8:00			8:00
09:45	6	9			15	21:45	3	2			5	Peak Volume	60	76			136
10:00	3	13			16	22:00	1	1			2	Peak Hour Factor	0.625	0.792			0.708
10:15	5	10			15	22:15	0	1			1	Peak Period	16:00	to	18:00		
10:30	4	5			9	22:30	4	2			6	Volume	93	84			177
10:45	4	5			9	22:45	1	1			2	Peak Hour	17:00	16:00			16:30
11:00	1	5			6	23:00	2	3			5	Peak Volume	60	53			99
11:15	6	5			11	23:15	1	0			1	Peak Hour Factor	0.789	0.663			0.825
11:30	7	6			13	23:30	2	2			4						
11:45	2	5			7	23:45	3	2			5						
TOTALS	170	288	0	0	458	TOTALS	396	330	0	0	726						
SPLIT %	37%	63%	0%	0%	39%	SPLIT %	55%	45%	0%	0%	61%						



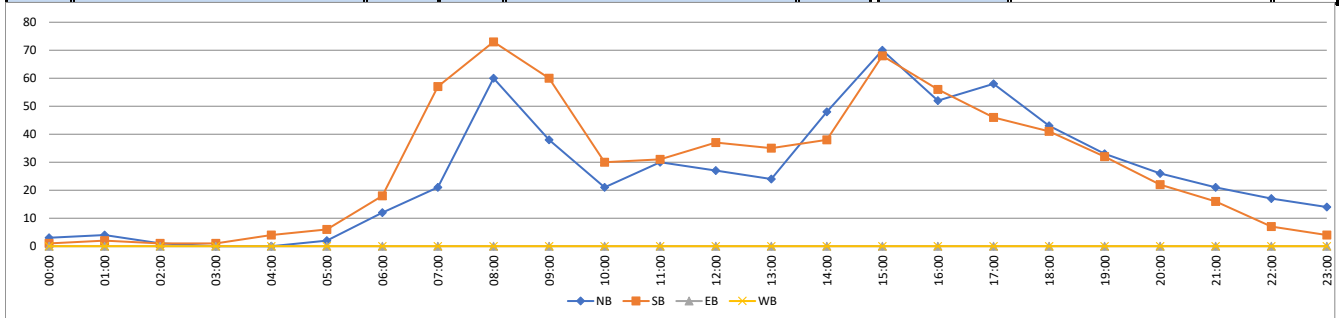
VOLUME

Lastner Ln Bet. Rosewood Dr & Ivy Ln

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_008

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					625	686	0	0	1,311								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	1	0			1	12:00	4	14			18	00:00	01:00	3	1		4
00:15	0	1			1	12:15	6	9			15	01:00	02:00	4	2		6
00:30	2	0			2	12:30	8	8			16	02:00	03:00	1	1		2
00:45	0	0			0	12:45	9	6			15	03:00	04:00	0	1		1
01:00	1	1			2	13:00	6	9			15	04:00	05:00	0	4		4
01:15	3	0			3	13:15	4	8			12	05:00	06:00	2	6		8
01:30	0	0			0	13:30	6	6			12	06:00	07:00	12	18		30
01:45	0	1			1	13:45	8	12			20	07:00	08:00	21	57		78
02:00	0	0			0	14:00	10	11			21	08:00	09:00	60	73		133
02:15	0	1			1	14:15	10	6			16	09:00	10:00	38	60		98
02:30	0	0			0	14:30	11	13			24	10:00	11:00	21	30		51
02:45	1	0			1	14:45	17	8			25	11:00	12:00	30	31		61
03:00	0	0			0	15:00	17	13			30	12:00	13:00	27	37		64
03:15	0	0			0	15:15	25	23			48	13:00	14:00	24	35		59
03:30	0	1			1	15:30	13	23			36	14:00	15:00	48	38		86
03:45	0	0			0	15:45	15	9			24	15:00	16:00	70	68		138
04:00	0	0			0	16:00	17	17			34	16:00	17:00	52	56		108
04:15	0	1			1	16:15	15	9			24	17:00	18:00	58	46		104
04:30	0	1			1	16:30	11	17			28	18:00	19:00	43	41		84
04:45	0	2			2	16:45	9	13			22	19:00	20:00	33	32		65
05:00	0	1			1	17:00	14	12			26	20:00	21:00	26	22		48
05:15	0	1			1	17:15	17	14			31	21:00	22:00	21	16		37
05:30	0	0			0	17:30	8	9			17	22:00	23:00	17	7		24
05:45	2	4			6	17:45	19	11			30	23:00	00:00	14	4		18
06:00	2	4			6	18:00	17	11			28	STATISTICS					
06:15	1	2			3	18:15	11	12			23		NB	SB	EB	WB	TOTAL
06:30	5	6			11	18:30	8	9			17	Peak Period	00:00 to 12:00				
06:45	4	6			10	18:45	7	9			16	Volume	192	284			476
07:00	5	14			19	19:00	9	6			15	Peak Hour	8:15	8:15			8:15
07:15	6	13			19	19:15	6	6			12	Peak Volume	66	88			154
07:30	2	19			21	19:30	9	12			21	Peak Hour Factor	0.825	0.880			0.917
07:45	8	11			19	19:45	9	8			17	Peak Period	12:00 to 00:00				
08:00	11	10			21	20:00	9	7			16	Volume	433	402			835
08:15	13	22			35	20:15	6	5			11	Peak Hour	14:45	15:15			15:15
08:30	16	19			35	20:30	5	6			11	Peak Volume	72	72			142
08:45	20	22			42	20:45	6	4			10	Peak Hour Factor	0.720	0.783			0.740
09:00	17	25			42	21:00	4	7			11	Peak Period	07:00 to 09:00				
09:15	7	17			24	21:15	6	3			9	Volume	81	130			211
09:30	8	8			16	21:30	8	1			9	Peak Hour	8:00	8:00			8:00
09:45	6	10			16	21:45	3	5			8	Peak Volume	60	73			133
10:00	4	6			10	22:00	5	0			5	Peak Hour Factor	0.750	0.830			0.792
10:15	7	9			16	22:15	5	2			7	Peak Period	16:00 to 18:00				
10:30	4	11			15	22:30	5	1			6	Volume	110	102			212
10:45	6	4			10	22:45	2	4			6	Peak Hour	17:00	16:00			16:00
11:00	5	8			13	23:00	2	1			3	Peak Volume	58	56			108
11:15	10	7			17	23:15	6	0			6	Peak Hour Factor	0.763	0.824			0.794
11:30	7	9			16	23:30	3	2			5						
11:45	8	7			15	23:45	3	1			4						
TOTALS	192	284	0	0	476	TOTALS	433	402	0	0	835						
SPLIT %	40%	60%	0%	0%	36%	SPLIT %	52%	48%	0%	0%	64%						



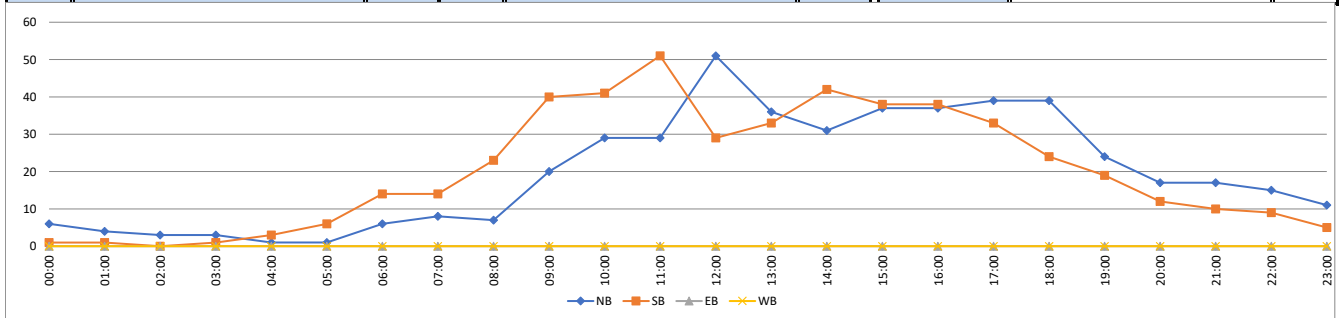
VOLUME

Lastner Ln Bet. Rosewood Dr & Ivy Ln

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_008

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					471	487	0	0	958									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	2	0			2	12:00	16	6			22	00:00	01:00	6	1			7
00:15	2	0			2	12:15	12	10			22	01:00	02:00	4	1			5
00:30	1	1			2	12:30	14	6			20	02:00	03:00	3	0			3
00:45	1	0			1	12:45	9	7			16	03:00	04:00	3	1			4
01:00	2	0			2	13:00	10	11			21	04:00	05:00	1	3			4
01:15	0	0			0	13:15	7	13			20	05:00	06:00	1	6			7
01:30	0	0			0	13:30	11	4			15	06:00	07:00	6	14			20
01:45	2	1			3	13:45	8	5			13	07:00	08:00	8	14			22
02:00	1	0			1	14:00	10	13			23	08:00	09:00	7	23			30
02:15	1	0			1	14:15	6	12			18	09:00	10:00	20	40			60
02:30	0	0			0	14:30	7	6			13	10:00	11:00	29	41			70
02:45	1	0			1	14:45	8	11			19	11:00	12:00	29	51			80
03:00	0	0			0	15:00	6	7			13	12:00	13:00	51	29			80
03:15	2	0			2	15:15	15	14			29	13:00	14:00	36	33			69
03:30	0	0			0	15:30	6	8			14	14:00	15:00	31	42			73
03:45	1	1			2	15:45	10	9			19	15:00	16:00	37	38			75
04:00	1	0			1	16:00	10	10			20	16:00	17:00	37	38			75
04:15	0	0			0	16:15	8	12			20	17:00	18:00	39	33			72
04:30	0	2			2	16:30	10	8			18	18:00	19:00	39	24			63
04:45	0	1			1	16:45	9	8			17	19:00	20:00	24	19			43
05:00	0	1			1	17:00	12	11			23	20:00	21:00	17	12			29
05:15	1	2			3	17:15	12	12			24	21:00	22:00	17	10			27
05:30	0	0			0	17:30	8	4			12	22:00	23:00	15	9			24
05:45	0	3			3	17:45	7	6			13	23:00	00:00	11	5			16
06:00	0	0			0	18:00	15	7			22	STATISTICS						
06:15	2	3			5	18:15	11	3			14		NB	SB	EB	WB	TOTAL	
06:30	1	4			5	18:30	5	7			12	Peak Period	00:00 to 12:00					
06:45	3	7			10	18:45	8	7			15	Volume	117	195			312	
07:00	3	6			9	19:00	9	9			18	Peak Hour	10:30	10:45			10:30	
07:15	2	5			7	19:15	6	8			14	Peak Volume	35	52			86	
07:30	2	1			3	19:30	5	2			7	Peak Hour Factor	0.729	0.813			0.796	
07:45	1	2			3	19:45	4	0			4	Peak Period	12:00 to 00:00					
08:00	2	6			8	20:00	7	5			12	Volume	354	292			646	
08:15	2	4			6	20:15	3	4			7	Peak Hour	12:00	14:00			15:15	
08:30	2	9			11	20:30	6	3			9	Peak Volume	51	42			82	
08:45	1	4			5	20:45	1	0			1	Peak Hour Factor	0.797	0.808			0.707	
09:00	8	12			20	21:00	7	3			10	Peak Period	07:00 to 09:00					
09:15	4	7			11	21:15	5	5			10	Volume	15	37			52	
09:30	6	12			18	21:30	1	2			3	Peak Hour	7:00	8:00			8:00	
09:45	2	9			11	21:45	4	0			4	Peak Volume	8	23			30	
10:00	7	7			14	22:00	6	5			11	Peak Hour Factor	0.667	0.639			0.682	
10:15	3	9			12	22:15	2	1			3	Peak Period	16:00 to 18:00					
10:30	12	15			27	22:30	5	2			7	Volume	76	71			147	
10:45	7	10			17	22:45	2	1			3	Peak Hour	16:30	16:15			16:30	
11:00	9	14			23	23:00	4	1			5	Peak Volume	43	39			82	
11:15	7	12			19	23:15	4	2			6	Peak Hour Factor	0.896	0.813			0.854	
11:30	5	16			21	23:30	2	1			3							
11:45	8	9			17	23:45	1	1			2							
TOTALS	117	195	0	0	312	TOTALS	354	292	0	0	646							
SPLIT %	38%	63%	0%	0%	33%	SPLIT %	55%	45%	0%	0%	67%							



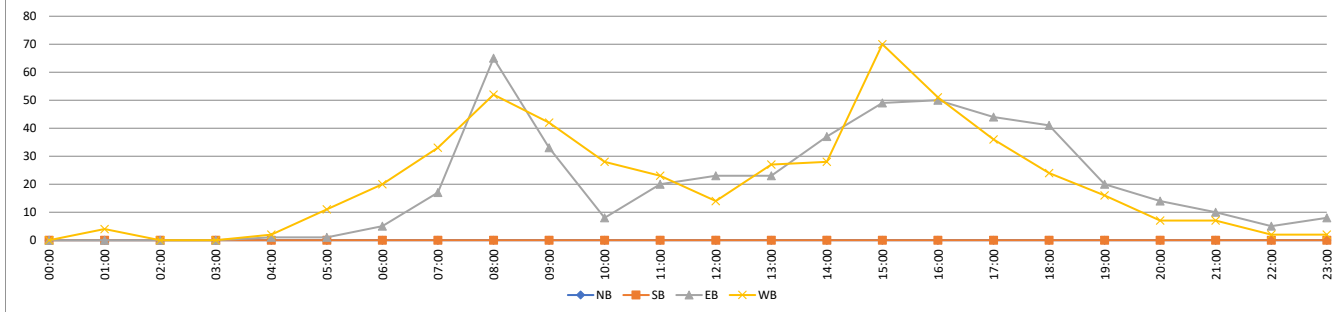
VOLUME

Greenhill Rd W/O Research Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_009

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS							
						0	0	474	499	973								
15-Minutes Interval												Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00			0	0	0	12:00			7	2	9	00:00	01:00			0	0	0
00:15			0	0	0	12:15			3	2	5	01:00	02:00			0	4	4
00:30			0	0	0	12:30			6	4	10	02:00	03:00			0	0	0
00:45			0	0	0	12:45			7	6	13	03:00	04:00			0	0	0
01:00			0	1	1	13:00			10	10	20	04:00	05:00			1	2	3
01:15			0	0	0	13:15			3	6	9	05:00	06:00			1	11	12
01:30			0	2	2	13:30			4	7	11	06:00	07:00			5	20	25
01:45			0	1	1	13:45			6	4	10	07:00	08:00			17	33	50
02:00			0	0	0	14:00			5	5	10	08:00	09:00			65	52	117
02:15			0	0	0	14:15			8	8	16	09:00	10:00			33	42	75
02:30			0	0	0	14:30			12	6	18	10:00	11:00			8	28	36
02:45			0	0	0	14:45			12	9	21	11:00	12:00			20	23	43
03:00			0	0	0	15:00			15	14	29	12:00	13:00			23	14	37
03:15			0	0	0	15:15			15	19	34	13:00	14:00			23	27	50
03:30			0	0	0	15:30			8	19	27	14:00	15:00			37	28	65
03:45			0	0	0	15:45			11	18	29	15:00	16:00			49	70	119
04:00			0	0	0	16:00			19	11	30	16:00	17:00			50	51	101
04:15			0	0	0	16:15			10	13	23	17:00	18:00			44	36	80
04:30			0	0	0	16:30			12	17	29	18:00	19:00			41	24	65
04:45			1	2	3	16:45			9	10	19	19:00	20:00			20	16	36
05:00			0	3	3	17:00			10	10	20	20:00	21:00			14	7	21
05:15			0	1	1	17:15			12	9	21	21:00	22:00			10	7	17
05:30			0	4	4	17:30			10	9	19	22:00	23:00			5	2	7
05:45			1	3	4	17:45			12	8	20	23:00	00:00			8	2	10
06:00			1	5	6	18:00			13	10	23	STATISTICS						
06:15			0	3	3	18:15			5	8	13		NB	SB	EB	WB	TOTAL	
06:30			1	6	7	18:30			16	4	20	Peak Period	00:00 to 12:00					
06:45			3	6	9	18:45			7	2	9	Volume			150	215	365	
07:00			6	9	15	19:00			5	5	10	Peak Hour			8:15	8:30	8:30	
07:15			3	5	8	19:15			2	1	3	Peak Volume			68	65	129	
07:30			2	9	11	19:30			4	7	11	Peak Hour Factor			0.654	0.903	0.733	
07:45			6	10	16	19:45			9	3	12	Peak Period	12:00 to 00:00					
08:00			12	10	22	20:00			3	2	5	Volume			324	284	608	
08:15			13	11	24	20:15			4	1	5	Peak Hour			14:30	15:00	15:15	
08:30			14	13	27	20:30			2	1	3	Peak Volume			54	70	120	
08:45			26	18	44	20:45			5	3	8	Peak Hour Factor			0.900	0.921	0.882	
09:00			15	17	32	21:00			2	3	5	Peak Period	07:00 to 09:00					
09:15			9	17	26	21:15			4	0	4	Volume			82	85	167	
09:30			5	6	11	21:30			4	2	6	Peak Hour			8:00	8:00	8:00	
09:45			4	2	6	21:45			0	2	2	Peak Volume			65	52	117	
10:00			1	6	7	22:00			1	0	1	Peak Hour Factor			0.625	0.722	0.665	
10:15			2	8	10	22:15			2	0	2	Peak Period	16:00 to 18:00					
10:30			3	9	12	22:30			2	0	2	Volume			94	87	181	
10:45			2	5	7	22:45			0	2	2	Peak Hour			16:00	16:00	16:00	
11:00			4	4	8	23:00			3	1	4	Peak Volume			50	51	101	
11:15			5	5	10	23:15			2	1	3	Peak Hour Factor			0.658	0.750	0.842	
11:30			6	8	14	23:30			1	0	1							
11:45			5	6	11	23:45			2	0	2							
TOTALS	0	0	150	215	365	TOTALS	0	0	324	284	608							
SPLIT %	0%	0%	41%	59%	38%	SPLIT %	0%	0%	53%	47%	62%							



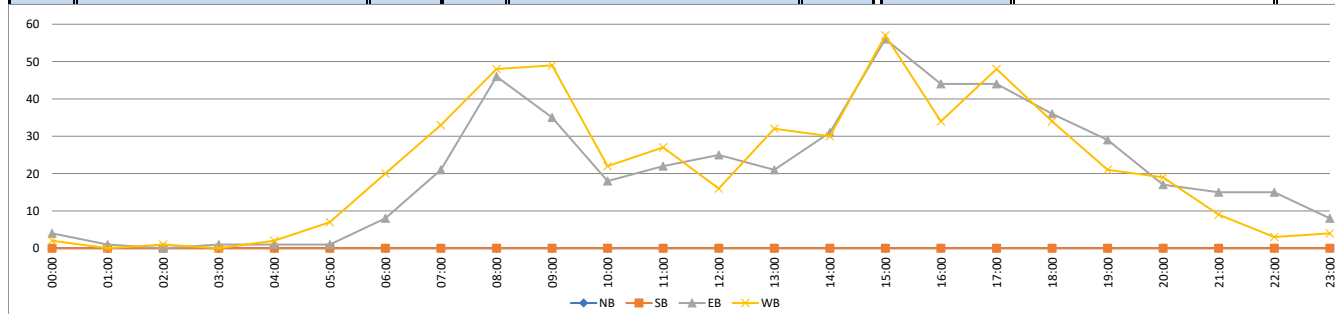
VOLUME

Greenhill Rd W/O Research Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_009

DAILY TOTALS											DAILY TOTALS							
						NB	SB	EB	WB	Total								
						0	0	499	518	1,017								
15-Minutes Interval												Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00			2	0	2	12:00			4	5	9	00:00	01:00			4	2	6
00:15			0	0	0	12:15			5	5	10	01:00	02:00			1	0	1
00:30			2	1	3	12:30			9	2	11	02:00	03:00			0	1	1
00:45			0	1	1	12:45			7	4	11	03:00	04:00			1	0	1
01:00			1	0	1	13:00			2	13	15	04:00	05:00			1	2	3
01:15			0	0	0	13:15			6	4	10	05:00	06:00			1	7	8
01:30			0	0	0	13:30			7	8	15	06:00	07:00			8	20	28
01:45			0	0	0	13:45			6	7	13	07:00	08:00			21	33	54
02:00			0	0	0	14:00			5	8	13	08:00	09:00			46	48	94
02:15			0	0	0	14:15			5	4	9	09:00	10:00			35	49	84
02:30			0	0	0	14:30			6	8	14	10:00	11:00			18	22	40
02:45			0	1	1	14:45			15	10	25	11:00	12:00			22	27	49
03:00			0	0	0	15:00			20	9	29	12:00	13:00			25	16	41
03:15			1	0	1	15:15			13	22	35	13:00	14:00			21	32	53
03:30			0	0	0	15:30			9	11	20	14:00	15:00			31	30	61
03:45			0	0	0	15:45			14	15	29	15:00	16:00			56	57	113
04:00			0	0	0	16:00			9	8	17	16:00	17:00			44	34	78
04:15			0	0	0	16:15			13	7	20	17:00	18:00			44	48	92
04:30			0	1	1	16:30			8	12	20	18:00	19:00			36	34	70
04:45			1	1	2	16:45			14	7	21	19:00	20:00			29	21	50
05:00			0	2	2	17:00			20	10	30	20:00	21:00			17	19	36
05:15			0	2	2	17:15			8	19	27	21:00	22:00			15	9	24
05:30			1	1	2	17:30			9	8	17	22:00	23:00			15	3	18
05:45			0	2	2	17:45			7	11	18	23:00	00:00			8	4	12
06:00			0	1	1	18:00			9	9	18	STATISTICS						
06:15			1	3	4	18:15			11	8	19		NB	SB	EB	WB	TOTAL	
06:30			3	7	10	18:30			11	8	19	Peak Period	00:00 to 12:00					
06:45			4	9	13	18:45			5	9	14	Volume			158	211	369	
07:00			5	7	12	19:00			6	8	14	Peak Hour			8:45	8:15	8:15	
07:15			3	7	10	19:15			11	5	16	Peak Volume			49	62	110	
07:30			9	11	20	19:30			8	2	10	Peak Hour Factor			0.721	0.620	0.724	
07:45			4	8	12	19:45			4	6	10	Peak Period	12:00 to 00:00					
08:00			11	11	22	20:00			5	5	10	Volume			341	307	648	
08:15			10	13	23	20:15			5	8	13	Peak Hour			14:45	15:00	15:00	
08:30			8	12	20	20:30			2	3	5	Peak Volume			57	57	113	
08:45			17	12	29	20:45			5	3	8	Peak Hour Factor			0.713	0.648	0.807	
09:00			13	25	38	21:00			5	1	6	Peak Period	07:00 to 09:00					
09:15			10	10	20	21:15			4	5	9	Volume			67	81	148	
09:30			9	9	18	21:30			0	2	2	Peak Hour			8:00	8:00	8:00	
09:45			3	5	8	21:45			6	1	7	Peak Volume			46	48	94	
10:00			5	5	10	22:00			5	1	6	Peak Hour Factor			0.676	0.923	0.810	
10:15			3	9	12	22:15			2	1	3	Peak Period	16:00 to 18:00					
10:30			4	4	8	22:30			5	1	6	Volume			88	82	170	
10:45			6	4	10	22:45			3	0	3	Peak Hour			16:15	16:30	16:30	
11:00			4	6	10	23:00			3	2	5	Peak Volume			55	48	98	
11:15			9	6	15	23:15			3	0	3	Peak Hour Factor			0.688	0.632	0.817	
11:30			5	11	16	23:30			1	1	2							
11:45			4	4	8	23:45			1	1	2							
TOTALS	0	0	158	211	369	TOTALS	0	0	341	307	648							
SPLIT %	0%	0%	43%	57%	36%	SPLIT %	0%	0%	53%	47%	64%							



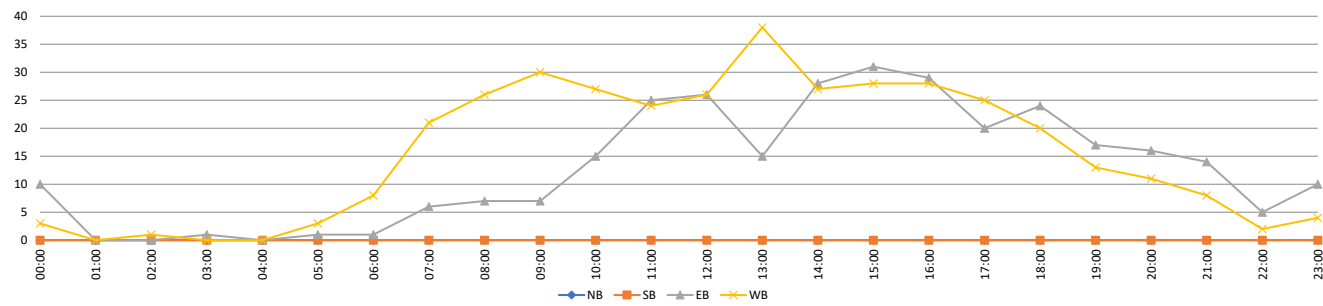
VOLUME

Greenhill Rd W/O Research Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_009

DAILY TOTALS											DAILY TOTALS							
						NB	SB	EB	WB	Total								
						0	0	308	373	681								
15-Minutes Interval												Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00			4	0	4	12:00			12	4	16	00:00	01:00			10	3	13
00:15			4	2	6	12:15			4	7	11	01:00	02:00			0	0	0
00:30			1	1	2	12:30			4	7	11	02:00	03:00			0	1	1
00:45			1	0	1	12:45			6	8	14	03:00	04:00			1	0	1
01:00			0	0	0	13:00			3	14	17	04:00	05:00			0	0	0
01:15			0	0	0	13:15			5	10	15	05:00	06:00			1	3	4
01:30			0	0	0	13:30			4	6	10	06:00	07:00			1	8	9
01:45			0	0	0	13:45			3	8	11	07:00	08:00			6	21	27
02:00			0	0	0	14:00			10	7	17	08:00	09:00			7	26	33
02:15			0	1	1	14:15			6	5	11	09:00	10:00			7	30	37
02:30			0	0	0	14:30			6	6	12	10:00	11:00			15	27	42
02:45			0	0	0	14:45			6	9	15	11:00	12:00			25	24	49
03:00			0	0	0	15:00			9	7	16	12:00	13:00			26	26	52
03:15			1	0	1	15:15			5	7	12	13:00	14:00			15	38	53
03:30			0	0	0	15:30			7	8	15	14:00	15:00			28	27	55
03:45			0	0	0	15:45			10	6	16	15:00	16:00			31	28	59
04:00			0	0	0	16:00			9	8	17	16:00	17:00			29	28	57
04:15			0	0	0	16:15			6	8	14	17:00	18:00			20	25	45
04:30			0	0	0	16:30			3	9	12	18:00	19:00			24	20	44
04:45			0	0	0	16:45			11	3	14	19:00	20:00			17	13	30
05:00			1	0	1	17:00			7	8	15	20:00	21:00			16	11	27
05:15			0	2	2	17:15			3	5	8	21:00	22:00			14	8	22
05:30			0	0	0	17:30			5	5	10	22:00	23:00			5	2	7
05:45			0	1	1	17:45			5	7	12	23:00	00:00			10	4	14
06:00			0	3	3	18:00			8	5	13	STATISTICS						
06:15			1	1	2	18:15			7	7	14		NB	SB	EB	WB	TOTAL	
06:30			0	4	4	18:30			5	5	10	Peak Period	00:00 to 12:00					
06:45			0	0	0	18:45			4	3	7	Volume			73	143	216	
07:00			2	2	4	19:00			7	3	10	Peak Hour			11:00	8:45	11:00	
07:15			2	9	11	19:15			1	2	3	Peak Volume			25	31	49	
07:30			1	6	7	19:30			9	4	13	Peak Hour Factor			0.781	0.775	0.875	
07:45			1	4	5	19:45			0	4	4	Peak Period	12:00 to 00:00					
08:00			1	6	7	20:00			3	3	6	Volume			235	230	465	
08:15			0	4	4	20:15			5	1	6	Peak Hour			15:30	12:30	15:30	
08:30			4	6	10	20:30			4	5	9	Peak Volume			32	39	62	
08:45			2	10	12	20:45			4	2	6	Peak Hour Factor			0.800	0.696	0.912	
09:00			2	8	10	21:00			4	1	5	Peak Period	07:00 to 09:00					
09:15			2	6	8	21:15			4	2	6	Volume			13	47	60	
09:30			2	7	9	21:30			1	2	3	Peak Hour			8:00	8:00	8:00	
09:45			1	9	10	21:45			5	3	8	Peak Volume			7	26	33	
10:00			7	7	14	22:00			1	1	2	Peak Hour Factor			0.438	0.650	0.688	
10:15			2	7	9	22:15			2	1	3	Peak Period	16:00 to 18:00					
10:30			4	6	10	22:30			1	0	1	Volume			49	53	102	
10:45			2	7	9	22:45			1	0	1	Peak Hour			16:00	16:00	16:00	
11:00			8	5	13	23:00			2	1	3	Peak Volume			29	28	57	
11:15			6	5	11	23:15			3	2	5	Peak Hour Factor			0.659	0.778	0.838	
11:30			4	7	11	23:30			3	1	4							
11:45			7	7	14	23:45			2	0	2							
TOTALS	0	0	73	143	216	TOTALS	0	0	235	230	465							
SPLIT %	0%	0%	34%	66%	32%	SPLIT %	0%	0%	51%	49%	68%							



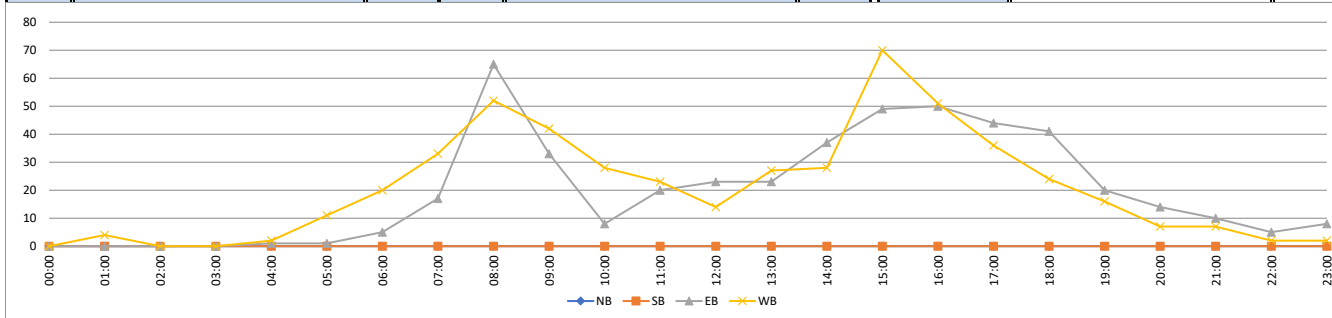
VOLUME

Greenhill Rd W/O Research Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_009

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS							
						0	0	474	499	973								
15-Minutes Interval												Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00			0	0	0	12:00			7	2	9	00:00	01:00			0	0	0
00:15			0	0	0	12:15			3	2	5	01:00	02:00			0	4	4
00:30			0	0	0	12:30			6	4	10	02:00	03:00			0	0	0
00:45			0	0	0	12:45			7	6	13	03:00	04:00			0	0	0
01:00			0	1	1	13:00			10	10	20	04:00	05:00			1	2	3
01:15			0	0	0	13:15			3	6	9	05:00	06:00			1	11	12
01:30			0	2	2	13:30			4	7	11	06:00	07:00			5	20	25
01:45			0	1	1	13:45			6	4	10	07:00	08:00			17	33	50
02:00			0	0	0	14:00			5	5	10	08:00	09:00			65	52	117
02:15			0	0	0	14:15			8	8	16	09:00	10:00			33	42	75
02:30			0	0	0	14:30			12	6	18	10:00	11:00			8	28	36
02:45			0	0	0	14:45			12	9	21	11:00	12:00			20	23	43
03:00			0	0	0	15:00			15	14	29	12:00	13:00			23	14	37
03:15			0	0	0	15:15			15	19	34	13:00	14:00			23	27	50
03:30			0	0	0	15:30			8	19	27	14:00	15:00			37	28	65
03:45			0	0	0	15:45			11	18	29	15:00	16:00			49	70	119
04:00			0	0	0	16:00			19	11	30	16:00	17:00			50	51	101
04:15			0	0	0	16:15			10	13	23	17:00	18:00			44	36	80
04:30			0	0	0	16:30			12	17	29	18:00	19:00			41	24	65
04:45			1	2	3	16:45			9	10	19	19:00	20:00			20	16	36
05:00			0	3	3	17:00			10	10	20	20:00	21:00			14	7	21
05:15			0	1	1	17:15			12	9	21	21:00	22:00			10	7	17
05:30			0	4	4	17:30			10	9	19	22:00	23:00			5	2	7
05:45			1	3	4	17:45			12	8	20	23:00	00:00			8	2	10
06:00			1	5	6	18:00			13	10	23	STATISTICS						
06:15			0	3	3	18:15			5	8	13		NB	SB	EB	WB	TOTAL	
06:30			1	6	7	18:30			16	4	20	Peak Period	00:00 to 12:00					
06:45			3	6	9	18:45			7	2	9	Volume			150	215	365	
07:00			6	9	15	19:00			5	5	10	Peak Hour			8:15	8:30	8:30	
07:15			3	5	8	19:15			2	1	3	Peak Volume			68	65	129	
07:30			2	9	11	19:30			4	7	11	Peak Hour Factor			0.654	0.903	0.733	
07:45			6	10	16	19:45			9	3	12	Peak Period	12:00 to 00:00					
08:00			12	10	22	20:00			3	2	5	Volume			324	284	608	
08:15			13	11	24	20:15			4	1	5	Peak Hour			14:30	15:00	15:15	
08:30			14	13	27	20:30			2	1	3	Peak Volume			54	70	120	
08:45			26	18	44	20:45			5	3	8	Peak Hour Factor			0.900	0.921	0.882	
09:00			15	17	32	21:00			2	3	5	Peak Period	07:00 to 09:00					
09:15			9	17	26	21:15			4	0	4	Volume			82	85	167	
09:30			5	6	11	21:30			4	2	6	Peak Hour			8:00	8:00	8:00	
09:45			4	2	6	21:45			0	2	2	Peak Volume			65	52	117	
10:00			1	6	7	22:00			1	0	1	Peak Hour Factor			0.625	0.722	0.665	
10:15			2	8	10	22:15			2	0	2	Peak Period	16:00 to 18:00					
10:30			3	9	12	22:30			2	0	2	Volume			94	87	181	
10:45			2	5	7	22:45			0	2	2	Peak Hour			16:00	16:00	16:00	
11:00			4	4	8	23:00			3	1	4	Peak Volume			50	51	101	
11:15			5	5	10	23:15			2	1	3	Peak Hour Factor			0.658	0.750	0.842	
11:30			6	8	14	23:30			1	0	1							
11:45			5	6	11	23:45			2	0	2							
TOTALS	0	0	150	215	365	TOTALS	0	0	324	284	608							
SPLIT %	0%	0%	41%	59%	38%	SPLIT %	0%	0%	53%	47%	62%							



SPEED Greenhill Rd W/O Research Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_009

Time	EASTBOUND													Total	WESTBOUND													Total	TOTALS												
	5	15	20	25	30	35	40	45	50	55	60	65	70		5	15	20	25	30	35	40	45	50	55	60	65	70		5	15	20	25	30	35	40	45	50	55	60	65	70
	15	20	25	30	35	40	45	50	55	60	65	70	15		20	25	30	35	40	45	50	55	60	65	70	15	20		25	30	35	40	45	50	55	60	65	70			
00:00	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	2	0	0	0	0	0	0	0	0	2	0	0	6	0	0	0	0	0	0	0	0	0	0	0	6
Totals	11	127	232	101	25	3	0	0	0	0	0	0	0	499	20	104	275	107	12	0	0	0	0	0	0	518	31	231	507	208	37	3	0	0	0	0	0	0	0	1,017	
% of Totals	2%	25%	46%	20%	5%	1%							100%	4%	20%	53%	21%	2%						100%	3%	23%	50%	20%	4%	0%									100%		

STATISTICS	Time	EASTBOUND													Total	WESTBOUND													Total	ADT													
		5	15	20	25	30	35	40	45	50	55	60	65	70		5	15	20	25	30	35	40	45	50	55	60	65	70			5	15	20	25	30	35	40	45	50	55	60	65	70
		15	20	25	30	35	40	45	50	55	60	65	70	15		20	25	30	35	40	45	50	55	60	65	70	15	20			25	30	35	40	45	50	55	60	65	70			
00:00 - 12:00	6	33	77	31	10	1	0	0	0	0	0	0	158	8	23	117	56	7	0	0	0	0	0	211	14	56	194	87	17	3	1	0	0	0	0	0	369						
	1%	7%	15%	6%	2%	0%	0%	0%	0%	0%	0%	0%	32%	2%	5%	23%	11%	1%	0%	0%	0%	0%	42%	3%	11%	39%	17%	3%	0%	0%	0%	0%	0%	0%	0%	74%							
	10:30	8:45	8:00	8:45	7:30	6:45	0:00	0:00	0:00	0:00	0:00	0:00	8:45	6:00	9:30	8:15	8:30	7:15	0:00	0:00	0:00	0:00	8:15	10:45	8:45	8:15	8:30	7:30	6:45	0:00	0:00	0:00	0:00	0:00	0:00	8:15							
	3	14	25	11	5	1	0	0	0	0	0	0	49	3	8	39	17	2	0	0	0	0	62	5	21	64	26	7	1	0	0	0	0	0	110								
16:00 - 18:00	1	22	43	17	4	1	0	0	0	0	0	88	1	23	40	14	4	0	0	0	0	82	2	45	83	31	8	1	0	0	0	0	0	0	170								
	0%	4%	9%	3%	1%	0%	0%	0%	0%	0%	0%	18%	0%	5%	8%	3%	1%	0%	0%	0%	0%	16%	0%	9%	17%	6%	2%	0%	0%	0%	0%	0%	0%	0%	34%								
	16:00	16:45	16:15	16:00	16:15	16:15	16:00	16:00	16:00	16:00	16:00	16:15	16:00	17:00	16:30	16:00	16:00	16:00	16:00	16:00	16:00	16:30	16:00	16:45	16:30	16:00	16:15	16:15	16:00	16:00	16:00	16:00	16:00	16:30									
	1	16	28	12	4	1	0	0	0	0	0	55	1	16	24	7	2	0	0	0	0	48	2	31	46	19	6	0	0	0	0	0	0	0	98								

Direction	Percentiles					ADT
	15th	50th	Average	85th	95th	
EASTBOUND	18	22	23	28	31	499
WESTBOUND	18	22	22	27	29	318
TOTALS	18	22	22	27	30	1017

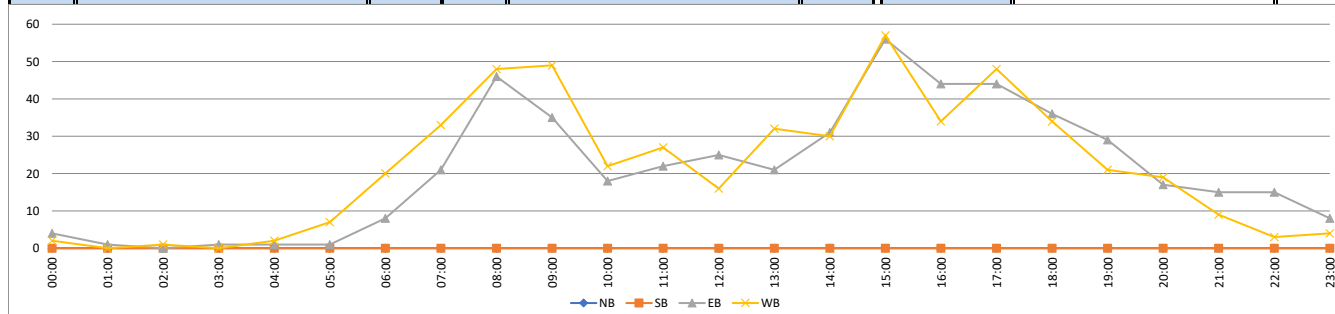
VOLUME

Greenhill Rd W/O Research Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_009

DAILY TOTALS											DAILY TOTALS							
						NB	SB	EB	WB	Total								
						0	0	499	518	1,017								
15-Minutes Interval												Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00			2	0	2	12:00			4	5	9	00:00	01:00			4	2	6
00:15			0	0	0	12:15			5	5	10	01:00	02:00			1	0	1
00:30			2	1	3	12:30			9	2	11	02:00	03:00			0	1	1
00:45			0	1	1	12:45			7	4	11	03:00	04:00			1	0	1
01:00			1	0	1	13:00			2	13	15	04:00	05:00			1	2	3
01:15			0	0	0	13:15			6	4	10	05:00	06:00			1	7	8
01:30			0	0	0	13:30			7	8	15	06:00	07:00			8	20	28
01:45			0	0	0	13:45			6	7	13	07:00	08:00			21	33	54
02:00			0	0	0	14:00			5	8	13	08:00	09:00			46	48	94
02:15			0	0	0	14:15			5	4	9	09:00	10:00			35	49	84
02:30			0	0	0	14:30			6	8	14	10:00	11:00			18	22	40
02:45			0	1	1	14:45			15	10	25	11:00	12:00			22	27	49
03:00			0	0	0	15:00			20	9	29	12:00	13:00			25	16	41
03:15			1	0	1	15:15			13	22	35	13:00	14:00			21	32	53
03:30			0	0	0	15:30			9	11	20	14:00	15:00			31	30	61
03:45			0	0	0	15:45			14	15	29	15:00	16:00			56	57	113
04:00			0	0	0	16:00			9	8	17	16:00	17:00			44	34	78
04:15			0	0	0	16:15			13	7	20	17:00	18:00			44	48	92
04:30			0	1	1	16:30			8	12	20	18:00	19:00			36	34	70
04:45			1	1	2	16:45			14	7	21	19:00	20:00			29	21	50
05:00			0	2	2	17:00			20	10	30	20:00	21:00			17	19	36
05:15			0	2	2	17:15			8	19	27	21:00	22:00			15	9	24
05:30			1	1	2	17:30			9	8	17	22:00	23:00			15	3	18
05:45			0	2	2	17:45			7	11	18	23:00	00:00			8	4	12
06:00			0	1	1	18:00			9	9	18	STATISTICS						
06:15			1	3	4	18:15			11	8	19		NB	SB	EB	WB	TOTAL	
06:30			3	7	10	18:30			11	8	19	Peak Period	00:00 to 12:00					
06:45			4	9	13	18:45			5	9	14	Volume			158	211	369	
07:00			5	7	12	19:00			6	8	14	Peak Hour			8:45	8:15	8:15	
07:15			3	7	10	19:15			11	5	16	Peak Volume			49	62	110	
07:30			9	11	20	19:30			8	2	10	Peak Hour Factor			0.721	0.620	0.724	
07:45			4	8	12	19:45			4	6	10	Peak Period	12:00 to 00:00					
08:00			11	11	22	20:00			5	5	10	Volume			341	307	648	
08:15			10	13	23	20:15			5	8	13	Peak Hour			14:45	15:00	15:00	
08:30			8	12	20	20:30			2	3	5	Peak Volume			57	57	113	
08:45			17	12	29	20:45			5	3	8	Peak Hour Factor			0.713	0.648	0.807	
09:00			13	25	38	21:00			5	1	6	Peak Period	07:00 to 09:00					
09:15			10	10	20	21:15			4	5	9	Volume			67	81	148	
09:30			9	9	18	21:30			0	2	2	Peak Hour			8:00	8:00	8:00	
09:45			3	5	8	21:45			6	1	7	Peak Volume			46	48	94	
10:00			5	5	10	22:00			5	1	6	Peak Hour Factor			0.676	0.923	0.810	
10:15			3	9	12	22:15			2	1	3	Peak Period	16:00 to 18:00					
10:30			4	4	8	22:30			5	1	6	Volume			88	82	170	
10:45			6	4	10	22:45			3	0	3	Peak Hour			16:15	16:30	16:30	
11:00			4	6	10	23:00			3	2	5	Peak Volume			55	48	98	
11:15			9	6	15	23:15			3	0	3	Peak Hour Factor			0.688	0.632	0.817	
11:30			5	11	16	23:30			1	1	2							
11:45			4	4	8	23:45			1	1	2							
TOTALS	0	0	158	211	369	TOTALS	0	0	341	307	648							
SPLIT %	0%	0%	43%	57%	36%	SPLIT %	0%	0%	53%	47%	64%							



SPEED
Greenhill Rd W/O Research Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_009

Table with columns for Time, EASTBOUND (5-70), Total, WESTBOUND (5-70), Total, TOTALES (5-70), and Total. Rows include 15-minute breakdowns from 12:00 to 23:45 and a final Totals row. Includes a vertical label '15-MINUTE BREAKDOWN' on the left side.

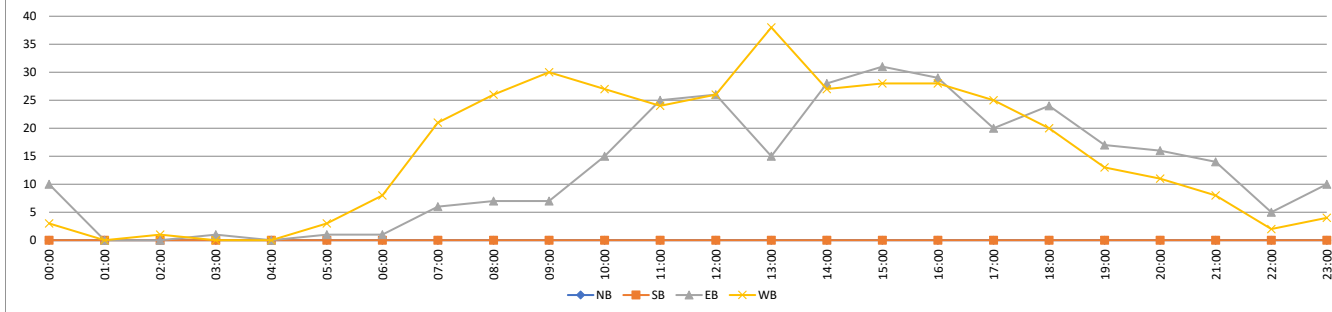
VOLUME

Greenhill Rd W/O Research Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_009

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS							
						0	0	308	373	681								
15-Minutes Interval											Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00			4	0	4	12:00			12	4	16	00:00	01:00			10	3	13
00:15			4	2	6	12:15			4	7	11	01:00	02:00			0	0	0
00:30			1	1	2	12:30			4	7	11	02:00	03:00			0	1	1
00:45			1	0	1	12:45			6	8	14	03:00	04:00			1	0	1
01:00			0	0	0	13:00			3	14	17	04:00	05:00			0	0	0
01:15			0	0	0	13:15			5	10	15	05:00	06:00			1	3	4
01:30			0	0	0	13:30			4	6	10	06:00	07:00			1	8	9
01:45			0	0	0	13:45			3	8	11	07:00	08:00			6	21	27
02:00			0	0	0	14:00			10	7	17	08:00	09:00			7	26	33
02:15			0	1	1	14:15			6	5	11	09:00	10:00			7	30	37
02:30			0	0	0	14:30			6	6	12	10:00	11:00			15	27	42
02:45			0	0	0	14:45			6	9	15	11:00	12:00			25	24	49
03:00			0	0	0	15:00			9	7	16	12:00	13:00			26	26	52
03:15			1	0	1	15:15			5	7	12	13:00	14:00			15	38	53
03:30			0	0	0	15:30			7	8	15	14:00	15:00			28	27	55
03:45			0	0	0	15:45			10	6	16	15:00	16:00			31	28	59
04:00			0	0	0	16:00			9	8	17	16:00	17:00			29	28	57
04:15			0	0	0	16:15			6	8	14	17:00	18:00			20	25	45
04:30			0	0	0	16:30			3	9	12	18:00	19:00			24	20	44
04:45			0	0	0	16:45			11	3	14	19:00	20:00			17	13	30
05:00			1	0	1	17:00			7	8	15	20:00	21:00			16	11	27
05:15			0	2	2	17:15			3	5	8	21:00	22:00			14	8	22
05:30			0	0	0	17:30			5	5	10	22:00	23:00			5	2	7
05:45			0	1	1	17:45			5	7	12	23:00	00:00			10	4	14
06:00			0	3	3	18:00			8	5	13	STATISTICS						
06:15			1	1	2	18:15			7	7	14		NB	SB	EB	WB	TOTAL	
06:30			0	4	4	18:30			5	5	10	Peak Period	00:00 to 12:00					
06:45			0	0	0	18:45			4	3	7	Volume			73	143	216	
07:00			2	2	4	19:00			7	3	10	Peak Hour			11:00	8:45	11:00	
07:15			2	9	11	19:15			1	2	3	Peak Volume			25	31	49	
07:30			1	6	7	19:30			9	4	13	Peak Hour Factor			0.781	0.775	0.875	
07:45			1	4	5	19:45			0	4	4	Peak Period	12:00 to 00:00					
08:00			1	6	7	20:00			3	3	6	Volume			235	230	465	
08:15			0	4	4	20:15			5	1	6	Peak Hour			15:30	12:30	15:30	
08:30			4	6	10	20:30			4	5	9	Peak Volume			32	39	62	
08:45			2	10	12	20:45			4	2	6	Peak Hour Factor			0.800	0.696	0.912	
09:00			2	8	10	21:00			4	1	5	Peak Period	07:00 to 09:00					
09:15			2	6	8	21:15			4	2	6	Volume			13	47	60	
09:30			2	7	9	21:30			1	2	3	Peak Hour			8:00	8:00	8:00	
09:45			1	9	10	21:45			5	3	8	Peak Volume			7	26	33	
10:00			7	7	14	22:00			1	1	2	Peak Hour Factor			0.438	0.650	0.688	
10:15			2	7	9	22:15			2	1	3	Peak Period	16:00 to 18:00					
10:30			4	6	10	22:30			1	0	1	Volume			49	53	102	
10:45			2	7	9	22:45			1	0	1	Peak Hour			16:00	16:00	16:00	
11:00			8	5	13	23:00			2	1	3	Peak Volume			29	28	57	
11:15			6	5	11	23:15			3	2	5	Peak Hour Factor			0.659	0.778	0.838	
11:30			4	7	11	23:30			3	1	4							
11:45			7	7	14	23:45			2	0	2							
TOTALS	0	0	73	143	216	TOTALS	0	0	235	230	465							
SPLIT %	0%	0%	34%	66%	32%	SPLIT %	0%	0%	51%	49%	68%							



CLASSIFICATION
Greenhill Rd N/O Greenway PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_010

Time	NORTHBOUND													SOUTHBOUND													TOTALS																	
	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total	#1	#2	#3	#4	#5	#6	#7	#8	#9	#10	#11	#12	#13	Total		
00:00	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	4
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	4
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	2	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
05:00	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	13	0	0	0	0	0	0	0	0	0	0	13	0	14	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
06:00	0	3	0	1	1	0	0	0	0	0	0	0	0	5	0	26	0	0	0	0	0	0	0	0	0	0	26	0	29	0	1	1	0	0	0	0	0	0	0	0	0	0	31	
07:00	0	16	1	0	1	0	0	0	0	0	0	0	0	18	0	42	2	1	2	0	0	0	0	0	0	47	0	58	3	1	3	0	0	0	0	0	0	0	0	0	0	65		
08:00	0	49	9	2	2	0	0	0	0	0	0	0	0	62	3	65	4	2	1	0	0	0	0	0	0	75	3	114	13	4	3	0	0	0	0	0	0	0	0	0	137			
09:00	0	27	3	3	2	0	0	0	0	0	0	0	0	35	0	36	6	4	0	0	1	0	0	0	47	0	63	9	7	2	0	1	0	0	0	0	0	0	0	82				
10:00	1	6	0	0	4	0	0	0	0	0	0	0	0	11	2	24	3	2	2	0	0	0	0	0	33	3	30	3	2	6	0	0	0	0	0	0	0	0	0	0	44			
11:00	0	19	7	0	4	0	0	0	0	0	0	0	0	30	0	25	4	0	2	0	0	0	0	0	31	0	44	11	0	6	0	0	0	0	0	0	0	0	0	0	61			
12:00	1	26	3	2	4	0	0	0	0	0	0	0	0	36	0	22	4	0	1	0	0	0	0	0	27	1	48	7	2	5	0	0	0	0	0	0	0	0	0	0	63			
13:00	0	25	2	0	3	0	0	0	0	0	0	0	0	30	1	27	3	0	1	0	0	0	0	0	32	1	52	5	0	4	0	0	0	0	0	0	0	0	0	0	62			
14:00	1	34	10	1	3	0	0	0	0	0	0	0	0	49	1	24	5	1	2	0	0	0	0	0	33	2	58	15	2	5	0	0	0	0	0	0	0	0	0	0	82			
15:00	0	47	9	3	3	0	0	0	0	0	0	0	0	62	0	65	5	1	4	0	0	0	0	0	75	0	112	14	4	7	0	0	0	0	0	0	0	0	0	137				
16:00	1	51	7	0	1	0	0	0	0	0	0	0	0	60	0	52	6	1	0	0	0	0	0	0	59	1	103	13	1	1	0	0	0	0	0	0	0	0	0	119				
17:00	0	55	2	0	2	0	0	0	0	0	0	0	0	59	0	39	2	1	2	0	0	0	0	0	44	0	94	4	1	4	0	0	0	0	0	0	0	0	0	103				
18:00	0	47	7	0	2	0	0	0	0	0	0	0	0	56	0	33	2	1	2	0	0	0	0	0	38	0	80	9	1	4	0	0	0	0	0	0	0	0	0	94				
19:00	1	32	1	0	2	0	0	0	0	0	0	0	0	36	0	20	0	0	1	0	0	0	0	0	21	1	52	1	0	3	0	0	0	0	0	0	0	0	0	57				
20:00	0	22	1	0	0	0	0	0	0	0	0	0	0	23	0	8	3	0	1	0	0	0	0	0	12	0	30	4	0	1	0	0	0	0	0	0	0	0	0	35				
21:00	0	16	1	0	1	0	0	0	0	0	0	0	0	18	0	6	2	0	0	0	0	0	0	0	8	0	22	3	0	1	0	0	0	0	0	0	0	0	0	26				
22:00	0	11	0	0	1	0	0	0	0	0	0	0	0	12	0	5	0	0	0	0	0	0	0	0	5	0	16	0	0	1	0	0	0	0	0	0	0	0	0	0	17			
23:00	0	9	1	0	0	0	0	0	0	0	0	0	0	10	0	3	1	0	0	0	0	0	0	0	4	0	12	2	0	0	0	0	0	0	0	0	0	0	0	0	14			
Totals	5	500	64	12	36	0	0	0	0	0	0	0	0	617	7	543	53	14	21	0	1	0	0	0	639	12	1,043	117	26	57	0	1	0	0	0	0	0	0	0	1,256				
% of Totals	1%	81%	10%	2%	6%									100%	1%	85%	8%	2%	3%	0%				100%	1%	83%	9%	2%	5%	0%										100%				

FHWA	CLASSIFICATION DEFINITIONS												
	#1 Motorcycles	#2 Passenger Cars	#3 2-Axle, 4-Tire Single Unit	#4 Buses	#5 2-Axle, 6-Tire Single Units	#6 3-Axle Single Units	#7 >=4-Axle Single Units	#8 <=4-Axle Single Trailers	#9 5-Axle Single Trailers	#10 >=6-Axle Single Trailers	#11 <=5-Axle Multi-Trailers	#12 6-Axle Multi-Trailers	#13 >=7-Axle Multi-Trailers
													ANY 7 OR MORE AXLE

STATISTICS	STATISTICS																																																																		
	00:00 - 12:00 %	00:00 - 12:00 Peak Hour	12:00 - 24:00 %	12:00 - 24:00 Peak Hour	07:00 - 09:00 %	07:00 - 09:00 Peak Hour	16:00 - 18:00 %	16:00 - 18:00 Peak Hour	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59
	0%	10:00	1%	12:00	0%	7:00	16:00	1	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60			

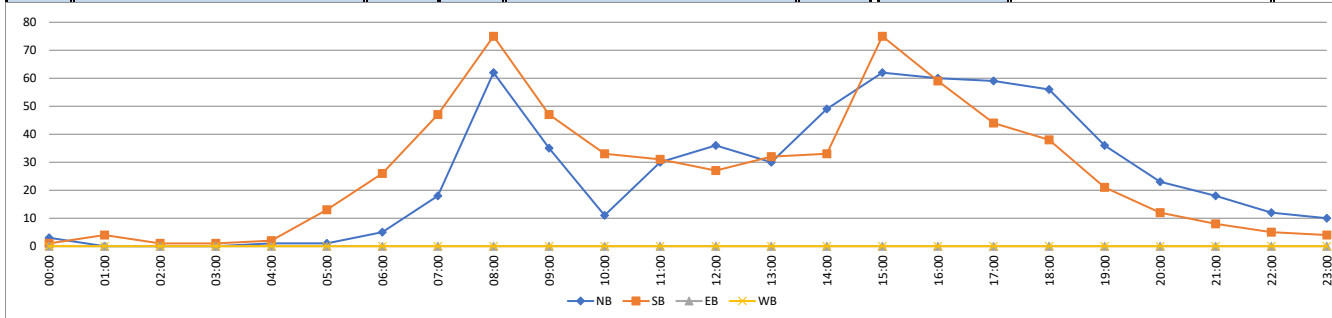
VOLUME

Greenhill Rd N/O Greenway PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_010

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					617	639	0	0	1,256								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	2	0			2	12:00	8	6			14	00:00	01:00	3	1		4
00:15	1	1			2	12:15	7	5			12	01:00	02:00	0	4		4
00:30	0	0			0	12:30	10	9			19	02:00	03:00	0	1		1
00:45	0	0			0	12:45	11	7			18	03:00	04:00	0	1		1
01:00	0	1			1	13:00	10	10			20	04:00	05:00	1	2		3
01:15	0	0			0	13:15	4	5			9	05:00	06:00	1	13		14
01:30	0	2			2	13:30	8	9			17	06:00	07:00	5	26		31
01:45	0	1			1	13:45	8	8			16	07:00	08:00	18	47		65
02:00	0	0			0	14:00	8	9			17	08:00	09:00	62	75		137
02:15	0	0			0	14:15	10	8			18	09:00	10:00	35	47		82
02:30	0	0			0	14:30	12	10			22	10:00	11:00	11	33		44
02:45	0	1			1	14:45	19	6			25	11:00	12:00	30	31		61
03:00	0	0			0	15:00	20	18			38	12:00	13:00	36	27		63
03:15	0	1			1	15:15	16	16			32	13:00	14:00	30	32		62
03:30	0	0			0	15:30	14	19			33	14:00	15:00	49	33		82
03:45	0	0			0	15:45	12	22			34	15:00	16:00	62	75		137
04:00	0	0			0	16:00	21	14			35	16:00	17:00	60	59		119
04:15	0	0			0	16:15	12	16			28	17:00	18:00	59	44		103
04:30	1	0			1	16:30	13	15			28	18:00	19:00	56	38		94
04:45	0	2			2	16:45	14	14			28	19:00	20:00	36	21		57
05:00	0	3			3	17:00	17	14			31	20:00	21:00	23	12		35
05:15	0	1			1	17:15	14	13			27	21:00	22:00	18	8		26
05:30	0	4			4	17:30	14	8			22	22:00	23:00	12	5		17
05:45	1	5			6	17:45	14	9			23	23:00	00:00	10	4		14
06:00	1	6			7	18:00	12	11			23	STATISTICS					
06:15	0	4			4	18:15	14	14			28		NB	SB	EB	WB	TOTAL
06:30	1	6			7	18:30	18	7			25	Peak Period	00:00 to 12:00				
06:45	3	10			13	18:45	12	6			18	Volume	166	281			447
07:00	4	10			14	19:00	10	3			13	Peak Hour	8:15	8:30			8:15
07:15	5	8			13	19:15	5	6			11	Peak Volume	63	78			140
07:30	3	17			20	19:30	7	7			14	Peak Hour Factor	0.685	0.780			0.729
07:45	6	12			18	19:45	14	5			19	Peak Period	12:00 to 00:00				
08:00	13	17			30	20:00	6	2			8	Volume	451	358			809
08:15	10	15			25	20:15	7	2			9	Peak Hour	14:45	15:00			15:00
08:30	16	18			34	20:30	5	1			6	Peak Volume	69	75			137
08:45	23	25			48	20:45	5	7			12	Peak Hour Factor	0.863	0.852			0.901
09:00	14	19			33	21:00	5	4			9	Peak Period	07:00 to 09:00				
09:15	8	16			24	21:15	8	0			8	Volume	80	122			202
09:30	7	8			15	21:30	4	2			6	Peak Hour	8:00	8:00			8:00
09:45	6	4			10	21:45	1	2			3	Peak Volume	62	75			137
10:00	3	9			12	22:00	2	0			2	Peak Hour Factor	0.674	0.750			0.714
10:15	1	9			10	22:15	5	2			7	Peak Period	16:00 to 18:00				
10:30	3	10			13	22:30	5	1			6	Volume	119	103			222
10:45	4	5			9	22:45	0	2			2	Peak Hour	16:00	16:00			16:00
11:00	8	6			14	23:00	3	2			5	Peak Volume	60	59			119
11:15	6	8			14	23:15	2	1			3	Peak Hour Factor	0.714	0.922			0.850
11:30	9	10			19	23:30	3	1			4						
11:45	7	7			14	23:45	2	0			2						
TOTALS	166	281	0	0	447	TOTALS	451	358	0	0	809						
SPLIT %	37%	63%	0%	0%	36%	SPLIT %	56%	44%	0%	0%	64%						



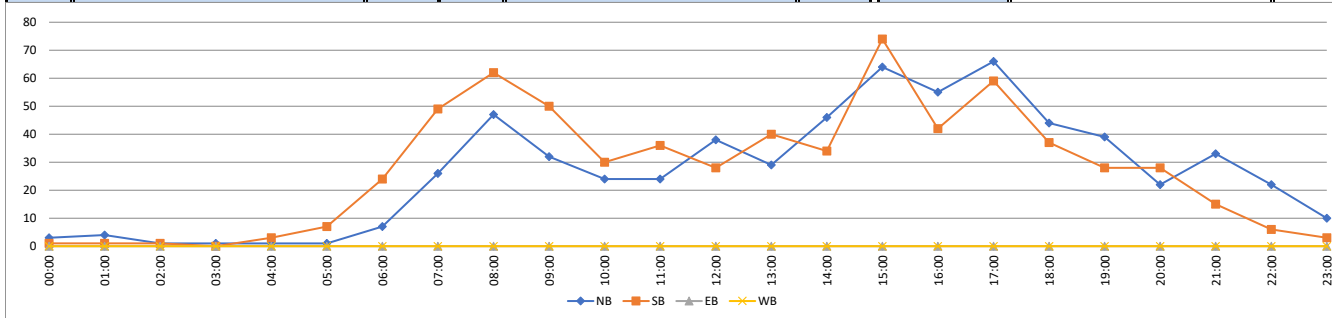
VOLUME

Greenhill Rd N/O Greenway PI

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_010

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					639	658	0	0	1,297								
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	2	0			2	12:00	5	6			11	00:00	01:00	3	1		4
00:15	0	0			0	12:15	10	10			20	01:00	02:00	4	1		5
00:30	1	0			1	12:30	13	3			16	02:00	03:00	1	1		2
00:45	0	1			1	12:45	10	9			19	03:00	04:00	1	0		1
01:00	1	0			1	13:00	4	13			17	04:00	05:00	1	3		4
01:15	0	0			0	13:15	7	4			11	05:00	06:00	1	7		8
01:30	2	0			2	13:30	9	12			21	06:00	07:00	7	24		31
01:45	1	1			2	13:45	9	11			20	07:00	08:00	26	49		75
02:00	0	0			0	14:00	10	4			14	08:00	09:00	47	62		109
02:15	1	0			1	14:15	8	7			15	09:00	10:00	32	50		82
02:30	0	0			0	14:30	10	12			22	10:00	11:00	24	30		54
02:45	0	1			1	14:45	18	11			29	11:00	12:00	24	36		60
03:00	0	0			0	15:00	22	16			38	12:00	13:00	38	28		66
03:15	1	0			1	15:15	15	23			38	13:00	14:00	29	40		69
03:30	0	0			0	15:30	10	16			26	14:00	15:00	46	34		80
03:45	0	0			0	15:45	17	19			36	15:00	16:00	64	74		138
04:00	0	0			0	16:00	13	10			23	16:00	17:00	55	42		97
04:15	0	1			1	16:15	16	11			27	17:00	18:00	66	59		125
04:30	1	1			2	16:30	10	11			21	18:00	19:00	44	37		81
04:45	0	1			1	16:45	16	10			26	19:00	20:00	39	28		67
05:00	0	2			2	17:00	30	10			40	20:00	21:00	22	28		50
05:15	0	2			2	17:15	16	19			35	21:00	22:00	33	15		48
05:30	1	1			2	17:30	9	14			23	22:00	23:00	22	6		28
05:45	0	2			2	17:45	11	16			27	23:00	00:00	10	3		13
06:00	1	4			5	18:00	10	8			18	STATISTICS					
06:15	1	5			6	18:15	18	8			26		NB	SB	EB	WB	TOTAL
06:30	3	7			10	18:30	11	13			24	Peak Period	00:00 to 12:00				
06:45	2	8			10	18:45	5	8			13	Volume	171	264			435
07:00	6	10			16	19:00	8	6			14	Peak Hour	8:00 8:15				8:15
07:15	4	8			12	19:15	13	9			22	Peak Volume	47	75			120
07:30	11	18			29	19:30	12	7			19	Peak Hour Factor	0.783	0.781			0.811
07:45	5	13			18	19:45	6	6			12	Peak Period	12:00 to 00:00				
08:00	15	11			26	20:00	8	8			16	Volume	468	394			862
08:15	11	19			30	20:15	5	12			17	Peak Hour	16:15 15:00				15:00
08:30	8	18			26	20:30	4	4			8	Peak Volume	72	74			138
08:45	13	14			27	20:45	5	4			9	Peak Hour Factor	0.600	0.804			0.908
09:00	13	24			37	21:00	13	3			16	Peak Period	07:00 to 09:00				
09:15	10	13			23	21:15	9	8			17	Volume	73	111			184
09:30	5	8			13	21:30	1	2			3	Peak Hour	8:00 8:00				8:00
09:45	4	5			9	21:45	10	2			12	Peak Volume	47	62			109
10:00	6	6			12	22:00	7	1			8	Peak Hour Factor	0.783	0.816			0.908
10:15	5	14			19	22:15	5	3			8	Peak Period	16:00 to 18:00				
10:30	6	6			12	22:30	7	2			9	Volume	121	101			222
10:45	7	4			11	22:45	3	0			3	Peak Hour	16:15 17:00				17:00
11:00	5	8			13	23:00	4	2			6	Peak Volume	72	59			125
11:15	8	9			17	23:15	3	0			3	Peak Hour Factor	0.600	0.776			0.781
11:30	7	11			18	23:30	1	0			1						
11:45	4	8			12	23:45	2	1			3						
TOTALS	171	264	0	0	435	TOTALS	468	394	0	0	862						
SPLIT %	39%	61%	0%	0%	34%	SPLIT %	54%	46%	0%	0%	66%						



CLASSIFICATION
Greenhill Rd N/O Greenway PI

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_010

Hourly Breakdown table with columns for Time, Northbound (Total, #1-#13), Southbound (Total, #1-#13), and Totals (Total, #1-#13). Rows represent hourly intervals from 00:00 to 23:00.

CLASSIFICATION DEFINITIONS section showing illustrations for vehicle types #1 through #13, including Motorcycles, Passenger Cars, 2-Axle Single Units, Buses, 2-Axle Single Units, 3-Axle Single Units, >=4-Axle Single Units, <=4-Axle Single Trailers, 5-Axle Single Trailers, >=6-Axle Single Trailers, <=5-Axle Multi-Trailers, 6-Axle Multi-Trailers, and >=7-Axle Multi-Trailers.

STATISTICS table with columns for time intervals (e.g., 00:00-12:00, 12:00-24:00, 07:00-09:00, 16:00-18:00) and rows for % and Peak Hour/Volume data across various vehicle classifications.

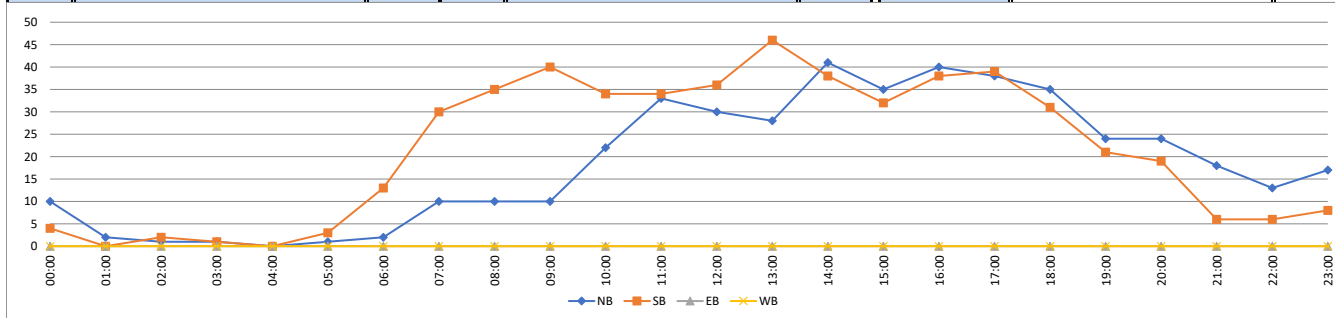
VOLUME

Greenhill Rd N/O Greenway PI

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_010

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS						
						445	516	0	0	961							
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	3	0			3	12:00	13	5			18	00:00	10	4			14
00:15	5	2			7	12:15	5	8			13	01:00	2	0			2
00:30	2	1			3	12:30	7	9			16	02:00	1	2			3
00:45	0	1			1	12:45	5	14			19	03:00	1	1			2
01:00	1	0			1	13:00	4	14			18	04:00	0	0			0
01:15	1	0			1	13:15	8	14			22	05:00	1	3			4
01:30	0	0			0	13:30	11	6			17	06:00	2	13			15
01:45	0	0			0	13:45	5	12			17	07:00	10	30			40
02:00	0	0			0	14:00	13	7			20	08:00	10	35			45
02:15	0	1			1	14:15	10	13			23	09:00	10	40			50
02:30	1	0			1	14:30	10	8			18	10:00	22	34			56
02:45	0	1			1	14:45	8	10			18	11:00	33	34			67
03:00	0	0			0	15:00	7	8			15	12:00	30	36			66
03:15	1	1			2	15:15	8	7			15	13:00	28	46			74
03:30	0	0			0	15:30	8	10			18	14:00	41	38			79
03:45	0	0			0	15:45	12	7			19	15:00	35	32			67
04:00	0	0			0	16:00	10	12			22	16:00	40	38			78
04:15	0	0			0	16:15	7	6			13	17:00	38	39			77
04:30	0	0			0	16:30	6	14			20	18:00	35	31			66
04:45	0	0			0	16:45	17	6			23	19:00	24	21			45
05:00	1	0			1	17:00	8	11			19	20:00	24	19			43
05:15	0	2			2	17:15	14	7			21	21:00	18	6			24
05:30	0	0			0	17:30	7	10			17	22:00	13	6			19
05:45	0	1			1	17:45	9	11			20	23:00	17	8			25
06:00	0	4			4	18:00	9	9			18	STATISTICS					
06:15	1	3			4	18:15	10	11			21		NB	SB	EB	WB	TOTAL
06:30	0	3			3	18:30	11	7			18	Peak Period	00:00 to 12:00				
06:45	1	3			4	18:45	5	4			9	Volume	102	196			298
07:00	2	4			6	19:00	7	4			11	Peak Hour	11:00 9:15				11:00
07:15	4	9			13	19:15	7	3			10	Peak Volume	33	43			67
07:30	4	9			13	19:30	8	8			16	Peak Hour Factor	0.688	0.827			0.728
07:45	0	8			8	19:45	2	6			8	Peak Period	12:00 to 00:00				
08:00	1	7			8	20:00	3	4			7	Volume	343	320			663
08:15	3	6			9	20:15	12	4			16	Peak Hour	16:45 12:30				16:30
08:30	4	10			14	20:30	5	5			10	Peak Volume	46	51			83
08:45	2	12			14	20:45	4	6			10	Peak Hour Factor	0.676	0.911			0.902
09:00	4	6			10	21:00	6	1			7	Peak Period	07:00 to 09:00				
09:15	1	13			14	21:15	4	2			6	Volume	20	65			85
09:30	3	9			12	21:30	2	2			4	Peak Hour	7:00 8:00				8:00
09:45	2	12			14	21:45	6	1			7	Peak Volume	10	35			45
10:00	9	9			18	22:00	2	3			5	Peak Hour Factor	0.625	0.729			0.804
10:15	4	5			9	22:15	3	1			4	Peak Period	16:00 to 18:00				
10:30	5	8			13	22:30	4	2			6	Volume	78	77			155
10:45	4	12			16	22:45	4	0			4	Peak Hour	16:45 17:00				16:30
11:00	10	7			17	23:00	5	3			8	Peak Volume	46	39			83
11:15	6	9			15	23:15	3	2			5	Peak Hour Factor	0.676	0.886			0.902
11:30	5	7			12	23:30	7	3			10						
11:45	12	11			23	23:45	2	0			2						
TOTALS	102	196	0	0	298	TOTALS	343	320	0	0	663						
SPLIT %	34%	66%	0%	0%	31%	SPLIT %	52%	48%	0%	0%	69%						



SPEED
Greenhill Rd N/O Greenway PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_010

Time	NORTHBOUND														Total	SOUTHBOUND														Total	TOTALS														Total				
	5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70					
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99								
00:00	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	1	0	0	0	0	0	0	0	0	0	0	1	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
01:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1	1	0	0	0	0	0	0	0	0	4	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
04:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	0	2	0	0	0	0	0	0	0	0	0	2	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
05:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	1	10	2	0	0	0	0	0	0	0	0	13	0	1	11	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
06:00	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5	0	5	18	3	0	0	0	0	0	0	0	26	0	9	19	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	31		
07:00	0	4	13	1	0	0	0	0	0	0	0	0	0	18	0	9	30	8	0	0	0	0	0	0	0	47	0	13	43	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	65		
08:00	8	26	22	4	2	0	0	0	0	0	0	0	0	62	3	11	51	7	2	1	0	0	0	0	75	11	37	73	11	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	137		
09:00	4	17	14	0	0	0	0	0	0	0	0	0	0	35	4	10	29	4	0	0	0	0	0	0	47	8	27	43	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	82		
10:00	1	6	3	1	0	0	0	0	0	0	0	0	0	11	2	4	25	1	1	0	0	0	0	0	33	3	10	28	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	44		
11:00	2	21	7	0	0	0	0	0	0	0	0	0	0	30	0	11	18	2	0	0	0	0	0	0	31	2	32	25	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	61		
12:00	5	18	12	1	0	0	0	0	0	0	0	0	0	36	3	9	13	2	0	0	0	0	0	0	27	8	27	25	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	63		
13:00	6	13	11	0	0	0	0	0	0	0	0	0	0	30	1	11	18	2	0	0	0	0	0	0	32	7	24	29	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	62		
14:00	15	27	7	0	0	0	0	0	0	0	0	0	0	49	2	16	13	1	1	0	0	0	0	0	33	17	43	20	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	82		
15:00	13	37	12	0	0	0	0	0	0	0	0	0	0	62	1	21	43	8	2	0	0	0	0	0	75	14	58	55	8	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	137		
16:00	10	32	18	0	0	0	0	0	0	0	0	0	0	60	3	13	40	3	0	0	0	0	0	0	59	13	45	58	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	119		
17:00	11	35	12	1	0	0	0	0	0	0	0	0	0	59	2	21	14	7	0	0	0	0	0	0	44	13	56	26	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	103		
18:00	8	28	17	2	1	0	0	0	0	0	0	0	0	56	0	17	17	4	0	0	0	0	0	0	38	8	45	34	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	94		
19:00	3	20	12	1	0	0	0	0	0	0	0	0	0	36	0	11	7	3	0	0	0	0	0	0	21	3	31	19	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	57		
20:00	4	10	7	2	0	0	0	0	0	0	0	0	0	23	1	2	8	1	0	0	0	0	0	0	12	5	12	15	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	35		
21:00	3	8	7	0	0	0	0	0	0	0	0	0	0	18	1	3	4	0	0	0	0	0	0	0	8	4	11	11	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	26		
22:00	2	6	3	1	0	0	0	0	0	0	0	0	0	12	0	2	3	0	0	0	0	0	0	0	5	2	8	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17		
23:00	1	7	2	0	0	0	0	0	0	0	0	0	0	10	1	1	2	0	0	0	0	0	0	0	4	2	8	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14		
Totals	97	321	182	14	3	0	0	0	0	0	0	0	0	617	24	181	367	60	6	1	0	0	0	0	639	121	502	549	74	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1,256			
% of Totals	16%	52%	29%	2%	0%									100%	4%	28%	57%	9%	1%	0%				100%	10%	40%	44%	6%	1%	0%														100%					

STATISTICS	NORTHBOUND														Total	SOUTHBOUND														Total	TOTALS														Total							
	00:00 - 12:00	15	20	25	30	35	40	45	50	55	60	65	70	70		00:00 - 12:00	15	20	25	30	35	40	45	50	55	60	65	70	70		00:00 - 12:00	15	20	25	30	35	40	45	50	55	60	65	70	70								
	Peak Hour	8:30	8:00	8:15	7:45	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:00		Peak Hour	8:30	8:00	8:15	7:45	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:00		Peak Hour	8:30	8:00	8:15	7:45	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:00		8:00	8:00	8:00	8:00	8:00	8:00	8:00
00:00 - 12:00	16	80	62	6	2	0	0	0	0	0	0	0	0	166	9	54	185	29	3	1	0	0	0	281	25	134	247	35	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	447							
Peak Hour	8:30	8:00	8:15	7:45	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:00	8:15	8:15	8:15	8:30	7:00	8:00	7:15	0:00	0:00	0:00	8:30	8:45	8:00	8:30	8:00	8:00	7:15	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	0:00	8:15									
Peak Volume	10	26	25	4	2	0	0	0	0	0	0	0	0	63	4	12	57	8	2	1	0	0	78	14	37	82	11	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	140									
12:00 - 24:00	81	241	120	8	1	0	0	0	0	0	0	0	0	451	15	127	182	31	3	0	0	0	0	558	96	368	302	39	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	809								
Peak Hour	14:30	14:45	18:15	17:45	17:45	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	14:45	15:45	15:00	15:00	15:00	14:15	12:00	12:00	12:00	12:00	15:00	14:30	15:00	16:00	17:15	14:15	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	12:00	15:00										
Peak Volume	19	42	20	3	1	0	0	0	0	0	0	0	0	69	4	21	43	8	2	0	0	0	75	19	58	58	10	2	0	0	0	0	0	0	0	0	0	0	0	0	0	137										
07:00 - 09:00	8	30	35	5	2	0	0	0	0	0	0	0	0	80	3	20	81	15	2	1	0	0	0	122	11	50	116	20	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	202								
Peak Hour	8:00	8:00	8:00	7:45	8:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	7:00	8:00	7:30	7:30	8:00	7:00	8:00	7:15	7:00	7:00	7:00	8:00	8:00	8:00	8:00	8:00	7:15	7:00	7:00	7:00	7:00	7																		

SPEED Greenhill Rd N/O Greenway PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_010

Time	NORTHBOUND													Total	SOUTHBOUND													Total	TOTALS													Total						
	5	15	20	25	30	35	40	45	50	55	60	65	70		5	15	20	25	30	35	40	45	50	55	60	65	70		5	15	20	25	30	35	40	45	50	55	60	65	70							
	15	20	25	30	35	40	45	50	55	60	65	70	99		15	20	25	30	35	40	45	50	55	60	65	70	99		15	20	25	30	35	40	45	50	55	60	65	70	99							
12:00	1	4	3	0	0	0	0	0	0	0	0	0	0	8	1	2	3	0	0	0	0	0	0	0	0	6	2	6	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14			
12:15	0	2	4	1	0	0	0	0	0	0	0	0	0	7	1	1	2	1	0	0	0	0	0	0	0	5	1	3	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12		
12:30	1	6	3	0	0	0	0	0	0	0	0	0	0	10	0	4	4	1	0	0	0	0	0	0	0	9	1	10	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19		
12:45	3	6	2	0	0	0	0	0	0	0	0	0	0	11	1	2	4	0	0	0	0	0	0	0	0	7	4	8	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20		
13:00	2	3	5	0	0	0	0	0	0	0	0	0	0	10	0	4	6	0	0	0	0	0	0	0	0	10	2	7	11	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18		
13:15	2	1	1	0	0	0	0	0	0	0	0	0	0	4	0	0	4	1	0	0	0	0	0	0	0	5	2	1	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9			
13:30	2	5	1	0	0	0	0	0	0	0	0	0	0	8	1	4	4	0	0	0	0	0	0	0	0	9	3	9	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17		
13:45	0	4	4	0	0	0	0	0	0	0	0	0	0	8	0	3	4	1	0	0	0	0	0	0	0	8	0	7	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16		
14:00	2	3	3	0	0	0	0	0	0	0	0	0	0	8	1	8	0	0	0	0	0	0	0	0	0	9	3	11	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17	
14:15	3	6	1	0	0	0	0	0	0	0	0	0	0	10	1	1	6	0	0	0	0	0	0	0	0	8	4	7	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18		
14:30	5	5	2	0	0	0	0	0	0	0	0	0	0	12	0	6	3	1	0	0	0	0	0	0	0	10	5	11	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	22	
14:45	5	13	1	0	0	0	0	0	0	0	0	0	0	19	0	1	4	0	1	0	0	0	0	0	0	6	5	14	5	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	25	
15:00	4	12	4	0	0	0	0	0	0	0	0	0	0	20	0	4	12	1	1	0	0	0	0	0	0	18	4	16	16	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	38		
15:15	5	9	2	0	0	0	0	0	0	0	0	0	0	16	0	4	10	2	0	0	0	0	0	0	0	16	5	13	12	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	32	
15:30	3	8	3	0	0	0	0	0	0	0	0	0	0	14	0	5	14	0	0	0	0	0	0	0	0	19	3	13	17	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	33	
15:45	1	8	3	0	0	0	0	0	0	0	0	0	0	12	1	8	7	5	1	0	0	0	0	0	0	22	2	16	10	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	34
16:00	6	9	6	0	0	0	0	0	0	0	0	0	0	21	1	4	8	1	0	0	0	0	0	0	0	14	7	13	14	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	35	
16:15	1	8	3	0	0	0	0	0	0	0	0	0	0	12	0	2	12	2	0	0	0	0	0	0	0	16	1	10	15	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	28	
16:30	1	9	3	0	0	0	0	0	0	0	0	0	0	13	2	3	10	0	0	0	0	0	0	0	0	15	3	12	13	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	28	
16:45	2	6	6	0	0	0	0	0	0	0	0	0	0	14	0	4	10	0	0	0	0	0	0	0	0	14	2	10	16	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	31
17:00	2	11	4	0	0	0	0	0	0	0	0	0	0	17	1	7	5	1	0	0	0	0	0	0	0	14	3	18	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	31	
17:15	6	5	3	0	0	0	0	0	0	0	0	0	0	14	1	6	4	2	0	0	0	0	0	0	0	13	7	11	7	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	27	
17:30	2	9	3	0	0	0	0	0	0	0	0	0	0	14	0	4	2	2	0	0	0	0	0	0	0	8	2	13	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	22	
17:45	1	10	2	1	0	0	0	0	0	0	0	0	0	14	0	4	3	2	0	0	0	0	0	0	0	9	1	14	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23	
18:00	2	7	2	1	0	0	0	0	0	0	0	0	0	12	0	6	3	2	0	0	0	0	0	0	0	11	2	13	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23	
18:15	4	8	2	0	0	0	0	0	0	0	0	0	0	14	0	7	5	2	0	0	0	0	0	0	0	14	4	15	7	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	28	
18:30	1	8	7	1	1	0	0	0	0	0	0	0	0	18	0	2	5	0	0	0	0	0	0	0	0	7	1	10	12	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	25	
18:45	1	5	6	0	0	0	0	0	0	0	0	0	0	12	0	2	4	0	0	0	0	0	0	0	0	6	1	7	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13	
19:00	2	2	5	1	0	0	0	0	0	0	0	0	0	10	0	1	1	1	0	0	0	0	0	0	0	3	2	3	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18	
19:15	0	4	1	0	0	0	0	0	0	0	0	0	0	5	0	3	2	1	0	0	0	0	0	0	0	6	0	7	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11		
19:30	0	6	1	0	0	0	0	0	0	0	0	0	0	7	0	4	3	0	0	0	0	0	0	0	0	7	0	10	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
19:45	1	8	5	0	0	0	0	0	0	0	0	0	0	14	0	3	1	1	0	0	0	0	0	0	0	5	1	11	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	
20:00	1	3	2	0	0	0	0	0	0	0	0	0	0	6	0	0	2	0	0	0	0	0	0	0	0	2	1	3	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8		
20:15	3	2	2	0	0	0	0	0	0	0	0	0	0	7	0	0	2	0	0	0	0	0	0	0	0	2	3	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
20:30	0	2	1	2	0	0	0	0	0	0	0	0	0	5	0	0	1	0	0	0	0	0	0	0	1	0	2	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6		
20:45	0	3	2	0	0	0	0	0	0	0	0	0	0	5	1	2	3	1	0	0	0	0	0	0	0	7	1	5	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12		
21:00	1	2	2	0	0	0	0	0	0	0	0	0	0	5	1	0	3	0	0	0	0	0	0	0	0	4	2	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
21:15	2	2	4	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	0	0	0	0	0	0	0	0	2	2	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8		
21:30	0	3	1	0	0	0	0	0	0	0	0	0	0	4	0	1	1	0	0	0	0	0	0	0	0	2	0	4	2	0	0																	

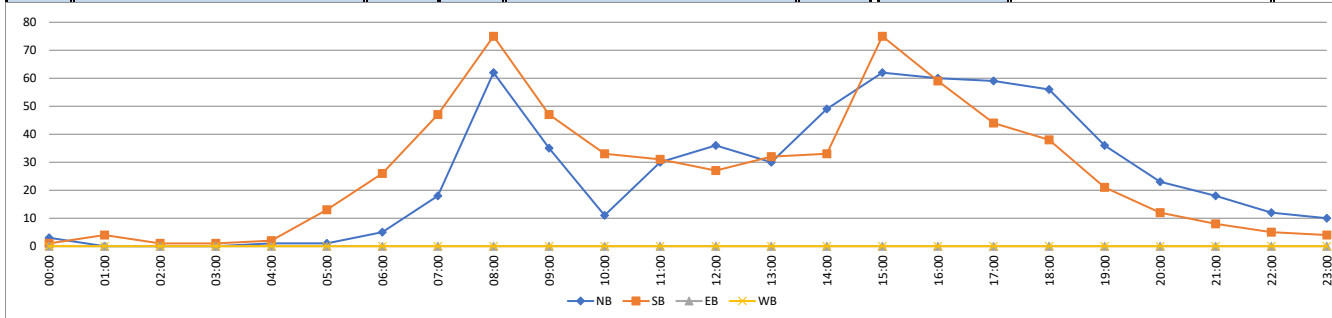
VOLUME

Greenhill Rd N/O Greenway PI

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_010

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					617	639	0	0	1,256								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	2	0			2	12:00	8	6			14	00:00	01:00	3	1		4
00:15	1	1			2	12:15	7	5			12	01:00	02:00	0	4		4
00:30	0	0			0	12:30	10	9			19	02:00	03:00	0	1		1
00:45	0	0			0	12:45	11	7			18	03:00	04:00	0	1		1
01:00	0	1			1	13:00	10	10			20	04:00	05:00	1	2		3
01:15	0	0			0	13:15	4	5			9	05:00	06:00	1	13		14
01:30	0	2			2	13:30	8	9			17	06:00	07:00	5	26		31
01:45	0	1			1	13:45	8	8			16	07:00	08:00	18	47		65
02:00	0	0			0	14:00	8	9			17	08:00	09:00	62	75		137
02:15	0	0			0	14:15	10	8			18	09:00	10:00	35	47		82
02:30	0	0			0	14:30	12	10			22	10:00	11:00	11	33		44
02:45	0	1			1	14:45	19	6			25	11:00	12:00	30	31		61
03:00	0	0			0	15:00	20	18			38	12:00	13:00	36	27		63
03:15	0	1			1	15:15	16	16			32	13:00	14:00	30	32		62
03:30	0	0			0	15:30	14	19			33	14:00	15:00	49	33		82
03:45	0	0			0	15:45	12	22			34	15:00	16:00	62	75		137
04:00	0	0			0	16:00	21	14			35	16:00	17:00	60	59		119
04:15	0	0			0	16:15	12	16			28	17:00	18:00	59	44		103
04:30	1	0			1	16:30	13	15			28	18:00	19:00	56	38		94
04:45	0	2			2	16:45	14	14			28	19:00	20:00	36	21		57
05:00	0	3			3	17:00	17	14			31	20:00	21:00	23	12		35
05:15	0	1			1	17:15	14	13			27	21:00	22:00	18	8		26
05:30	0	4			4	17:30	14	8			22	22:00	23:00	12	5		17
05:45	1	5			6	17:45	14	9			23	23:00	00:00	10	4		14
06:00	1	6			7	18:00	12	11			23	STATISTICS					
06:15	0	4			4	18:15	14	14			28		NB	SB	EB	WB	TOTAL
06:30	1	6			7	18:30	18	7			25	Peak Period	00:00 to 12:00				
06:45	3	10			13	18:45	12	6			18	Volume	166	281			447
07:00	4	10			14	19:00	10	3			13	Peak Hour	8:15	8:30			8:15
07:15	5	8			13	19:15	5	6			11	Peak Volume	63	78			140
07:30	3	17			20	19:30	7	7			14	Peak Hour Factor	0.685	0.780			0.729
07:45	6	12			18	19:45	14	5			19	Peak Period	12:00 to 00:00				
08:00	13	17			30	20:00	6	2			8	Volume	451	358			809
08:15	10	15			25	20:15	7	2			9	Peak Hour	14:45	15:00			15:00
08:30	16	18			34	20:30	5	1			6	Peak Volume	69	75			137
08:45	23	25			48	20:45	5	7			12	Peak Hour Factor	0.863	0.852			0.901
09:00	14	19			33	21:00	5	4			9	Peak Period	07:00 to 09:00				
09:15	8	16			24	21:15	8	0			8	Volume	80	122			202
09:30	7	8			15	21:30	4	2			6	Peak Hour	8:00	8:00			8:00
09:45	6	4			10	21:45	1	2			3	Peak Volume	62	75			137
10:00	3	9			12	22:00	2	0			2	Peak Hour Factor	0.674	0.750			0.714
10:15	1	9			10	22:15	5	2			7	Peak Period	16:00 to 18:00				
10:30	3	10			13	22:30	5	1			6	Volume	119	103			222
10:45	4	5			9	22:45	0	2			2	Peak Hour	16:00	16:00			16:00
11:00	8	6			14	23:00	3	2			5	Peak Volume	60	59			119
11:15	6	8			14	23:15	2	1			3	Peak Hour Factor	0.714	0.922			0.850
11:30	9	10			19	23:30	3	1			4						
11:45	7	7			14	23:45	2	0			2						
TOTALS	166	281	0	0	447	TOTALS	451	358	0	0	809						
SPLIT %	37%	63%	0%	0%	36%	SPLIT %	56%	44%	0%	0%	64%						



SPEED
Greenhill Rd N/O Greenway PI

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_010

Time	NORTHBOUND														Total	SOUTHBOUND														Total	TOTALS														Total	
	5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70		
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99					
12:00	1	2	2	0	0	0	0	0	0	0	0	0	0	5	15	20	4	2	0	0	0	0	0	0	0	0	6	1	6	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	
12:15	0	2	7	1	0	0	0	0	0	0	0	0	0	10	0	5	3	2	2	0	0	0	0	0	0	0	10	0	7	10	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20
12:30	2	4	6	1	0	0	0	0	0	0	0	0	0	13	0	1	2	0	0	0	0	0	0	0	0	0	3	2	5	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	
12:45	2	5	3	0	0	0	0	0	0	0	0	0	0	10	0	2	6	1	0	0	0	0	0	0	0	0	9	2	7	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	17	
13:00	0	3	1	0	0	0	0	0	0	0	0	0	0	4	3	0	9	1	0	0	0	0	0	0	0	0	13	3	3	10	1	0	0	0	0	0	0	0	0	0	0	0	0	0	19	
13:15	0	5	2	0	0	0	0	0	0	0	0	0	0	7	1	1	2	0	0	0	0	0	0	0	0	4	1	6	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11		
13:30	0	5	4	0	0	0	0	0	0	0	0	0	0	9	0	3	8	1	0	0	0	0	0	0	0	12	0	8	12	1	0	0	0	0	0	0	0	0	0	0	0	0	0	21		
13:45	0	6	3	0	0	0	0	0	0	0	0	0	0	9	0	4	6	1	0	0	0	0	0	0	0	11	0	10	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	20		
14:00	0	6	3	1	0	0	0	0	0	0	0	0	0	10	0	1	3	0	0	0	0	0	0	0	0	4	0	7	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	14		
14:15	0	3	5	0	0	0	0	0	0	0	0	0	0	8	0	4	3	0	0	0	0	0	0	0	0	7	0	7	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	15		
14:30	1	3	6	0	0	0	0	0	0	0	0	0	0	10	0	5	6	1	0	0	0	0	0	0	0	12	1	8	12	1	0	0	0	0	0	0	0	0	0	0	0	0	0	22		
14:45	2	10	6	0	0	0	0	0	0	0	0	0	0	18	1	4	5	1	0	0	0	0	0	0	0	11	3	14	11	1	0	0	0	0	0	0	0	0	0	0	0	0	29			
15:00	2	13	6	1	0	0	0	0	0	0	0	0	0	22	1	2	10	3	0	0	0	0	0	0	0	16	3	15	16	4	0	0	0	0	0	0	0	0	0	0	0	0	38			
15:15	2	3	9	1	0	0	0	0	0	0	0	0	0	15	1	3	17	1	1	0	0	0	0	0	0	23	3	6	26	2	1	0	0	0	0	0	0	0	0	0	0	0	38			
15:30	1	4	4	1	0	0	0	0	0	0	0	0	0	10	1	2	12	1	0	0	0	0	0	0	0	16	2	6	16	2	0	0	0	0	0	0	0	0	0	0	0	0	26			
15:45	4	11	2	0	0	0	0	0	0	0	0	0	0	17	0	3	12	4	0	0	0	0	0	0	0	19	4	14	14	4	0	0	0	0	0	0	0	0	0	0	0	0	36			
16:00	1	6	6	0	0	0	0	0	0	0	0	0	0	13	0	4	6	0	0	0	0	0	0	0	0	10	1	10	12	0	0	0	0	0	0	0	0	0	0	0	0	0	23			
16:15	1	8	5	2	0	0	0	0	0	0	0	0	0	16	0	3	6	2	0	0	0	0	0	0	0	11	1	11	11	4	0	0	0	0	0	0	0	0	0	0	0	0	27			
16:30	0	10	0	0	0	0	0	0	0	0	0	0	0	10	1	3	7	0	0	0	0	0	0	0	0	11	1	13	7	0	0	0	0	0	0	0	0	0	0	0	0	0	21			
16:45	5	4	7	0	0	0	0	0	0	0	0	0	0	16	0	2	6	2	0	0	0	0	0	0	0	10	5	6	13	2	0	0	0	0	0	0	0	0	0	0	0	0	0	26		
17:00	4	18	6	2	0	0	0	0	0	0	0	0	0	30	0	4	5	1	0	0	0	0	0	0	0	10	4	22	11	3	0	0	0	0	0	0	0	0	0	0	0	0	40			
17:15	6	6	3	1	0	0	0	0	0	0	0	0	0	16	0	4	14	1	0	0	0	0	0	0	0	19	6	10	17	2	0	0	0	0	0	0	0	0	0	0	0	0	35			
17:30	5	2	2	0	0	0	0	0	0	0	0	0	0	9	0	7	4	2	1	0	0	0	0	0	0	14	5	9	6	2	1	0	0	0	0	0	0	0	0	0	0	0	23			
17:45	0	5	5	1	0	0	0	0	0	0	0	0	0	11	0	4	11	1	0	0	0	0	0	0	0	16	0	9	16	2	0	0	0	0	0	0	0	0	0	0	0	0	27			
18:00	2	3	5	0	0	0	0	0	0	0	0	0	0	10	0	3	5	0	0	0	0	0	0	0	0	8	2	6	10	0	0	0	0	0	0	0	0	0	0	0	0	0	18			
18:15	5	10	2	1	0	0	0	0	0	0	0	0	0	18	0	4	4	0	0	0	0	0	0	0	0	8	5	14	6	1	0	0	0	0	0	0	0	0	0	0	0	0	26			
18:30	1	8	2	0	0	0	0	0	0	0	0	0	0	11	1	4	8	0	0	0	0	0	0	0	0	13	2	12	10	0	0	0	0	0	0	0	0	0	0	0	0	0	24			
18:45	1	2	2	0	0	0	0	0	0	0	0	0	0	5	0	2	4	2	0	0	0	0	0	0	0	8	1	4	6	2	0	0	0	0	0	0	0	0	0	0	0	0	13			
19:00	2	6	0	0	0	0	0	0	0	0	0	0	0	8	0	2	3	1	0	0	0	0	0	0	0	6	2	8	3	1	0	0	0	0	0	0	0	0	0	0	0	0	14			
19:15	2	8	3	0	0	0	0	0	0	0	0	0	0	13	3	1	5	0	0	0	0	0	0	0	0	9	5	9	8	0	0	0	0	0	0	0	0	0	0	0	0	22				
19:30	1	8	3	0	0	0	0	0	0	0	0	0	0	12	0	0	7	0	0	0	0	0	0	0	0	7	1	8	10	0	0	0	0	0	0	0	0	0	0	0	0	0	19			
19:45	0	5	1	0	0	0	0	0	0	0	0	0	0	6	0	3	2	1	0	0	0	0	0	0	0	6	0	8	3	1	0	0	0	0	0	0	0	0	0	0	0	0	12			
20:00	1	4	3	0	0	0	0	0	0	0	0	0	0	8	1	3	3	0	1	0	0	0	0	0	0	8	2	7	6	0	1	0	0	0	0	0	0	0	0	0	0	0	16			
20:15	0	3	2	0	0	0	0	0	0	0	0	0	0	5	0	3	8	1	0	0	0	0	0	0	0	12	0	6	10	1	0	0	0	0	0	0	0	0	0	0	0	0	17			
20:30	1	3	0	0	0	0	0	0	0	0	0	0	0	4	0	3	0	1	0	0	0	0	0	0	0	4	1	6	0	1	0	0	0	0	0	0	0	0	0	0	0	0	8			
20:45	1	3	0	1	0	0	0	0	0	0	0	0	0	5	0	1	3	0	0	0	0	0	0	0	0	4	1	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9			
21:00	0	8	5	0	0	0	0	0	0	0	0	0	0	13	0	2	1	0	0	0	0	0	0	0	0	3	0	10	6	0	0	0	0	0	0	0	0	0	0	0	0	0	16			
21:15	1	7	1	0	0	0	0	0	0	0	0	0	0	9	1	2	5	0	0	0	0	0	0	0	0	8	2	9	6	0	0	0	0	0	0	0	0	0	0	0	0	0	17			
21:30	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3			
21:45	1	7	2	0	0	0	0	0	0	0	0	0	0	10	0	1	1	0	0	0	0	0	0	0	0	2	1	8	3	0	0	0	0	0	0	0	0	0	0	0	0	0	12			
22:00	0	6	1	0	0	0	0	0	0	0	0	0	0	7	0	0	1	0	0	0	0	0	0	0	0	1	0	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	8			
22:15	1	4	0	0																																										

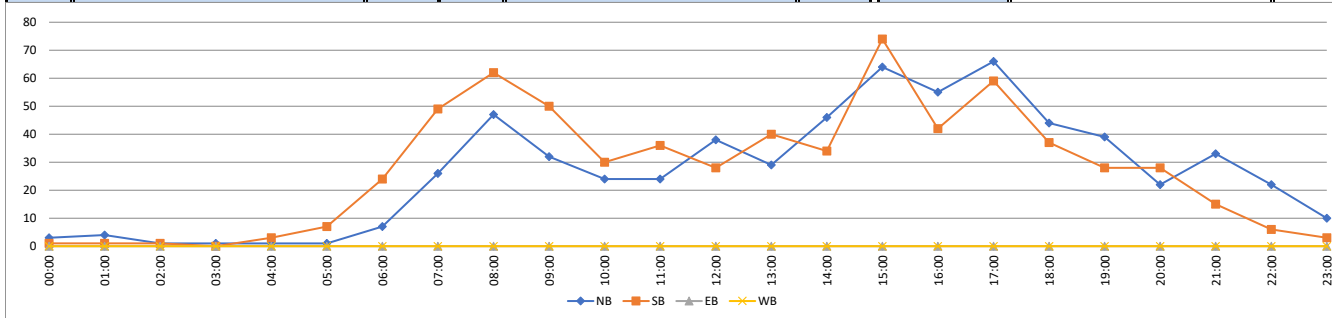
VOLUME

Greenhill Rd N/O Greenway PI

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_010

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					639	658	0	0	1,297								
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	2	0			2	12:00	5	6			11	00:00	01:00	3	1		4
00:15	0	0			0	12:15	10	10			20	01:00	02:00	4	1		5
00:30	1	0			1	12:30	13	3			16	02:00	03:00	1	1		2
00:45	0	1			1	12:45	10	9			19	03:00	04:00	1	0		1
01:00	1	0			1	13:00	4	13			17	04:00	05:00	1	3		4
01:15	0	0			0	13:15	7	4			11	05:00	06:00	1	7		8
01:30	2	0			2	13:30	9	12			21	06:00	07:00	7	24		31
01:45	1	1			2	13:45	9	11			20	07:00	08:00	26	49		75
02:00	0	0			0	14:00	10	4			14	08:00	09:00	47	62		109
02:15	1	0			1	14:15	8	7			15	09:00	10:00	32	50		82
02:30	0	0			0	14:30	10	12			22	10:00	11:00	24	30		54
02:45	0	1			1	14:45	18	11			29	11:00	12:00	24	36		60
03:00	0	0			0	15:00	22	16			38	12:00	13:00	38	28		66
03:15	1	0			1	15:15	15	23			38	13:00	14:00	29	40		69
03:30	0	0			0	15:30	10	16			26	14:00	15:00	46	34		80
03:45	0	0			0	15:45	17	19			36	15:00	16:00	64	74		138
04:00	0	0			0	16:00	13	10			23	16:00	17:00	55	42		97
04:15	0	1			1	16:15	16	11			27	17:00	18:00	66	59		125
04:30	1	1			2	16:30	10	11			21	18:00	19:00	44	37		81
04:45	0	1			1	16:45	16	10			26	19:00	20:00	39	28		67
05:00	0	2			2	17:00	30	10			40	20:00	21:00	22	28		50
05:15	0	2			2	17:15	16	19			35	21:00	22:00	33	15		48
05:30	1	1			2	17:30	9	14			23	22:00	23:00	22	6		28
05:45	0	2			2	17:45	11	16			27	23:00	00:00	10	3		13
06:00	1	4			5	18:00	10	8			18	STATISTICS					
06:15	1	5			6	18:15	18	8			26		NB	SB	EB	WB	TOTAL
06:30	3	7			10	18:30	11	13			24	Peak Period	00:00 to 12:00				
06:45	2	8			10	18:45	5	8			13	Volume	171	264			435
07:00	6	10			16	19:00	8	6			14	Peak Hour	8:00 8:15				8:15
07:15	4	8			12	19:15	13	9			22	Peak Volume	47	75			120
07:30	11	18			29	19:30	12	7			19	Peak Hour Factor	0.783	0.781			0.811
07:45	5	13			18	19:45	6	6			12	Peak Period	12:00 to 00:00				
08:00	15	11			26	20:00	8	8			16	Volume	468	394			862
08:15	11	19			30	20:15	5	12			17	Peak Hour	16:15 15:00				15:00
08:30	8	18			26	20:30	4	4			8	Peak Volume	72	74			138
08:45	13	14			27	20:45	5	4			9	Peak Hour Factor	0.600	0.804			0.908
09:00	13	24			37	21:00	13	3			16	Peak Period	07:00 to 09:00				
09:15	10	13			23	21:15	9	8			17	Volume	73	111			184
09:30	5	8			13	21:30	1	2			3	Peak Hour	8:00 8:00				8:00
09:45	4	5			9	21:45	10	2			12	Peak Volume	47	62			109
10:00	6	6			12	22:00	7	1			8	Peak Hour Factor	0.783	0.816			0.908
10:15	5	14			19	22:15	5	3			8	Peak Period	16:00 to 18:00				
10:30	6	6			12	22:30	7	2			9	Volume	121	101			222
10:45	7	4			11	22:45	3	0			3	Peak Hour	16:15 17:00				17:00
11:00	5	8			13	23:00	4	2			6	Peak Volume	72	59			125
11:15	8	9			17	23:15	3	0			3	Peak Hour Factor	0.600	0.776			0.781
11:30	7	11			18	23:30	1	0			1						
11:45	4	8			12	23:45	2	1			3						
TOTALS	171	264	0	0	435	TOTALS	468	394	0	0	862						
SPLIT %	39%	61%	0%	0%	34%	SPLIT %	54%	46%	0%	0%	66%						



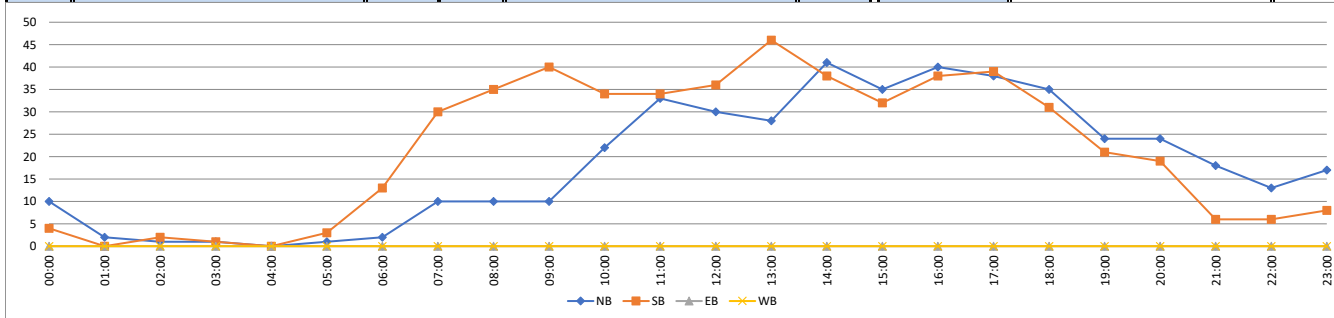
VOLUME

Greenhill Rd N/O Greenway PI

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_010

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS						
						445	516	0	0	961							
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	3	0			3	12:00	13	5			18	00:00	10	4			14
00:15	5	2			7	12:15	5	8			13	01:00	2	0			2
00:30	2	1			3	12:30	7	9			16	02:00	1	2			3
00:45	0	1			1	12:45	5	14			19	03:00	1	1			2
01:00	1	0			1	13:00	4	14			18	04:00	0	0			0
01:15	1	0			1	13:15	8	14			22	05:00	1	3			4
01:30	0	0			0	13:30	11	6			17	06:00	2	13			15
01:45	0	0			0	13:45	5	12			17	07:00	10	30			40
02:00	0	0			0	14:00	13	7			20	08:00	10	35			45
02:15	0	1			1	14:15	10	13			23	09:00	10	40			50
02:30	1	0			1	14:30	10	8			18	10:00	22	34			56
02:45	0	1			1	14:45	8	10			18	11:00	33	34			67
03:00	0	0			0	15:00	7	8			15	12:00	30	36			66
03:15	1	1			2	15:15	8	7			15	13:00	28	46			74
03:30	0	0			0	15:30	8	10			18	14:00	41	38			79
03:45	0	0			0	15:45	12	7			19	15:00	35	32			67
04:00	0	0			0	16:00	10	12			22	16:00	40	38			78
04:15	0	0			0	16:15	7	6			13	17:00	38	39			77
04:30	0	0			0	16:30	6	14			20	18:00	35	31			66
04:45	0	0			0	16:45	17	6			23	19:00	24	21			45
05:00	1	0			1	17:00	8	11			19	20:00	24	19			43
05:15	0	2			2	17:15	14	7			21	21:00	18	6			24
05:30	0	0			0	17:30	7	10			17	22:00	13	6			19
05:45	0	1			1	17:45	9	11			20	23:00	17	8			25
06:00	0	4			4	18:00	9	9			18	STATISTICS					
06:15	1	3			4	18:15	10	11			21		NB	SB	EB	WB	TOTAL
06:30	0	3			3	18:30	11	7			18	Peak Period	00:00 to 12:00				
06:45	1	3			4	18:45	5	4			9	Volume	102	196			298
07:00	2	4			6	19:00	7	4			11	Peak Hour	11:00 9:15				11:00
07:15	4	9			13	19:15	7	3			10	Peak Volume	33	43			67
07:30	4	9			13	19:30	8	8			16	Peak Hour Factor	0.688	0.827			0.728
07:45	0	8			8	19:45	2	6			8	Peak Period	12:00 to 00:00				
08:00	1	7			8	20:00	3	4			7	Volume	343	320			663
08:15	3	6			9	20:15	12	4			16	Peak Hour	16:45 12:30				16:30
08:30	4	10			14	20:30	5	5			10	Peak Volume	46	51			83
08:45	2	12			14	20:45	4	6			10	Peak Hour Factor	0.676	0.911			0.902
09:00	4	6			10	21:00	6	1			7	Peak Period	07:00 to 09:00				
09:15	1	13			14	21:15	4	2			6	Volume	20	65			85
09:30	3	9			12	21:30	2	2			4	Peak Hour	7:00 8:00				8:00
09:45	2	12			14	21:45	6	1			7	Peak Volume	10	35			45
10:00	9	9			18	22:00	2	3			5	Peak Hour Factor	0.625	0.729			0.804
10:15	4	5			9	22:15	3	1			4	Peak Period	16:00 to 18:00				
10:30	5	8			13	22:30	4	2			6	Volume	78	77			155
10:45	4	12			16	22:45	4	0			4	Peak Hour	16:45 17:00				16:30
11:00	10	7			17	23:00	5	3			8	Peak Volume	46	39			83
11:15	6	9			15	23:15	3	2			5	Peak Hour Factor	0.676	0.886			0.902
11:30	5	7			12	23:30	7	3			10						
11:45	12	11			23	23:45	2	0			2						
TOTALS	102	196	0	0	298	TOTALS	343	320	0	0	663						
SPLIT %	34%	66%	0%	0%	31%	SPLIT %	52%	48%	0%	0%	69%						



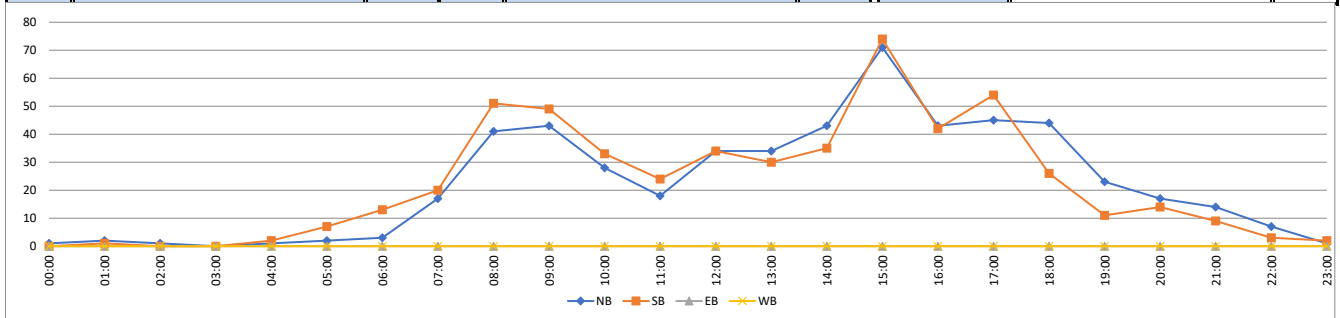
VOLUME

Hillside Rd N/O Northway Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_011

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					533	534	0	0	1,067									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	0	0			0	12:00	7	10			17	00:00	01:00	1	0			1
00:15	1	0			1	12:15	10	5			15	01:00	02:00	2	1			3
00:30	0	0			0	12:30	15	7			22	02:00	03:00	1	0			1
00:45	0	0			0	12:45	2	12			14	03:00	04:00	0	0			0
01:00	0	1			1	13:00	7	7			14	04:00	05:00	1	2			3
01:15	0	0			0	13:15	9	8			17	05:00	06:00	2	7			9
01:30	2	0			2	13:30	10	8			18	06:00	07:00	3	13			16
01:45	0	0			0	13:45	8	7			15	07:00	08:00	17	20			37
02:00	1	0			1	14:00	8	14			22	08:00	09:00	41	51			92
02:15	0	0			0	14:15	12	6			18	09:00	10:00	43	49			92
02:30	0	0			0	14:30	13	6			19	10:00	11:00	28	33			61
02:45	0	0			0	14:45	10	9			19	11:00	12:00	18	24			42
03:00	0	0			0	15:00	17	13			30	12:00	13:00	34	34			68
03:15	0	0			0	15:15	18	23			41	13:00	14:00	34	30			64
03:30	0	0			0	15:30	20	25			45	14:00	15:00	43	35			78
03:45	0	0			0	15:45	16	13			29	15:00	16:00	71	74			145
04:00	0	0			0	16:00	9	12			21	16:00	17:00	43	42			85
04:15	1	0			1	16:15	13	11			24	17:00	18:00	45	54			99
04:30	0	2			2	16:30	11	13			24	18:00	19:00	44	26			70
04:45	0	0			0	16:45	10	6			16	19:00	20:00	23	11			34
05:00	0	2			2	17:00	8	14			22	20:00	21:00	17	14			31
05:15	0	1			1	17:15	11	11			22	21:00	22:00	14	9			23
05:30	0	2			2	17:30	14	18			32	22:00	23:00	7	3			10
05:45	2	2			4	17:45	12	11			23	23:00	00:00	1	2			3
06:00	1	5			6	18:00	12	9			21	STATISTICS						
06:15	0	1			1	18:15	14	6			20							NB
06:30	1	3			4	18:30	8	7			15	Peak Period	00:00	to	12:00			
06:45	1	4			5	18:45	10	4			14	Volume	157	200			357	
07:00	6	5			11	19:00	9	5			14	Peak Hour	8:30	8:15			8:15	
07:15	2	2			4	19:15	4	4			8	Peak Volume	55	71			123	
07:30	5	6			11	19:30	7	0			7	Peak Hour Factor	0.724	0.612			0.641	
07:45	4	7			11	19:45	3	2			5	Peak Period	12:00	to	00:00			
08:00	8	9			17	20:00	8	7			15	Volume	376	334			710	
08:15	8	11			19	20:15	3	4			7	Peak Hour	15:00	15:00			15:00	
08:30	9	14			23	20:30	2	1			3	Peak Volume	71	74			145	
08:45	16	17			33	20:45	4	2			6	Peak Hour Factor	0.888	0.740			0.806	
09:00	19	29			48	21:00	6	1			7	Peak Period	07:00	to	09:00			
09:15	11	4			15	21:15	4	3			7	Volume	58	71			129	
09:30	7	11			18	21:30	3	3			6	Peak Hour	8:00	8:00			8:00	
09:45	6	5			11	21:45	1	2			3	Peak Volume	41	51			92	
10:00	6	6			12	22:00	2	1			3	Peak Hour Factor	0.641	0.750			0.697	
10:15	6	9			15	22:15	1	2			3	Peak Period	16:00	to	18:00			
10:30	9	8			17	22:30	2	0			2	Volume	88	96			184	
10:45	7	10			17	22:45	2	0			2	Peak Hour	17:00	17:00			17:00	
11:00	3	7			10	23:00	1	0			1	Peak Volume	45	54			99	
11:15	4	1			5	23:15	0	1			1	Peak Hour Factor	0.804	0.750			0.773	
11:30	6	7			13	23:30	0	1			1							
11:45	5	9			14	23:45	0	0			0							
TOTALS	157	200	0	0	357	TOTALS	376	334	0	0	710							
SPLIT %	44%	56%	0%	0%	33%	SPLIT %	53%	47%	0%	0%	67%							



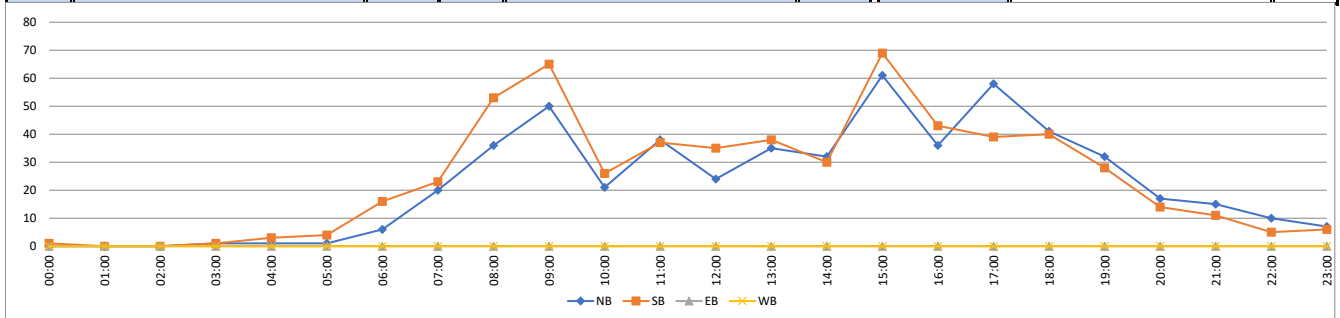
VOLUME

Hillside Rd N/O Northway Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_011

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					542	587	0	0	1,129								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	0			0	12:00	4	5			9	00:00	0	1			1
00:15	0	1			1	12:15	8	14			22	01:00	0	0			0
00:30	0	0			0	12:30	10	8			18	02:00	0	0			0
00:45	0	0			0	12:45	2	8			10	03:00	1	1			2
01:00	0	0			0	13:00	7	7			14	04:00	1	3			4
01:15	0	0			0	13:15	10	12			22	05:00	1	4			5
01:30	0	0			0	13:30	9	11			20	06:00	6	16			22
01:45	0	0			0	13:45	9	8			17	07:00	20	23			43
02:00	0	0			0	14:00	8	7			15	08:00	36	53			89
02:15	0	0			0	14:15	6	7			13	09:00	50	65			115
02:30	0	0			0	14:30	9	8			17	10:00	21	26			47
02:45	0	0			0	14:45	9	8			17	11:00	38	37			75
03:00	0	0			0	15:00	22	10			32	12:00	24	35			59
03:15	0	1			1	15:15	16	27			43	13:00	35	38			73
03:30	1	0			1	15:30	9	14			23	14:00	32	30			62
03:45	0	0			0	15:45	14	18			32	15:00	61	69			130
04:00	0	1			1	16:00	13	11			24	16:00	36	43			79
04:15	1	0			1	16:15	6	17			23	17:00	58	39			97
04:30	0	2			2	16:30	9	5			14	18:00	41	40			81
04:45	0	0			0	16:45	8	10			18	19:00	32	28			60
05:00	0	0			0	17:00	12	8			20	20:00	17	14			31
05:15	0	1			1	17:15	22	10			32	21:00	15	11			26
05:30	0	2			2	17:30	10	12			22	22:00	10	5			15
05:45	1	1			2	17:45	14	9			23	23:00	7	6			13
06:00	0	4			4	18:00	10	11			21	STATISTICS					
06:15	3	2			5	18:15	12	8			20		NB	SB	EB	WB	TOTAL
06:30	0	4			4	18:30	10	11			21	Peak Period	00:00 to 12:00				
06:45	3	6			9	18:45	9	10			19	Volume	174	229			403
07:00	4	4			8	19:00	11	9			20	Peak Hour	8:45	8:30			8:30
07:15	6	9			15	19:15	5	6			11	Peak Volume	55	83			136
07:30	6	5			11	19:30	8	8			16	Peak Hour Factor	0.809	0.576			0.642
07:45	4	5			9	19:45	8	5			13	Peak Period	12:00 to 00:00				
08:00	6	7			13	20:00	5	4			9	Volume	368	358			726
08:15	5	10			15	20:15	6	8			14	Peak Hour	15:00	15:15			15:00
08:30	9	17			26	20:30	3	1			4	Peak Volume	61	70			130
08:45	16	19			35	20:45	3	1			4	Peak Hour Factor	0.693	0.648			0.756
09:00	17	36			53	21:00	4	4			8	Peak Period	07:00 to 09:00				
09:15	11	11			22	21:15	3	2			5	Volume	56	76			132
09:30	11	12			23	21:30	3	3			6	Peak Hour	8:00	8:00			8:00
09:45	11	6			17	21:45	5	2			7	Peak Volume	36	53			89
10:00	6	4			10	22:00	3	0			3	Peak Hour Factor	0.563	0.697			0.636
10:15	6	7			13	22:15	3	2			5	Peak Period	16:00 to 18:00				
10:30	4	5			9	22:30	1	2			3	Volume	94	82			176
10:45	5	10			15	22:45	3	1			4	Peak Hour	17:00	16:00			17:00
11:00	10	10			20	23:00	3	4			7	Peak Volume	58	43			97
11:15	10	11			21	23:15	1	0			1	Peak Hour Factor	0.659	0.632			0.758
11:30	10	11			21	23:30	1	0			1						
11:45	8	5			13	23:45	2	2			4						
TOTALS	174	229	0	0	403	TOTALS	368	358	0	0	726						
SPLIT %	43%	57%	0%	0%	36%	SPLIT %	51%	49%	0%	0%	64%						



CLASSIFICATION
Hillside Rd N/O Northway Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_011

HOURLY BREAKDOWN table with columns for Time, NORTHBOUND (Total, #1-#13), SOUTHBOUND (Total, #1-#13), and TOTALS (Total, #1-#13). Rows include times from 00:00 to 23:00 and a Totals row with percentage breakdowns.

CLASSIFICATION DEFINITIONS diagram showing vehicle icons for: #1 Motorcycles, #2 Passenger Cars, #3 2-Axle, 4-Tire Single Unit, #4 Buses, #5 2-Axle, 6-Tire Single Units, #6 3-Axle Single Units, #7 >>4-Axle Single Units, #8 <=4-Axle Single Trailers, #9 5-Axle Single Trailers, #10 >>=6-Axle Single Trailers, #11 <=5-Axle Multi-Trailers, #12 6-Axle Multi-Trailers, #13 >>=7-Axle Multi-Trailers.

STATISTICS table showing peak hour data for time intervals: 00:00 - 12:00, 12:00 - 24:00, 07:00 - 09:00, and 16:00 - 18:00. Columns include Peak Hour, Peak Volume, and percentage breakdowns for all 13 vehicle classification categories.

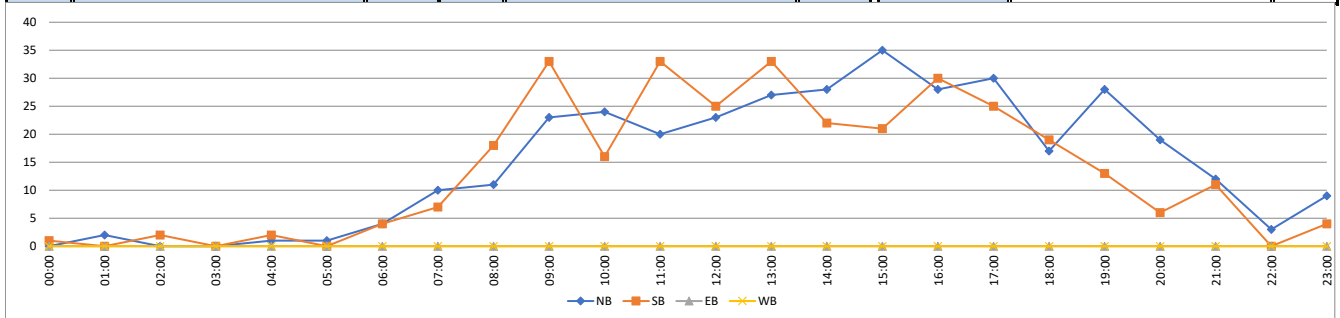
VOLUME

Hillside Rd N/O Northway Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_011

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					355	325	0	0	680								
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	1			1	12:00	4	7			11	00:00	0	1			1
00:15	0	0			0	12:15	4	5			9	01:00	2	0			2
00:30	0	0			0	12:30	6	6			12	02:00	0	0			2
00:45	0	0			0	12:45	9	7			16	03:00	0	0			0
01:00	2	0			2	13:00	9	11			20	04:00	1	2			3
01:15	0	0			0	13:15	8	9			17	05:00	1	0			1
01:30	0	0			0	13:30	4	5			9	06:00	4	4			8
01:45	0	0			0	13:45	6	8			14	07:00	10	7			17
02:00	0	0			0	14:00	7	6			13	08:00	11	18			29
02:15	0	1			1	14:15	5	5			10	09:00	23	33			56
02:30	0	0			0	14:30	7	3			10	10:00	24	16			40
02:45	0	1			1	14:45	9	8			17	11:00	20	33			53
03:00	0	0			0	15:00	11	4			15	12:00	23	25			48
03:15	0	0			0	15:15	11	1			12	13:00	27	33			60
03:30	0	0			0	15:30	4	8			12	14:00	28	22			50
03:45	0	0			0	15:45	9	8			17	15:00	35	21			56
04:00	0	0			0	16:00	4	7			11	16:00	28	30			58
04:15	1	0			1	16:15	7	7			14	17:00	30	25			55
04:30	0	1			1	16:30	6	6			12	18:00	17	19			36
04:45	0	1			1	16:45	11	10			21	19:00	28	13			41
05:00	0	0			0	17:00	6	7			13	20:00	19	6			25
05:15	0	0			0	17:15	12	9			21	21:00	12	11			23
05:30	0	0			0	17:30	6	8			14	22:00	3	0			3
05:45	1	0			1	17:45	6	1			7	23:00	9	4			13
06:00	0	0			0	18:00	3	4			7	STATISTICS					
06:15	0	0			0	18:15	5	3			8		NB	SB	EB	WB	TOTAL
06:30	2	1			3	18:30	4	7			11	Peak Period	00:00 to 12:00				
06:45	2	3			5	18:45	5	5			10	Volume	96	116			212
07:00	1	3			4	19:00	6	3			9	Peak Hour	9:45	9:00			9:00
07:15	3	3			6	19:15	11	4			15	Peak Volume	28	33			56
07:30	2	0			2	19:30	5	4			9	Peak Hour Factor	0.583	0.825			0.667
07:45	4	1			5	19:45	6	2			8	Peak Period	12:00 to 00:00				
08:00	0	7			7	20:00	7	2			9	Volume	259	209			468
08:15	2	4			6	20:15	4	2			6	Peak Hour	14:30	16:45			16:45
08:30	1	3			4	20:30	6	1			7	Peak Volume	38	34			69
08:45	8	4			12	20:45	2	1			3	Peak Hour Factor	0.864	0.850			0.821
09:00	5	5			10	21:00	3	2			5	Peak Period	07:00 to 09:00				
09:15	0	10			10	21:15	1	2			3	Volume	21	25			46
09:30	6	9			15	21:30	3	2			5	Peak Hour	8:00	8:00			8:00
09:45	12	9			21	21:45	5	5			10	Peak Volume	11	18			29
10:00	5	3			8	22:00	1	0			1	Peak Hour Factor	0.344	0.643			0.604
10:15	4	5			9	22:15	0	0			0	Peak Period	16:00 to 18:00				
10:30	7	2			9	22:30	1	0			1	Volume	58	55			113
10:45	8	6			14	22:45	1	0			1	Peak Hour	16:30	16:45			16:45
11:00	5	9			14	23:00	3	2			5	Peak Volume	35	34			69
11:15	3	9			12	23:15	3	0			3	Peak Hour Factor	0.729	0.850			0.821
11:30	6	7			13	23:30	2	1			3						
11:45	6	8			14	23:45	1	1			2						
TOTALS	96	116	0	0	212	TOTALS	259	209	0	0	468						
SPLIT %	45%	55%	0%	0%	31%	SPLIT %	55%	45%	0%	0%	69%						



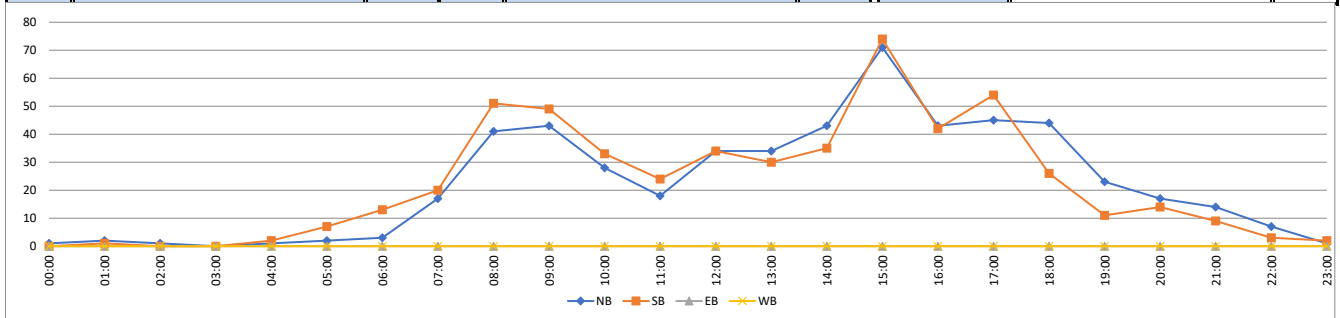
VOLUME

Hillside Rd N/O Northway Rd

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_011

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					533	534	0	0	1,067								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	0			0	12:00	7	10			17	00:00	01:00	1	0		1
00:15	1	0			1	12:15	10	5			15	01:00	02:00	2	1		3
00:30	0	0			0	12:30	15	7			22	02:00	03:00	1	0		1
00:45	0	0			0	12:45	2	12			14	03:00	04:00	0	0		0
01:00	0	1			1	13:00	7	7			14	04:00	05:00	1	2		3
01:15	0	0			0	13:15	9	8			17	05:00	06:00	2	7		9
01:30	2	0			2	13:30	10	8			18	06:00	07:00	3	13		16
01:45	0	0			0	13:45	8	7			15	07:00	08:00	17	20		37
02:00	1	0			1	14:00	8	14			22	08:00	09:00	41	51		92
02:15	0	0			0	14:15	12	6			18	09:00	10:00	43	49		92
02:30	0	0			0	14:30	13	6			19	10:00	11:00	28	33		61
02:45	0	0			0	14:45	10	9			19	11:00	12:00	18	24		42
03:00	0	0			0	15:00	17	13			30	12:00	13:00	34	34		68
03:15	0	0			0	15:15	18	23			41	13:00	14:00	34	30		64
03:30	0	0			0	15:30	20	25			45	14:00	15:00	43	35		78
03:45	0	0			0	15:45	16	13			29	15:00	16:00	71	74		145
04:00	0	0			0	16:00	9	12			21	16:00	17:00	43	42		85
04:15	1	0			1	16:15	13	11			24	17:00	18:00	45	54		99
04:30	0	2			2	16:30	11	13			24	18:00	19:00	44	26		70
04:45	0	0			0	16:45	10	6			16	19:00	20:00	23	11		34
05:00	0	2			2	17:00	8	14			22	20:00	21:00	17	14		31
05:15	0	1			1	17:15	11	11			22	21:00	22:00	14	9		23
05:30	0	2			2	17:30	14	18			32	22:00	23:00	7	3		10
05:45	2	2			4	17:45	12	11			23	23:00	00:00	1	2		3
06:00	1	5			6	18:00	12	9			21	STATISTICS					
06:15	0	1			1	18:15	14	6			20		NB	SB	EB	WB	TOTAL
06:30	1	3			4	18:30	8	7			15	Peak Period	00:00	to	12:00		
06:45	1	4			5	18:45	10	4			14	Volume	157	200			357
07:00	6	5			11	19:00	9	5			14	Peak Hour	8:30	8:15			8:15
07:15	2	2			4	19:15	4	4			8	Peak Volume	55	71			123
07:30	5	6			11	19:30	7	0			7	Peak Hour Factor	0.724	0.612			0.641
07:45	4	7			11	19:45	3	2			5	Peak Period	12:00	to	00:00		
08:00	8	9			17	20:00	8	7			15	Volume	376	334			710
08:15	8	11			19	20:15	3	4			7	Peak Hour	15:00	15:00			15:00
08:30	9	14			23	20:30	2	1			3	Peak Volume	71	74			145
08:45	16	17			33	20:45	4	2			6	Peak Hour Factor	0.888	0.740			0.806
09:00	19	29			48	21:00	6	1			7	Peak Period	07:00	to	09:00		
09:15	11	4			15	21:15	4	3			7	Volume	58	71			129
09:30	7	11			18	21:30	3	3			6	Peak Hour	8:00	8:00			8:00
09:45	6	5			11	21:45	1	2			3	Peak Volume	41	51			92
10:00	6	6			12	22:00	2	1			3	Peak Hour Factor	0.641	0.750			0.697
10:15	6	9			15	22:15	1	2			3	Peak Period	16:00	to	18:00		
10:30	9	8			17	22:30	2	0			2	Volume	88	96			184
10:45	7	10			17	22:45	2	0			2	Peak Hour	17:00	17:00			17:00
11:00	3	7			10	23:00	1	0			1	Peak Volume	45	54			99
11:15	4	1			5	23:15	0	1			1	Peak Hour Factor	0.804	0.750			0.773
11:30	6	7			13	23:30	0	1			1						
11:45	5	9			14	23:45	0	0			0						
TOTALS	157	200	0	0	357	TOTALS	376	334	0	0	710						
SPLIT %	44%	56%	0%	0%	33%	SPLIT %	53%	47%	0%	0%	67%						



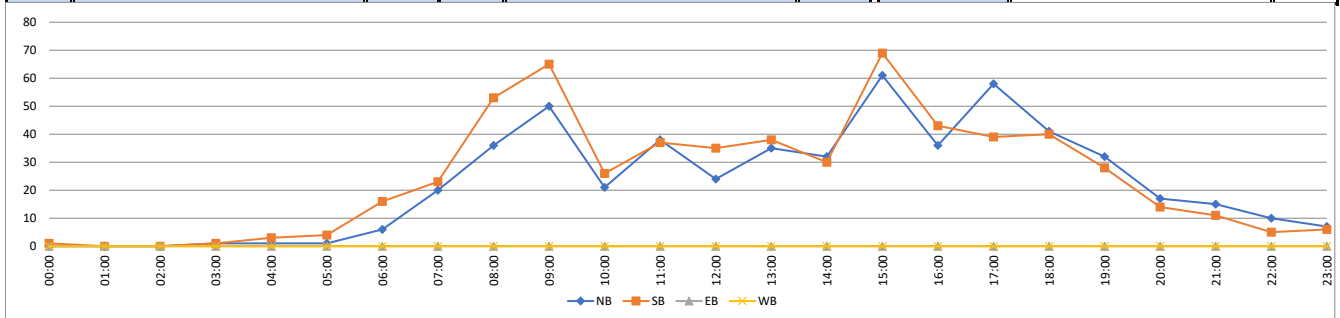
VOLUME

Hillside Rd N/O Northway Rd

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_011

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					542	587	0	0	1,129								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	0			0	12:00	4	5			9	00:00	0	1			1
00:15	0	1			1	12:15	8	14			22	01:00	0	0			0
00:30	0	0			0	12:30	10	8			18	02:00	0	0			0
00:45	0	0			0	12:45	2	8			10	03:00	1	1			2
01:00	0	0			0	13:00	7	7			14	04:00	1	3			4
01:15	0	0			0	13:15	10	12			22	05:00	1	4			5
01:30	0	0			0	13:30	9	11			20	06:00	6	16			22
01:45	0	0			0	13:45	9	8			17	07:00	20	23			43
02:00	0	0			0	14:00	8	7			15	08:00	36	53			89
02:15	0	0			0	14:15	6	7			13	09:00	50	65			115
02:30	0	0			0	14:30	9	8			17	10:00	21	26			47
02:45	0	0			0	14:45	9	8			17	11:00	38	37			75
03:00	0	0			0	15:00	22	10			32	12:00	24	35			59
03:15	0	1			1	15:15	16	27			43	13:00	35	38			73
03:30	1	0			1	15:30	9	14			23	14:00	32	30			62
03:45	0	0			0	15:45	14	18			32	15:00	61	69			130
04:00	0	1			1	16:00	13	11			24	16:00	36	43			79
04:15	1	0			1	16:15	6	17			23	17:00	58	39			97
04:30	0	2			2	16:30	9	5			14	18:00	41	40			81
04:45	0	0			0	16:45	8	10			18	19:00	32	28			60
05:00	0	0			0	17:00	12	8			20	20:00	17	14			31
05:15	0	1			1	17:15	22	10			32	21:00	15	11			26
05:30	0	2			2	17:30	10	12			22	22:00	10	5			15
05:45	1	1			2	17:45	14	9			23	23:00	7	6			13
06:00	0	4			4	18:00	10	11			21	STATISTICS					
06:15	3	2			5	18:15	12	8			20		NB	SB	EB	WB	TOTAL
06:30	0	4			4	18:30	10	11			21	Peak Period	00:00	to	12:00		
06:45	3	6			9	18:45	9	10			19	Volume	174	229			403
07:00	4	4			8	19:00	11	9			20	Peak Hour	8:45	8:30			8:30
07:15	6	9			15	19:15	5	6			11	Peak Volume	55	83			136
07:30	6	5			11	19:30	8	8			16	Peak Hour Factor	0.809	0.576			0.642
07:45	4	5			9	19:45	8	5			13	Peak Period	12:00	to	00:00		
08:00	6	7			13	20:00	5	4			9	Volume	368	358			726
08:15	5	10			15	20:15	6	8			14	Peak Hour	15:00	15:15			15:00
08:30	9	17			26	20:30	3	1			4	Peak Volume	61	70			130
08:45	16	19			35	20:45	3	1			4	Peak Hour Factor	0.693	0.648			0.756
09:00	17	36			53	21:00	4	4			8	Peak Period	07:00	to	09:00		
09:15	11	11			22	21:15	3	2			5	Volume	56	76			132
09:30	11	12			23	21:30	3	3			6	Peak Hour	8:00	8:00			8:00
09:45	11	6			17	21:45	5	2			7	Peak Volume	36	53			89
10:00	6	4			10	22:00	3	0			3	Peak Hour Factor	0.563	0.697			0.636
10:15	6	7			13	22:15	3	2			5	Peak Period	16:00	to	18:00		
10:30	4	5			9	22:30	1	2			3	Volume	94	82			176
10:45	5	10			15	22:45	3	1			4	Peak Hour	17:00	16:00			17:00
11:00	10	10			20	23:00	3	4			7	Peak Volume	58	43			97
11:15	10	11			21	23:15	1	0			1	Peak Hour Factor	0.659	0.632			0.758
11:30	10	11			21	23:30	1	0			1						
11:45	8	5			13	23:45	2	2			4						
TOTALS	174	229	0	0	403	TOTALS	368	358	0	0	726						
SPLIT %	43%	57%	0%	0%	36%	SPLIT %	51%	49%	0%	0%	64%						



SPEED

Hillside Rd N/O Northway Rd

Day: Saturday

Date: 11/18/2023

City: Greenbelt

Project #: MD23_280047_011

Time	NORTHBOUND														Total	SOUTHBOUND														Total	TOTALS														Total									
	5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70		5	15	20	25	30	35	40	45	50	55	60	65	70	70										
	15	20	25	30	35	40	45	50	55	60	65	70	99	15		20	25	30	35	40	45	50	55	60	65	70	99	15	20		25	30	35	40	45	50	55	60	65	70	99													
12:00	0	2	2	0	0	0	0	0	0	0	0	0	0	4	0	1	3	2	1	0	0	0	0	0	0	0	0	7	0	3	5	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11			
12:15	1	0	2	1	0	0	0	0	0	0	0	0	0	4	0	0	2	2	1	0	0	0	0	0	0	0	0	5	1	0	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
12:30	1	1	4	0	0	0	0	0	0	0	0	0	0	6	0	4	1	1	0	0	0	0	0	0	0	0	0	6	1	0	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12			
12:45	1	1	5	2	0	0	0	0	0	0	0	0	0	9	0	1	3	2	1	0	0	0	0	0	0	0	0	7	1	2	8	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16			
13:00	1	2	5	1	0	0	0	0	0	0	0	0	0	9	1	3	2	5	0	0	0	0	0	0	0	0	11	2	5	7	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20			
13:15	0	1	5	2	0	0	0	0	0	0	0	0	0	8	0	0	4	5	0	0	0	0	0	0	0	0	9	0	1	9	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17			
13:30	0	2	1	1	0	0	0	0	0	0	0	0	0	4	0	0	5	0	0	0	0	0	0	0	0	0	5	0	2	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9				
13:45	0	3	1	1	1	0	0	0	0	0	0	0	0	6	0	2	3	2	0	1	0	0	0	0	0	0	8	0	5	4	3	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14		
14:00	1	4	1	1	0	0	0	0	0	0	0	0	0	7	0	1	5	0	0	0	0	0	0	0	0	0	6	1	5	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13			
14:15	0	3	2	0	0	0	0	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	0	0	0	5	0	3	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10			
14:30	1	3	2	1	0	0	0	0	0	0	0	0	0	7	0	0	3	0	0	0	0	0	0	0	0	3	1	3	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10			
14:45	0	1	7	1	0	0	0	0	0	0	0	0	0	9	1	3	3	1	0	0	0	0	0	0	0	0	8	1	4	10	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17		
15:00	1	3	7	0	0	0	0	0	0	0	0	0	0	11	0	0	3	1	0	0	0	0	0	0	0	4	1	3	10	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	15		
15:15	1	3	6	1	0	0	0	0	0	0	0	0	0	11	0	0	1	0	0	0	0	0	0	0	0	0	1	1	3	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12		
15:30	1	1	2	0	0	0	0	0	0	0	0	0	0	4	1	4	1	2	0	0	0	0	0	0	0	0	8	2	5	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12		
15:45	0	4	5	0	0	0	0	0	0	0	0	0	0	9	2	1	4	1	0	0	0	0	0	0	0	0	8	2	5	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17	
16:00	1	0	2	1	0	0	0	0	0	0	0	0	0	4	0	1	2	4	0	0	0	0	0	0	0	0	7	1	1	1	4	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11		
16:15	0	1	6	0	0	0	0	0	0	0	0	0	0	7	0	0	4	3	0	0	0	0	0	0	0	0	7	0	1	10	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
16:30	2	4	0	0	0	0	0	0	0	0	0	0	0	6	1	1	3	0	0	0	0	1	0	0	0	6	3	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	
16:45	3	4	4	0	0	0	0	0	0	0	0	0	0	11	0	3	5	2	0	0	0	0	0	0	0	10	3	7	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21
17:00	1	4	1	0	0	0	0	0	0	0	0	0	0	6	0	2	2	3	0	0	0	0	0	0	0	7	1	6	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13		
17:15	3	5	3	1	0	0	0	0	0	0	0	0	0	12	1	4	3	1	0	0	0	0	0	0	0	9	4	9	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21	
17:30	0	2	4	0	0	0	0	0	0	0	0	0	0	6	0	2	4	2	0	0	0	0	0	0	0	8	0	4	8	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	
17:45	0	4	2	0	0	0	0	0	0	0	0	0	0	6	0	1	0	0	0	0	0	0	0	0	0	1	0	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7		
18:00	0	3	0	0	0	0	0	0	0	0	0	0	0	3	1	1	2	0	0	0	0	0	0	0	0	4	1	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7		
18:15	0	5	0	0	0	0	0	0	0	0	0	0	0	5	0	1	1	1	0	0	0	0	0	0	0	3	0	6	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8		
18:30	0	1	2	1	0	0	0	0	0	0	0	0	0	4	0	1	3	3	0	0	0	0	0	0	0	7	0	2	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11		
18:45	0	2	2	1	0	0	0	0	0	0	0	0	0	5	1	2	1	1	0	0	0	0	0	0	0	5	1	4	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10		
19:00	0	3	3	0	0	0	0	0	0	0	0	0	0	6	0	0	3	0	0	0	0	0	0	0	0	3	0	3	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
19:15	0	4	7	0	0	0	0	0	0	0	0	0	0	11	0	1	3	0	0	0	0	0	0	0	0	4	0	5	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	15		
19:30	0	2	3	0	0	0	0	0	0	0	0	0	0	5	0	1	2	1	0	0	0	0	0	0	0	4	0	3	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
19:45	1	2	2	1	0	0	0	0	0	0	0	0	0	6	0	1	0	1	0	0	0	0	0	0	0	2	1	3	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8		
20:00	0	4	2	1	0	0	0	0	0	0	0	0	0	7	0	1	0	1	0	0	0	0	0	0	0	2	0	5	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9		
20:15	0	1	3	0	0	0	0	0	0	0	0	0	0	4	0	0	1	1	0	0	0	0	0	0	0	2	0	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6		
20:30	1	3	2	0	0	0	0	0	0	0	0	0	0	6	0	0	0	1	0	0	0	0	0	0	0	1	1	3	2	0	1	0	0	0	0	0																		

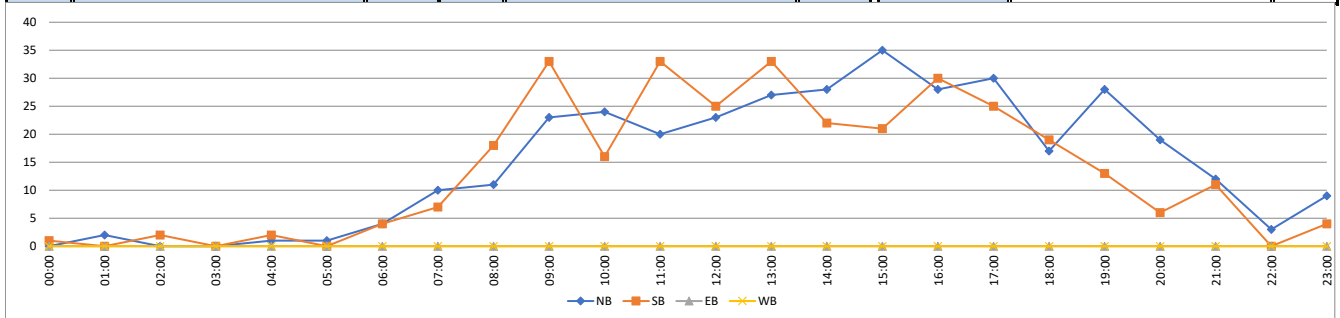
VOLUME

Hillside Rd N/O Northway Rd

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_011

DAILY TOTALS						NB	SB	EB	WB	Total	DAILY TOTALS						
						355	325	0	0	680							
15-Minutes Interval											Hourly Intervals						
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	1			1	12:00	4	7			11	00:00	0	1			1
00:15	0	0			0	12:15	4	5			9	01:00	2	0			2
00:30	0	0			0	12:30	6	6			12	02:00	0	0			2
00:45	0	0			0	12:45	9	7			16	03:00	0	0			0
01:00	2	0			2	13:00	9	11			20	04:00	1	2			3
01:15	0	0			0	13:15	8	9			17	05:00	1	0			1
01:30	0	0			0	13:30	4	5			9	06:00	4	4			8
01:45	0	0			0	13:45	6	8			14	07:00	10	7			17
02:00	0	0			0	14:00	7	6			13	08:00	11	18			29
02:15	0	1			1	14:15	5	5			10	09:00	23	33			56
02:30	0	0			0	14:30	7	3			10	10:00	24	16			40
02:45	0	1			1	14:45	9	8			17	11:00	20	33			53
03:00	0	0			0	15:00	11	4			15	12:00	23	25			48
03:15	0	0			0	15:15	11	1			12	13:00	27	33			60
03:30	0	0			0	15:30	4	8			12	14:00	28	22			50
03:45	0	0			0	15:45	9	8			17	15:00	35	21			56
04:00	0	0			0	16:00	4	7			11	16:00	28	30			58
04:15	1	0			1	16:15	7	7			14	17:00	30	25			55
04:30	0	1			1	16:30	6	6			12	18:00	17	19			36
04:45	0	1			1	16:45	11	10			21	19:00	28	13			41
05:00	0	0			0	17:00	6	7			13	20:00	19	6			25
05:15	0	0			0	17:15	12	9			21	21:00	12	11			23
05:30	0	0			0	17:30	6	8			14	22:00	3	0			3
05:45	1	0			1	17:45	6	1			7	23:00	9	4			13
06:00	0	0			0	18:00	3	4			7	STATISTICS					
06:15	0	0			0	18:15	5	3			8		NB	SB	EB	WB	TOTAL
06:30	2	1			3	18:30	4	7			11	Peak Period	00:00 to 12:00				
06:45	2	3			5	18:45	5	5			10	Volume	96	116			212
07:00	1	3			4	19:00	6	3			9	Peak Hour	9:45	9:00			9:00
07:15	3	3			6	19:15	11	4			15	Peak Volume	28	33			56
07:30	2	0			2	19:30	5	4			9	Peak Hour Factor	0.583	0.825			0.667
07:45	4	1			5	19:45	6	2			8	Peak Period	12:00 to 00:00				
08:00	0	7			7	20:00	7	2			9	Volume	259	209			468
08:15	2	4			6	20:15	4	2			6	Peak Hour	14:30	16:45			16:45
08:30	1	3			4	20:30	6	1			7	Peak Volume	38	34			69
08:45	8	4			12	20:45	2	1			3	Peak Hour Factor	0.864	0.850			0.821
09:00	5	5			10	21:00	3	2			5	Peak Period	07:00 to 09:00				
09:15	0	10			10	21:15	1	2			3	Volume	21	25			46
09:30	6	9			15	21:30	3	2			5	Peak Hour	8:00	8:00			8:00
09:45	12	9			21	21:45	5	5			10	Peak Volume	11	18			29
10:00	5	3			8	22:00	1	0			1	Peak Hour Factor	0.344	0.643			0.604
10:15	4	5			9	22:15	0	0			0	Peak Period	16:00 to 18:00				
10:30	7	2			9	22:30	1	0			1	Volume	58	55			113
10:45	8	6			14	22:45	1	0			1	Peak Hour	16:30	16:45			16:45
11:00	5	9			14	23:00	3	2			5	Peak Volume	35	34			69
11:15	3	9			12	23:15	3	0			3	Peak Hour Factor	0.729	0.850			0.821
11:30	6	7			13	23:30	2	1			3						
11:45	6	8			14	23:45	1	1			2						
TOTALS	96	116	0	0	212	TOTALS	259	209	0	0	468						
SPLIT %	45%	55%	0%	0%	31%	SPLIT %	55%	45%	0%	0%	69%						



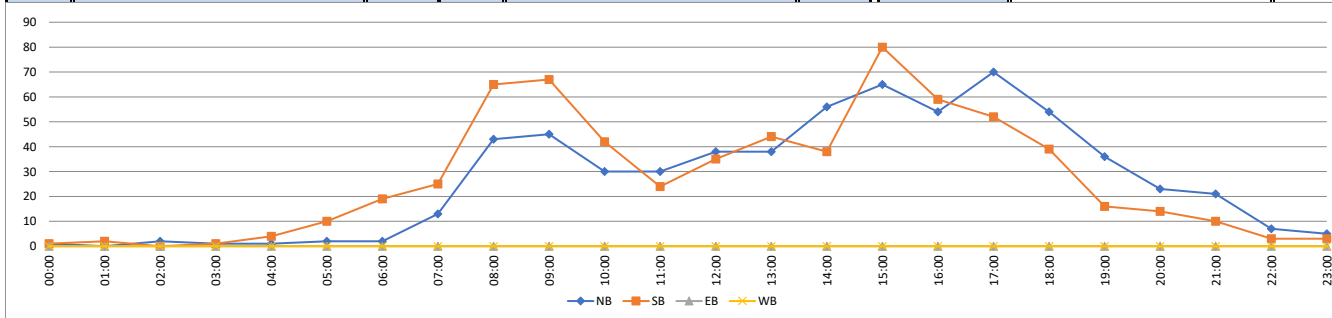
VOLUME

Hillside Rd S/O Woodland Way

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_012

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					637	653	0	0	1,290								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	0			0	12:00	10	11			21	00:00	01:00	1	1		2
00:15	1	0			1	12:15	9	8			17	01:00	02:00	0	2		2
00:30	0	1			1	12:30	13	8			21	02:00	03:00	2	0		2
00:45	0	0			0	12:45	6	8			14	03:00	04:00	1	1		2
01:00	0	2			2	13:00	9	11			20	04:00	05:00	1	4		5
01:15	0	0			0	13:15	10	13			23	05:00	06:00	2	10		12
01:30	0	0			0	13:30	12	10			22	06:00	07:00	2	19		21
01:45	0	0			0	13:45	7	10			17	07:00	08:00	13	25		38
02:00	1	0			1	14:00	9	11			20	08:00	09:00	43	65		108
02:15	0	0			0	14:15	15	10			25	09:00	10:00	45	67		112
02:30	1	0			1	14:30	21	10			31	10:00	11:00	30	42		72
02:45	0	0			0	14:45	11	7			18	11:00	12:00	30	24		54
03:00	0	0			0	15:00	16	19			35	12:00	13:00	38	35		73
03:15	0	0			0	15:15	16	19			35	13:00	14:00	38	44		82
03:30	1	0			1	15:30	16	25			41	14:00	15:00	56	38		94
03:45	0	1			1	15:45	17	17			34	15:00	16:00	65	80		145
04:00	0	1			1	16:00	13	13			26	16:00	17:00	54	59		113
04:15	1	0			1	16:15	12	14			26	17:00	18:00	70	52		122
04:30	0	2			2	16:30	13	18			31	18:00	19:00	54	39		93
04:45	0	1			1	16:45	16	14			30	19:00	20:00	36	16		52
05:00	0	1			1	17:00	11	16			27	20:00	21:00	23	14		37
05:15	0	3			3	17:15	16	9			25	21:00	22:00	21	10		31
05:30	0	4			4	17:30	20	16			36	22:00	23:00	7	3		10
05:45	2	2			4	17:45	23	11			34	23:00	00:00	5	3		8
06:00	0	5			5	18:00	15	11			26	STATISTICS					
06:15	0	1			1	18:15	14	10			24		NB	SB	EB	WB	TOTAL
06:30	0	4			4	18:30	12	7			19	Peak Period	00:00 to 12:00				
06:45	2	9			11	18:45	13	11			24	Volume	170	260			430
07:00	5	6			11	19:00	10	5			15	Peak Hour	8:30	8:15			8:30
07:15	3	6			9	19:15	8	7			15	Peak Volume	61	86			143
07:30	4	7			11	19:30	10	1			11	Peak Hour Factor	0.763	0.597			0.638
07:45	1	6			7	19:45	8	3			11	Peak Period	12:00 to 00:00				
08:00	7	15			22	20:00	8	6			14	Volume	467	393			860
08:15	7	15			22	20:15	1	5			6	Peak Hour	17:15	15:00			15:00
08:30	10	14			24	20:30	3	1			4	Peak Volume	74	80			145
08:45	19	21			40	20:45	11	2			13	Peak Hour Factor	0.804	0.800			0.884
09:00	20	36			56	21:00	9	2			11	Peak Period	07:00 to 09:00				
09:15	12	11			23	21:15	3	3			6	Volume	56	90			146
09:30	4	12			16	21:30	8	3			11	Peak Hour	8:00	8:00			8:00
09:45	9	8			17	21:45	1	2			3	Peak Volume	43	65			108
10:00	11	12			23	22:00	4	1			5	Peak Hour Factor	0.566	0.774			0.675
10:15	5	11			16	22:15	1	2			3	Peak Period	16:00 to 18:00				
10:30	7	6			13	22:30	2	0			2	Volume	124	111			235
10:45	7	13			20	22:45	0	0			0	Peak Hour	17:00	16:15			17:00
11:00	6	5			11	23:00	4	1			5	Peak Volume	70	62			122
11:15	8	2			10	23:15	0	1			1	Peak Hour Factor	0.761	0.861			0.847
11:30	6	8			14	23:30	1	1			2						
11:45	10	9			19	23:45	0	0			0						
TOTALS	170	260	0	0	430	TOTALS	467	393	0	0	860						
SPLIT %	40%	60%	0%	0%	33%	SPLIT %	54%	46%	0%	0%	67%						



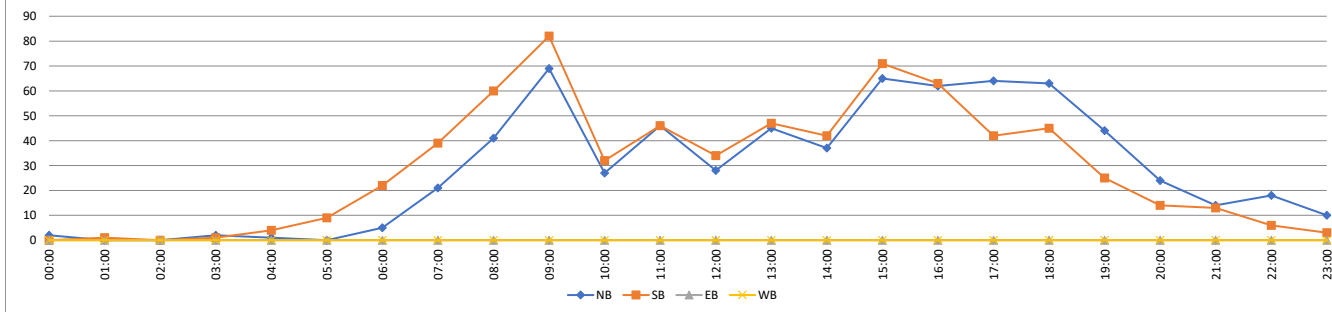
VOLUME

Hillside Rd S/O Woodland Way

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_012

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					688	701	0	0	1,389									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	0	0			0	12:00	7	6			13	00:00	01:00	2	0			2
00:15	2	0			2	12:15	8	15			23	01:00	02:00	0	1			1
00:30	0	0			0	12:30	9	5			14	02:00	03:00	0	0			0
00:45	0	0			0	12:45	4	8			12	03:00	04:00	2	1			3
01:00	0	0			0	13:00	10	13			23	04:00	05:00	1	4			5
01:15	0	0			0	13:15	7	13			20	05:00	06:00	0	9			9
01:30	0	1			1	13:30	15	15			30	06:00	07:00	5	22			27
01:45	0	0			0	13:45	13	6			19	07:00	08:00	21	39			60
02:00	0	0			0	14:00	9	11			20	08:00	09:00	41	60			101
02:15	0	0			0	14:15	8	9			17	09:00	10:00	69	82			151
02:30	0	0			0	14:30	12	9			21	10:00	11:00	27	32			59
02:45	0	0			0	14:45	8	13			21	11:00	12:00	46	46			92
03:00	0	0			0	15:00	23	7			30	12:00	13:00	28	34			62
03:15	0	1			1	15:15	14	27			41	13:00	14:00	45	47			92
03:30	1	0			1	15:30	11	18			29	14:00	15:00	37	42			79
03:45	1	0			1	15:45	17	19			36	15:00	16:00	65	71			136
04:00	0	2			2	16:00	19	16			35	16:00	17:00	62	63			125
04:15	1	0			1	16:15	17	20			37	17:00	18:00	64	42			106
04:30	0	2			2	16:30	13	10			23	18:00	19:00	63	45			108
04:45	0	0			0	16:45	13	17			30	19:00	20:00	44	25			69
05:00	0	2			2	17:00	20	7			27	20:00	21:00	24	14			38
05:15	0	3			3	17:15	19	12			31	21:00	22:00	14	13			27
05:30	0	2			2	17:30	11	14			25	22:00	23:00	18	6			24
05:45	0	2			2	17:45	14	9			23	23:00	00:00	10	3			13
06:00	0	5			5	18:00	11	11			22	STATISTICS						
06:15	2	2			4	18:15	20	12			32		NB	SB	EB	WB	TOTAL	
06:30	0	4			4	18:30	18	12			30	Peak Period	00:00 to 12:00					
06:45	3	11			14	18:45	14	10			24	Volume	214	296			510	
07:00	5	11			16	19:00	11	8			19	Peak Hour	8:30 8:30				8:30	
07:15	6	8			14	19:15	10	7			17	Peak Volume	69	98			167	
07:30	7	7			14	19:30	13	6			19	Peak Hour Factor	0.616	0.628			0.623	
07:45	3	13			16	19:45	10	4			14	Peak Period	12:00 to 00:00					
08:00	8	6			14	20:00	6	4			10	Volume	474	405			879	
08:15	6	11			17	20:15	6	7			13	Peak Hour	15:45 15:15				15:15	
08:30	12	23			35	20:30	5	3			8	Peak Volume	66	80			141	
08:45	15	20			35	20:45	7	0			7	Peak Hour Factor	0.868	0.741			0.860	
09:00	28	39			67	21:00	5	6			11	Peak Period	07:00 to 09:00					
09:15	14	16			30	21:15	1	1			2	Volume	62	99			161	
09:30	10	16			26	21:30	4	4			8	Peak Hour	8:00 8:00				8:00	
09:45	17	11			28	21:45	4	2			6	Peak Volume	41	60			101	
10:00	10	9			19	22:00	4	0			4	Peak Hour Factor	0.683	0.652			0.721	
10:15	5	5			10	22:15	4	2			6	Peak Period	16:00 to 18:00					
10:30	6	6			12	22:30	5	2			7	Volume	126	105			231	
10:45	6	12			18	22:45	5	2			7	Peak Hour	16:30 16:00				16:00	
11:00	13	14			27	23:00	3	1			4	Peak Volume	65	63			125	
11:15	12	12			24	23:15	2	0			2	Peak Hour Factor	0.813	0.788			0.845	
11:30	9	12			21	23:30	3	0			3							
11:45	12	8			20	23:45	2	2			4							
TOTALS	214	296	0	0	510	TOTALS	474	405	0	0	879							
SPLIT %	42%	58%	0%	0%	37%	SPLIT %	54%	46%	0%	0%	63%							



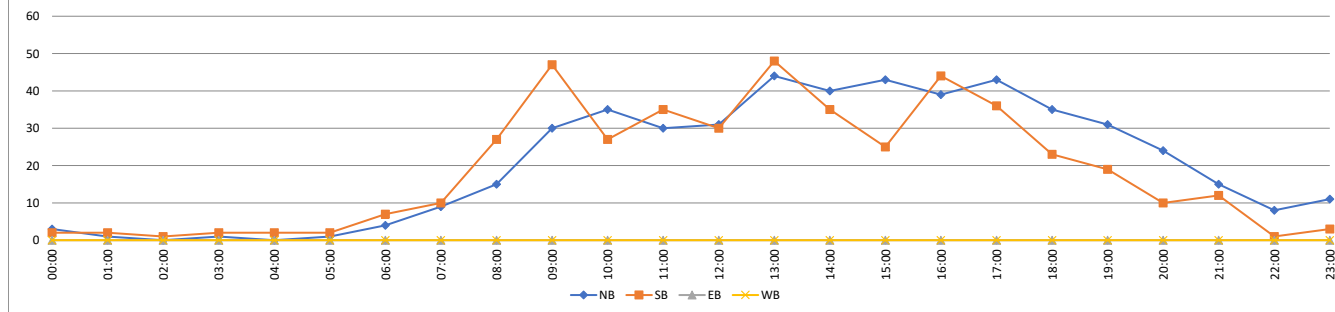
VOLUME

Hillside Rd S/O Woodland Way

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_012

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					493	450	0	0	943								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	1			1	12:00	4	10			14	00:00	01:00	3	2		5
00:15	1	1			2	12:15	5	3			8	01:00	02:00	1	2		3
00:30	1	0			1	12:30	8	8			16	02:00	03:00	0	1		1
00:45	1	0			1	12:45	14	9			23	03:00	04:00	1	2		3
01:00	1	0			1	13:00	11	12			23	04:00	05:00	0	2		2
01:15	0	0			0	13:15	10	11			21	05:00	06:00	1	2		3
01:30	0	2			2	13:30	12	11			23	06:00	07:00	4	7		11
01:45	0	0			0	13:45	11	14			25	07:00	08:00	9	10		19
02:00	0	0			0	14:00	8	8			16	08:00	09:00	15	27		42
02:15	0	0			0	14:15	9	10			19	09:00	10:00	30	47		77
02:30	0	0			0	14:30	12	10			22	10:00	11:00	35	27		62
02:45	0	1			1	14:45	11	7			18	11:00	12:00	30	35		65
03:00	0	1			1	15:00	15	4			19	12:00	13:00	31	30		61
03:15	0	0			0	15:15	10	4			14	13:00	14:00	44	48		92
03:30	0	0			0	15:30	6	9			15	14:00	15:00	40	35		75
03:45	1	1			2	15:45	12	8			20	15:00	16:00	43	25		68
04:00	0	0			0	16:00	8	7			15	16:00	17:00	39	44		83
04:15	0	0			0	16:15	12	14			26	17:00	18:00	43	36		79
04:30	0	1			1	16:30	5	12			17	18:00	19:00	35	23		58
04:45	0	1			1	16:45	14	11			25	19:00	20:00	31	19		50
05:00	0	1			1	17:00	9	10			19	20:00	21:00	24	10		34
05:15	1	1			2	17:15	14	11			25	21:00	22:00	15	12		27
05:30	0	0			0	17:30	10	10			20	22:00	23:00	8	1		9
05:45	0	0			0	17:45	10	5			15	23:00	00:00	11	3		14
06:00	0	0			0	18:00	11	6			17	STATISTICS					
06:15	0	1			1	18:15	10	4			14		NB	SB	EB	WB	TOTAL
06:30	2	2			4	18:30	7	8			15	Peak Period	00:00 to 12:00				
06:45	2	4			6	18:45	7	5			12	Volume	129	164			293
07:00	2	3			5	19:00	5	6			11	Peak Hour	10:15	9:15			9:30
07:15	1	3			4	19:15	11	3			14	Peak Volume	37	48			80
07:30	3	2			5	19:30	10	7			17	Peak Hour Factor	0.771	0.800			0.800
07:45	3	2			5	19:45	5	3			8	Peak Period	12:00 to 00:00				
08:00	2	9			11	20:00	9	4			13	Volume	364	286			650
08:15	5	2			7	20:15	3	3			6	Peak Hour	14:30	13:00			13:00
08:30	2	5			7	20:30	8	2			10	Peak Volume	48	48			92
08:45	6	11			17	20:45	4	1			5	Peak Hour Factor	0.800	0.857			0.920
09:00	6	5			11	21:00	2	2			4	Peak Period	07:00 to 09:00				
09:15	4	12			16	21:15	3	0			3	Volume	24	37			61
09:30	10	15			25	21:30	4	3			7	Peak Hour	8:00	8:00			8:00
09:45	10	15			25	21:45	6	7			13	Peak Volume	15	27			42
10:00	6	6			12	22:00	1	0			1	Peak Hour Factor	0.625	0.614			0.618
10:15	8	10			18	22:15	1	0			1	Peak Period	16:00 to 18:00				
10:30	9	4			13	22:30	2	1			3	Volume	82	80			162
10:45	12	7			19	22:45	4	0			4	Peak Hour	16:45	16:15			16:45
11:00	8	9			17	23:00	4	2			6	Peak Volume	47	47			89
11:15	6	10			16	23:15	3	1			4	Peak Hour Factor	0.839	0.839			0.890
11:30	7	6			13	23:30	2	0			2						
11:45	9	10			19	23:45	2	0			2						
TOTALS	129	164	0	0	293	TOTALS	364	286	0	0	650						
SPLIT %	44%	56%	0%	0%	31%	SPLIT %	56%	44%	0%	0%	69%						



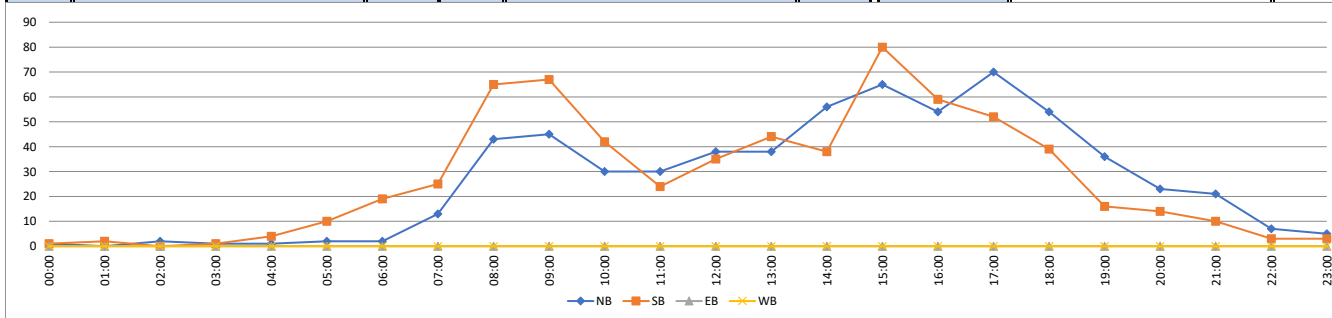
VOLUME

Hillside Rd S/O Woodland Way

Day: Thursday
Date: 11/16/2023

City: Greenbelt
Project #: MD23_280047_012

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					637	653	0	0	1,290								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	0			0	12:00	10	11			21	00:00	01:00	1	1		2
00:15	1	0			1	12:15	9	8			17	01:00	02:00	0	2		2
00:30	0	1			1	12:30	13	8			21	02:00	03:00	2	0		2
00:45	0	0			0	12:45	6	8			14	03:00	04:00	1	1		2
01:00	0	2			2	13:00	9	11			20	04:00	05:00	1	4		5
01:15	0	0			0	13:15	10	13			23	05:00	06:00	2	10		12
01:30	0	0			0	13:30	12	10			22	06:00	07:00	2	19		21
01:45	0	0			0	13:45	7	10			17	07:00	08:00	13	25		38
02:00	1	0			1	14:00	9	11			20	08:00	09:00	43	65		108
02:15	0	0			0	14:15	15	10			25	09:00	10:00	45	67		112
02:30	1	0			1	14:30	21	10			31	10:00	11:00	30	42		72
02:45	0	0			0	14:45	11	7			18	11:00	12:00	30	24		54
03:00	0	0			0	15:00	16	19			35	12:00	13:00	38	35		73
03:15	0	0			0	15:15	16	19			35	13:00	14:00	38	44		82
03:30	1	0			1	15:30	16	25			41	14:00	15:00	56	38		94
03:45	0	1			1	15:45	17	17			34	15:00	16:00	65	80		145
04:00	0	1			1	16:00	13	13			26	16:00	17:00	54	59		113
04:15	1	0			1	16:15	12	14			26	17:00	18:00	70	52		122
04:30	0	2			2	16:30	13	18			31	18:00	19:00	54	39		93
04:45	0	1			1	16:45	16	14			30	19:00	20:00	36	16		52
05:00	0	1			1	17:00	11	16			27	20:00	21:00	23	14		37
05:15	0	3			3	17:15	16	9			25	21:00	22:00	21	10		31
05:30	0	4			4	17:30	20	16			36	22:00	23:00	7	3		10
05:45	2	2			4	17:45	23	11			34	23:00	00:00	5	3		8
06:00	0	5			5	18:00	15	11			26	STATISTICS					
06:15	0	1			1	18:15	14	10			24		NB	SB	EB	WB	TOTAL
06:30	0	4			4	18:30	12	7			19	Peak Period	00:00 to 12:00				
06:45	2	9			11	18:45	13	11			24	Volume	170	260			430
07:00	5	6			11	19:00	10	5			15	Peak Hour	8:30	8:15			8:30
07:15	3	6			9	19:15	8	7			15	Peak Volume	61	86			143
07:30	4	7			11	19:30	10	1			11	Peak Hour Factor	0.763	0.597			0.638
07:45	1	6			7	19:45	8	3			11	Peak Period	12:00 to 00:00				
08:00	7	15			22	20:00	8	6			14	Volume	467	393			860
08:15	7	15			22	20:15	1	5			6	Peak Hour	17:15	15:00			15:00
08:30	10	14			24	20:30	3	1			4	Peak Volume	74	80			145
08:45	19	21			40	20:45	11	2			13	Peak Hour Factor	0.804	0.800			0.884
09:00	20	36			56	21:00	9	2			11	Peak Period	07:00 to 09:00				
09:15	12	11			23	21:15	3	3			6	Volume	56	90			146
09:30	4	12			16	21:30	8	3			11	Peak Hour	8:00	8:00			8:00
09:45	9	8			17	21:45	1	2			3	Peak Volume	43	65			108
10:00	11	12			23	22:00	4	1			5	Peak Hour Factor	0.566	0.774			0.675
10:15	5	11			16	22:15	1	2			3	Peak Period	16:00 to 18:00				
10:30	7	6			13	22:30	2	0			2	Volume	124	111			235
10:45	7	13			20	22:45	0	0			0	Peak Hour	17:00	16:15			17:00
11:00	6	5			11	23:00	4	1			5	Peak Volume	70	62			122
11:15	8	2			10	23:15	0	1			1	Peak Hour Factor	0.761	0.861			0.847
11:30	6	8			14	23:30	1	1			2						
11:45	10	9			19	23:45	0	0			0						
TOTALS	170	260	0	0	430	TOTALS	467	393	0	0	860						
SPLIT %	40%	60%	0%	0%	33%	SPLIT %	54%	46%	0%	0%	67%						



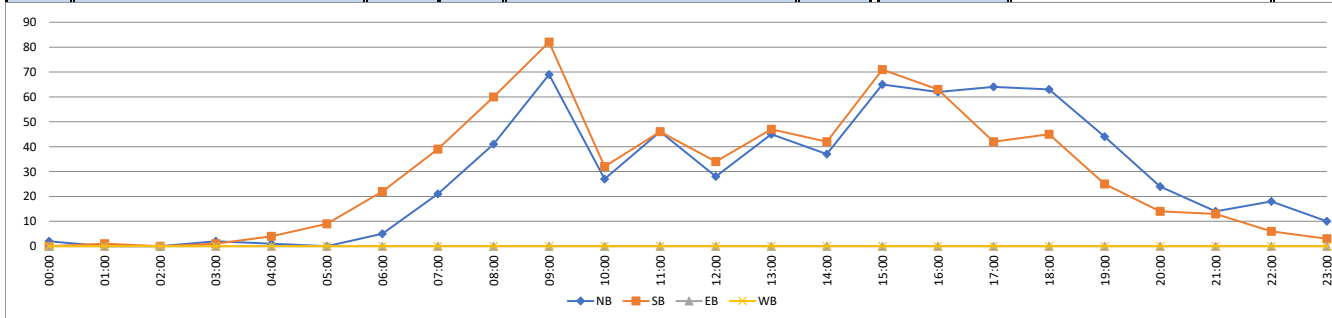
VOLUME

Hillside Rd S/O Woodland Way

Day: Friday
Date: 11/17/2023

City: Greenbelt
Project #: MD23_280047_012

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS								
					688	701	0	0	1,389									
15-Minutes Interval										Hourly Intervals								
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	
00:00	0	0			0	12:00	7	6			13	00:00	01:00	2	0			2
00:15	2	0			2	12:15	8	15			23	01:00	02:00	0	1			1
00:30	0	0			0	12:30	9	5			14	02:00	03:00	0	0			0
00:45	0	0			0	12:45	4	8			12	03:00	04:00	2	1			3
01:00	0	0			0	13:00	10	13			23	04:00	05:00	1	4			5
01:15	0	0			0	13:15	7	13			20	05:00	06:00	0	9			9
01:30	0	1			1	13:30	15	15			30	06:00	07:00	5	22			27
01:45	0	0			0	13:45	13	6			19	07:00	08:00	21	39			60
02:00	0	0			0	14:00	9	11			20	08:00	09:00	41	60			101
02:15	0	0			0	14:15	8	9			17	09:00	10:00	69	82			151
02:30	0	0			0	14:30	12	9			21	10:00	11:00	27	32			59
02:45	0	0			0	14:45	8	13			21	11:00	12:00	46	46			92
03:00	0	0			0	15:00	23	7			30	12:00	13:00	28	34			62
03:15	0	1			1	15:15	14	27			41	13:00	14:00	45	47			92
03:30	1	0			1	15:30	11	18			29	14:00	15:00	37	42			79
03:45	1	0			1	15:45	17	19			36	15:00	16:00	65	71			136
04:00	0	2			2	16:00	19	16			35	16:00	17:00	62	63			125
04:15	1	0			1	16:15	17	20			37	17:00	18:00	64	42			106
04:30	0	2			2	16:30	13	10			23	18:00	19:00	63	45			108
04:45	0	0			0	16:45	13	17			30	19:00	20:00	44	25			69
05:00	0	2			2	17:00	20	7			27	20:00	21:00	24	14			38
05:15	0	3			3	17:15	19	12			31	21:00	22:00	14	13			27
05:30	0	2			2	17:30	11	14			25	22:00	23:00	18	6			24
05:45	0	2			2	17:45	14	9			23	23:00	00:00	10	3			13
06:00	0	5			5	18:00	11	11			22	STATISTICS						
06:15	2	2			4	18:15	20	12			32		NB	SB	EB	WB	TOTAL	
06:30	0	4			4	18:30	18	12			30	Peak Period	00:00 to 12:00					
06:45	3	11			14	18:45	14	10			24	Volume	214	296			510	
07:00	5	11			16	19:00	11	8			19	Peak Hour	8:30 8:30				8:30	
07:15	6	8			14	19:15	10	7			17	Peak Volume	69	98			167	
07:30	7	7			14	19:30	13	6			19	Peak Hour Factor	0.616	0.628			0.623	
07:45	3	13			16	19:45	10	4			14	Peak Period	12:00 to 00:00					
08:00	8	6			14	20:00	6	4			10	Volume	474	405			879	
08:15	6	11			17	20:15	6	7			13	Peak Hour	15:45 15:15				15:15	
08:30	12	23			35	20:30	5	3			8	Peak Volume	66	80			141	
08:45	15	20			35	20:45	7	0			7	Peak Hour Factor	0.868	0.741			0.860	
09:00	28	39			67	21:00	5	6			11	Peak Period	07:00 to 09:00					
09:15	14	16			30	21:15	1	1			2	Volume	62	99			161	
09:30	10	16			26	21:30	4	4			8	Peak Hour	8:00 8:00				8:00	
09:45	17	11			28	21:45	4	2			6	Peak Volume	41	60			101	
10:00	10	9			19	22:00	4	0			4	Peak Hour Factor	0.683	0.652			0.721	
10:15	5	5			10	22:15	4	2			6	Peak Period	16:00 to 18:00					
10:30	6	6			12	22:30	5	2			7	Volume	126	105			231	
10:45	6	12			18	22:45	5	2			7	Peak Hour	16:30 16:00				16:00	
11:00	13	14			27	23:00	3	1			4	Peak Volume	65	63			125	
11:15	12	12			24	23:15	2	0			2	Peak Hour Factor	0.813	0.788			0.845	
11:30	9	12			21	23:30	3	0			3							
11:45	12	8			20	23:45	2	2			4							
TOTALS	214	296	0	0	510	TOTALS	474	405	0	0	879							
SPLIT %	42%	58%	0%	0%	37%	SPLIT %	54%	46%	0%	0%	63%							



SPEED Hillside Rd S/O Woodland Way

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_012

Time	NORTHBOUND											Total	SOUTHBOUND											Total	TOTALS											Total											
	5	15	20	25	30	35	40	45	50	55	60		65	70	70	99	5	15	20	25	30	35	40		45	50	55	60	65	70	70	99	5	15	20		25	30	35	40	45	50	55	60	65	70	70
00:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	3	1	1	0	0	0	0	0	0	0	0	0	0	0	5
01:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	1	0	0	0	0	0	0	0	2	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	3		
02:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1		
03:00	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2		
04:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	1	0	0	0	0	0	0	2	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	3	
05:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	1	0	0	0	0	0	0	0	2	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	3	
06:00	1	0	1	1	1	0	1	0	0	0	0	0	0	0	0	4	0	0	3	3	1	0	0	0	0	0	0	0	7	1	0	1	4	3	2	0	0	0	0	0	0	0	0	0	11		
07:00	0	0	3	2	4	0	0	0	0	0	0	0	0	0	9	0	1	1	5	3	0	0	0	0	0	0	0	10	0	1	4	7	7	0	0	0	0	0	0	0	0	0	0	19			
08:00	1	0	5	4	5	0	0	0	0	0	0	0	0	0	15	0	1	5	14	5	2	0	0	0	0	0	0	27	1	1	10	18	10	2	0	0	0	0	0	0	0	0	0	42			
09:00	0	1	13	12	3	1	0	0	0	0	0	0	0	0	30	0	5	16	18	6	2	0	0	0	0	0	0	47	0	6	29	30	9	3	0	0	0	0	0	0	0	0	0	77			
10:00	2	2	15	11	4	1	0	0	0	0	0	0	0	0	35	0	0	6	15	5	1	0	0	0	0	0	0	27	2	2	21	26	9	2	0	0	0	0	0	0	0	0	62				
11:00	0	0	9	17	4	0	0	0	0	0	0	0	0	0	30	0	2	8	18	7	0	0	0	0	0	0	0	35	0	2	17	35	11	0	0	0	0	0	0	0	0	0	65				
12:00	2	4	9	14	2	0	0	0	0	0	0	0	0	0	31	0	1	13	10	5	1	0	0	0	0	0	0	30	2	5	22	24	7	1	0	0	0	0	0	0	0	0	61				
13:00	1	4	12	20	6	1	0	0	0	0	0	0	0	0	44	2	5	14	20	5	1	1	0	0	0	0	0	48	3	9	26	40	11	2	1	0	0	0	0	0	0	0	92				
14:00	1	6	10	16	7	0	0	0	0	0	0	0	0	0	40	1	2	8	17	6	1	0	0	0	0	0	0	35	2	8	18	33	13	1	0	0	0	0	0	0	0	0	75				
15:00	0	5	11	21	5	1	0	0	0	0	0	0	0	0	43	1	0	8	11	5	0	0	0	0	0	0	0	25	1	5	19	32	10	1	0	0	0	0	0	0	0	68					
16:00	0	5	12	17	4	1	0	0	0	0	0	0	0	0	39	1	3	16	17	5	0	1	1	0	0	0	0	44	1	8	28	34	9	1	1	0	0	0	0	0	0	83					
17:00	1	4	15	19	4	0	0	0	0	0	0	0	0	0	43	0	2	11	15	8	0	0	0	0	0	0	0	36	1	6	26	34	12	0	0	0	0	0	0	0	0	79					
18:00	1	6	8	19	1	0	0	0	0	0	0	0	0	0	35	0	2	9	10	2	0	0	0	0	0	0	0	23	1	8	17	29	3	0	0	0	0	0	0	0	0	0	58				
19:00	0	1	12	15	3	0	0	0	0	0	0	0	0	0	31	0	1	9	5	4	0	0	0	0	0	0	0	19	0	2	21	20	7	0	0	0	0	0	0	0	0	0	50				
20:00	0	1	6	11	6	0	0	0	0	0	0	0	0	0	24	0	0	2	5	2	1	0	0	0	0	0	0	10	0	1	8	16	8	1	0	0	0	0	0	0	0	0	34				
21:00	0	0	5	7	2	1	0	0	0	0	0	0	0	0	15	0	1	5	4	2	0	0	0	0	0	0	0	12	0	1	10	11	4	1	0	0	0	0	0	0	0	0	27				
22:00	0	0	1	7	0	0	0	0	0	0	0	0	0	0	8	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	8	0	0	0	0	0	0	0	0	0	0	9				
23:00	1	1	4	4	1	0	0	0	0	0	0	0	0	0	11	0	0	1	1	1	0	0	0	0	0	0	0	3	1	1	5	5	2	0	0	0	0	0	0	0	0	0	0	14			
Totals	12	40	154	218	62	7	0	0	0	0	0	0	0	493	5	27	134	193	76	12	2	1	0	0	0	450	17	67	288	411	138	19	2	1	0	0	0	0	0	0	943						
% of Totals	2%	8%	31%	44%	13%	1%								100%	1%	6%	30%	43%	17%	3%	0%	0%				100%	2%	7%	31%	44%	15%	2%	0%	0%									100%				

Direction	Percentiles					
	15th	50th	Average	85th	95th	ADT
NORTHBOUND	21	26	25	30	34	493
SOUTHBOUND	21	27	26	32	35	450
TOTALS	21	26	26	31	34	943

STATISTICS	00:00 - 12:00		12:00 - 24:00		07:00 - 09:00		16:00 - 18:00	
	%	Peak Volume	%	Peak Volume	%	Peak Volume	%	Peak Volume
00:00 - 12:00	5%	3	49%	48	21%	3	0%	0
12:00 - 24:00	7%	37	105%	170	41%	4	0%	0
07:00 - 09:00	1%	0	8%	6	9%	0	0%	0
16:00 - 18:00	1%	9	27%	36	8%	1	0%	0

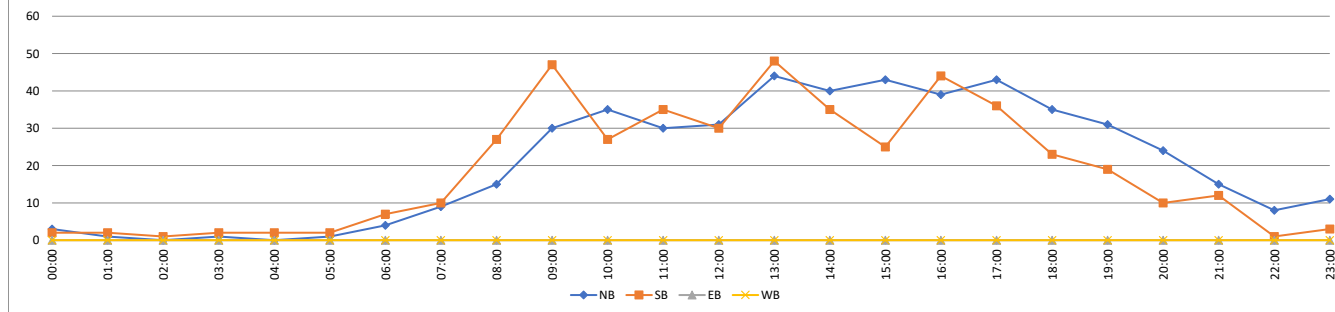
VOLUME

Hillside Rd S/O Woodland Way

Day: Saturday
Date: 11/18/2023

City: Greenbelt
Project #: MD23_280047_012

DAILY TOTALS					NB	SB	EB	WB	Total	DAILY TOTALS							
					493	450	0	0	943								
15-Minutes Interval										Hourly Intervals							
TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL	TIME	NB	SB	EB	WB	TOTAL
00:00	0	1			1	12:00	4	10			14	00:00	01:00	3	2		5
00:15	1	1			2	12:15	5	3			8	01:00	02:00	1	2		3
00:30	1	0			1	12:30	8	8			16	02:00	03:00	0	1		1
00:45	1	0			1	12:45	14	9			23	03:00	04:00	1	2		3
01:00	1	0			1	13:00	11	12			23	04:00	05:00	0	2		2
01:15	0	0			0	13:15	10	11			21	05:00	06:00	1	2		3
01:30	0	2			2	13:30	12	11			23	06:00	07:00	4	7		11
01:45	0	0			0	13:45	11	14			25	07:00	08:00	9	10		19
02:00	0	0			0	14:00	8	8			16	08:00	09:00	15	27		42
02:15	0	0			0	14:15	9	10			19	09:00	10:00	30	47		77
02:30	0	0			0	14:30	12	10			22	10:00	11:00	35	27		62
02:45	0	1			1	14:45	11	7			18	11:00	12:00	30	35		65
03:00	0	1			1	15:00	15	4			19	12:00	13:00	31	30		61
03:15	0	0			0	15:15	10	4			14	13:00	14:00	44	48		92
03:30	0	0			0	15:30	6	9			15	14:00	15:00	40	35		75
03:45	1	1			2	15:45	12	8			20	15:00	16:00	43	25		68
04:00	0	0			0	16:00	8	7			15	16:00	17:00	39	44		83
04:15	0	0			0	16:15	12	14			26	17:00	18:00	43	36		79
04:30	0	1			1	16:30	5	12			17	18:00	19:00	35	23		58
04:45	0	1			1	16:45	14	11			25	19:00	20:00	31	19		50
05:00	0	1			1	17:00	9	10			19	20:00	21:00	24	10		34
05:15	1	1			2	17:15	14	11			25	21:00	22:00	15	12		27
05:30	0	0			0	17:30	10	10			20	22:00	23:00	8	1		9
05:45	0	0			0	17:45	10	5			15	23:00	00:00	11	3		14
06:00	0	0			0	18:00	11	6			17	STATISTICS					
06:15	0	1			1	18:15	10	4			14		NB	SB	EB	WB	TOTAL
06:30	2	2			4	18:30	7	8			15	Peak Period	00:00 to 12:00				
06:45	2	4			6	18:45	7	5			12	Volume	129	164			293
07:00	2	3			5	19:00	5	6			11	Peak Hour	10:15	9:15			9:30
07:15	1	3			4	19:15	11	3			14	Peak Volume	37	48			80
07:30	3	2			5	19:30	10	7			17	Peak Hour Factor	0.771	0.800			0.800
07:45	3	2			5	19:45	5	3			8	Peak Period	12:00 to 00:00				
08:00	2	9			11	20:00	9	4			13	Volume	364	286			650
08:15	5	2			7	20:15	3	3			6	Peak Hour	14:30	13:00			13:00
08:30	2	5			7	20:30	8	2			10	Peak Volume	48	48			92
08:45	6	11			17	20:45	4	1			5	Peak Hour Factor	0.800	0.857			0.920
09:00	6	5			11	21:00	2	2			4	Peak Period	07:00 to 09:00				
09:15	4	12			16	21:15	3	0			3	Volume	24	37			61
09:30	10	15			25	21:30	4	3			7	Peak Hour	8:00	8:00			8:00
09:45	10	15			25	21:45	6	7			13	Peak Volume	15	27			42
10:00	6	6			12	22:00	1	0			1	Peak Hour Factor	0.625	0.614			0.618
10:15	8	10			18	22:15	1	0			1	Peak Period	16:00 to 18:00				
10:30	9	4			13	22:30	2	1			3	Volume	82	80			162
10:45	12	7			19	22:45	4	0			4	Peak Hour	16:45	16:15			16:45
11:00	8	9			17	23:00	4	2			6	Peak Volume	47	47			89
11:15	6	10			16	23:15	3	1			4	Peak Hour Factor	0.839	0.839			0.890
11:30	7	6			13	23:30	2	0			2						
11:45	9	10			19	23:45	2	0			2						
TOTALS	129	164	0	0	293	TOTALS	364	286	0	0	650						
SPLIT %	44%	56%	0%	0%	31%	SPLIT %	56%	44%	0%	0%	69%						



APPENDIX B – PUBLIC TRANSPORTATION ROUTES

How to use this timetable

- Use the map to find the stops closest to where you will get on and off the bus.
- Select the schedule (Weekday, Saturday, Sunday) for when you will travel. Along the top of the schedule, find the stop at or nearest the point where you will get on the bus. Follow that column down to the time you want to leave.
- Use the same method to find the times the bus is scheduled to arrive at the stop where you will get off the bus.
- If the bus stop is not listed, use the time shown for the bus stop before it as the time to wait at the stop.
- The end-of-the-line or last stop is listed in ALL CAPS on the schedule.

Cómo Usar este Horario

- Use este mapa para localizar las paradas más cercanas a donde se subirá y bajará del autobús.
- Seleccione el horario (Entre semana, sábado, domingo) de cuando viajará. A lo largo de la parte superior del horario, localice la parada o el punto más cercano a la parada en la que se subirá al autobús. Siga esa columna hacia abajo hasta la hora en la que desee salir.
- Utilice el mismo método para localizar las horas en que el autobús está programado para llegar a la parada en donde desea bajarse del autobús.
- Si la parada del autobús no está listada use la hora que se muestra en la parada anterior como la hora de espera en la parada.
- El final de la ruta o la última parada del autobús aparece en letras MAYÚSCULAS en el horario.

English-Español

Effective 12-11-22

G12

Greenbelt-New Carrollton Line

metrobus



**Serves these locations-
Brinda servicio a estas ubicaciones**

- Greenbelt station
- Capital Office Park
- North End, Old Greenbelt
- Roosevelt Center
- South End, Old Greenbelt
- Greenbelt East
- Doctors Community Hospital
- Lanham
- New Carrollton station



www.wmata.com

Information Anytime 202-637-7000 TTY 202-962-2033



**Washington
Metropolitan Area
Transit Authority**

*A District of Columbia,
Maryland and Virginia
Transit Partnership*

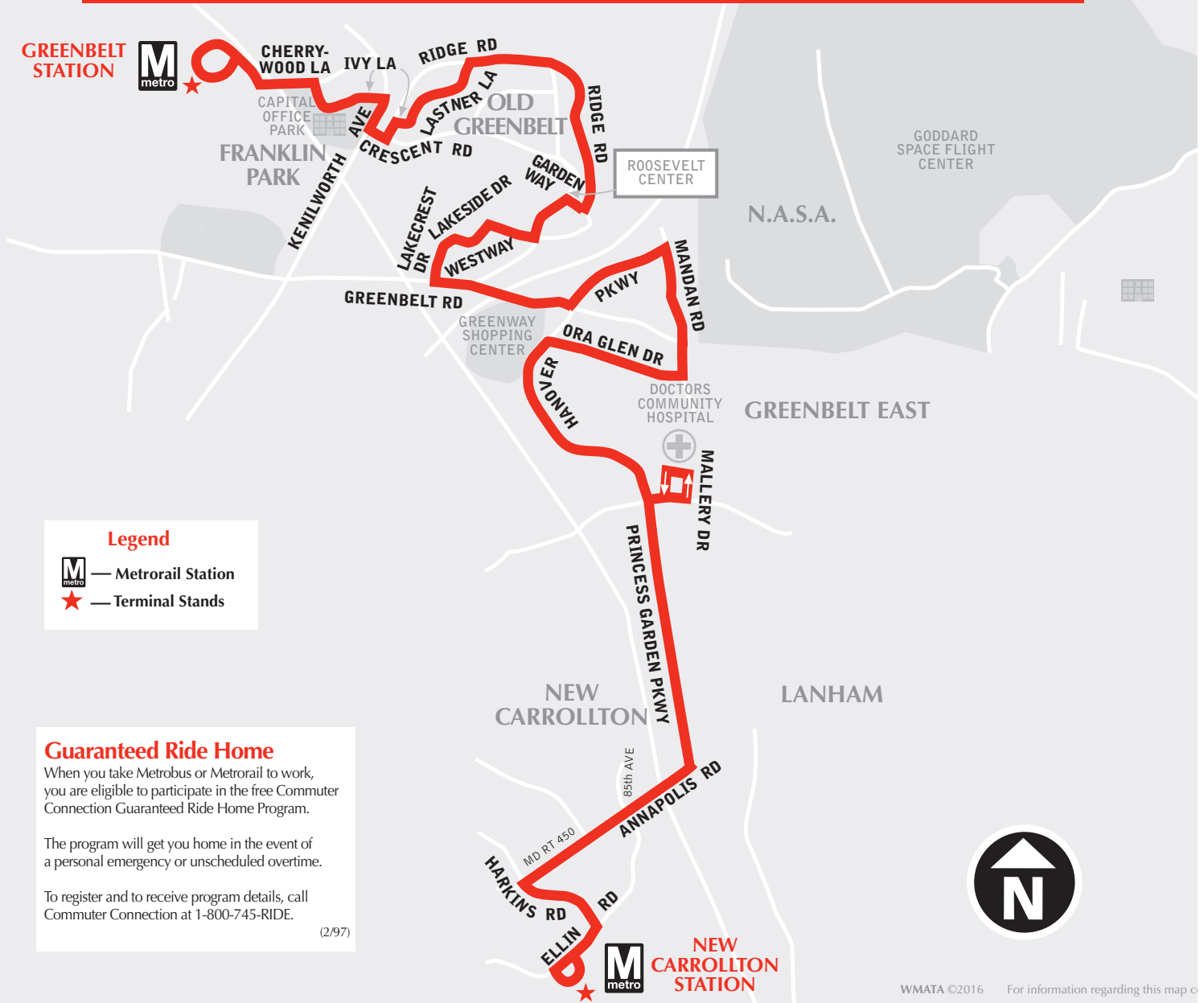
G12

Greenbelt-New Carrollton Line

For route and schedule information



Call 202-637-7000

www.wmata.com





▶ Eastbound to New Carrollton station

Monday thru Friday — Lunes a viernes

Route Number	Greenbelt 	Ivy La. & Ridge Rd.	Crescent Rd. & Gardenway (Roosevelt Center)	Greenbelt Rd. at Greenway Shopping Center	Ora Glen Dr. & Mandan Rd.	Doctors Community Hospital	Annapolis Rd. & 85th Ave.	NEW CARROLLTON 
AM Service — Servicio matutino								
G12	-	-	5:30	5:36	5:41	5:48	5:55	5:59
G12	6:00	6:05	6:13	6:19	6:24	6:31	6:38	6:42
G12	6:30	6:35	6:43	6:49	6:54	7:01	7:08	7:12
G12	7:00	7:05	7:13	7:19	7:24	7:31	7:38	7:42
G12	7:30	7:36	7:45	7:52	7:58	8:05	8:12	8:17
G12	8:00	8:06	8:15	8:22	8:28	8:35	8:42	8:47
G12	8:30	8:36	8:45	8:52	8:58	9:05	9:12	9:17
G12	9:00	9:06	9:15	9:22	9:28	9:35	9:42	9:47
G12	9:30	9:36	9:45	9:52	9:58	10:05	10:12	10:17
G12	10:15	10:20	10:28	10:35	10:41	10:48	10:56	11:02
G12	11:15	11:20	11:28	11:35	11:41	11:48	11:56	12:02
PM Service — Servicio vespertino								
G12	12:15	12:20	12:28	12:35	12:41	12:48	12:56	1:02
G12	1:15	1:20	1:28	1:35	1:41	1:48	1:56	2:02
G12	2:15	2:20	2:28	2:35	2:41	2:48	2:56	3:02
G12	3:00	3:06	3:16	3:24	3:30	3:37	3:46	3:52
G12	3:30	3:36	3:46	3:54	4:00	4:07	4:16	4:22
G12	4:00	4:06	4:16	4:24	4:30	4:37	4:46	4:52
G12	4:30	4:36	4:46	4:54	5:00	5:07	5:16	5:22
G12	5:00	5:06	5:16	5:24	5:30	5:37	5:46	5:52
G12	5:30	5:36	5:46	5:54	6:00	6:07	6:16	6:22
G12	6:00	6:06	6:16	6:24	6:30	6:37	6:46	6:52
G12	6:30	6:36	6:46	6:54	7:00	7:07	7:16	7:22
G12	7:00	7:06	7:15	7:22	7:27	7:33	7:39	7:44
G12	7:45	7:51	8:00	8:07	8:12	8:18	8:24	8:29
G12	8:45	8:51	9:00	9:07	9:12	9:18	9:24	9:29
G12	9:45	9:50	9:58	10:03	10:08	10:13	10:19	10:25
G12/	11:00	11:05	11:13	-	-	-	-	-
G12/	11:35	11:40	11:48	-	-	-	-	-



▶ Westbound to Greenbelt station

Monday thru Friday — Lunes a viernes

Route Number	New Carrollton 	Annapolis Rd. & 85th Ave.	Doctors Community Hospital	Ora Glen Dr. & Mandan Rd.	Greenbelt Rd. opposite Greenway Shopping Center	Crescent Rd. & Gardenway (Roosevelt Center)	Ivy La. & Ridge Rd.	GREENBELT 
AM Service — Servicio matutino								
G12	5:15	5:19	5:26	5:33	5:39	5:46	5:51	5:55
G12	5:45	5:49	5:56	6:03	6:09	6:16	6:21	6:25
G12	6:15	6:20	6:29	6:36	6:44	6:51	6:57	7:02
G12	6:45	6:50	6:59	7:06	7:14	7:21	7:27	7:32
G12	7:15	7:20	7:29	7:36	7:44	7:51	7:57	8:02
G12	7:45	7:51	7:59	8:07	8:14	8:22	8:27	8:32
G12	8:15	8:21	8:29	8:37	8:44	8:52	8:57	9:02
G12	8:45	8:51	8:59	9:07	9:14	9:22	9:27	9:32
G12	9:45	9:51	9:59	10:07	10:14	10:22	10:27	10:32
G12	10:45	10:51	10:59	11:07	11:14	11:22	11:27	11:32
G12	11:45	11:51	11:59	12:07	12:14	12:22	12:27	12:32
PM Service — Servicio vespertino								
G12	12:45	12:51	12:59	1:07	1:14	1:22	1:27	1:32
G12	1:45	1:51	2:00	2:07	2:13	2:22	2:26	2:31
G12	2:15	2:21	2:30	2:37	2:43	2:52	2:56	3:01
G12	2:45	2:51	3:00	3:07	3:13	3:22	3:26	3:31
G12	3:15	3:21	3:30	3:37	3:43	3:52	3:56	4:01
G12	3:45	3:52	4:02	4:10	4:17	4:25	4:29	4:35
G12	4:15	4:22	4:32	4:40	4:47	4:55	4:59	5:05
G12	4:45	4:52	5:02	5:10	5:17	5:25	5:29	5:35
G12	5:15	5:22	5:32	5:40	5:47	5:55	5:59	6:05
G12	5:45	5:52	6:02	6:10	6:17	6:25	6:29	6:35
G12	6:15	6:22	6:32	6:40	6:47	6:55	6:59	7:05
G12	7:15	7:21	7:29	7:36	7:42	7:48	7:52	7:57
G12	8:15	8:21	8:29	8:36	8:42	8:48	8:52	8:57
G12	9:15	9:21	9:29	9:36	9:42	9:48	9:52	9:57

► Eastbound to New Carrollton station

Saturday — Sabados



Route Number	Greenbelt 	Ivy La. & Ridge Rd.	Crescent Rd. & Gardenway (Roosevelt Center)	Greenbelt Rd. at Greenway Shopping Center	Ora Glen Dr. & Mandan Rd.	Doctors Community Hospital	Annapolis Rd. & 85th Ave.	NEW CARROLLTON 
AM Service — Servicio matutino								
G12	7:00	7:05	7:11	7:17	7:22	7:28	7:35	7:40
G12	8:00	8:05	8:11	8:17	8:22	8:28	8:35	8:40
G12	9:00	9:05	9:11	9:17	9:22	9:28	9:35	9:40
G12	10:00	10:05	10:11	10:17	10:22	10:28	10:35	10:40
G12	11:00	11:05	11:11	11:17	11:22	11:28	11:35	11:40
PM Service — Servicio vespertino								
G12	12:00	12:05	12:14	12:21	12:26	12:33	12:41	12:46
G12	1:00	1:05	1:14	1:21	1:26	1:33	1:41	1:46
G12	2:00	2:05	2:14	2:21	2:26	2:33	2:41	2:46
G12	3:00	3:05	3:14	3:21	3:26	3:33	3:41	3:46
G12	4:00	4:05	4:14	4:21	4:26	4:33	4:41	4:46
G12	5:00	5:05	5:14	5:21	5:26	5:33	5:41	5:46
G12	6:00	6:05	6:14	6:21	6:26	6:33	6:41	6:46
G12	7:00	7:05	7:13	7:19	7:24	7:31	7:37	7:42
G12	8:00	8:05	8:13	8:19	8:24	8:31	8:37	8:42
G12	9:00	9:05	9:13	9:19	9:24	9:31	9:37	9:42
G12	10:00	10:05	10:13	10:19	10:24	10:31	10:37	10:42

On five Federal holidays, Juneteenth, Columbus Day, Veterans' Day, Martin Luther King, Jr. Day, and Presidents' Day, the Saturday schedule will be in effect.

Metrobus proveerá servicio con horario de sábado durante los cinco días festivos de Juneteenth, Columbus Day, Veterans Day, Martin Luther King Jr. Day, y Presidents' Day.

► Westbound to Greenbelt station

Saturday — Sabados



Route Number	New Carrollton 	Annapolis Rd. & 85th Ave.	Doctors Community Hospital	Ora Glen Dr. & Mandan Rd.	Greenbelt Rd. opposite Greenway Shopping Center	Crescent Rd. & Gardenway (Roosevelt Center)	Ivy La. & Ridge Rd.	GREENBELT 
AM Service — Servicio matutino								
G12	6:30	6:35	6:42	6:50	6:56	7:05	7:09	7:13
G12	7:30	7:35	7:42	7:50	7:56	8:05	8:09	8:13
G12	8:30	8:35	8:42	8:50	8:56	9:05	9:09	9:13
G12	9:30	9:35	9:42	9:50	9:56	10:05	10:09	10:13
G12	10:30	10:35	10:42	10:50	10:56	11:05	11:09	11:13
G12	11:30	11:36	11:44	11:52	11:58	12:07	12:11	12:16
PM Service — Servicio vespertino								
G12	12:30	12:36	12:44	12:52	12:58	1:07	1:11	1:16
G12	1:30	1:36	1:44	1:52	1:58	2:07	2:11	2:16
G12	2:30	2:36	2:44	2:52	2:58	3:07	3:11	3:16
G12	3:30	3:36	3:44	3:52	3:58	4:07	4:11	4:16
G12	4:30	4:36	4:44	4:52	4:58	5:07	5:11	5:16
G12	5:30	5:36	5:44	5:52	5:58	6:07	6:11	6:16
G12	6:30	6:36	6:44	6:52	6:58	7:07	7:11	7:16
G12	7:30	7:35	7:43	7:50	7:55	8:04	8:08	8:12
G12	8:30	8:35	8:43	8:50	8:55	9:04	9:08	9:12
G12	9:30	9:35	9:43	9:50	9:55	10:04	10:08	10:12

On five Federal holidays, Juneteenth, Columbus Day, Veterans' Day, Martin Luther King, Jr. Day, and Presidents' Day, the Saturday schedule will be in effect.

Metrobus proveerá servicio con horario de sábado durante los cinco días festivos de Juneteenth, Columbus Day, Veterans Day, Martin Luther King Jr. Day, y Presidents' Day.



► Eastbound to New Carrollton station

Sunday — Domingos

Route Number	Greenbelt 	Ivy La. & Ridge Rd.	Crescent Rd. & Gardenway (Roosevelt Center)	Greenbelt Rd. at Greenway Shopping Center	Ora Glen Dr. & Mandan Rd.	Doctors Community Hospital	Annapolis Rd. & 85th Ave.	NEW CARROLLTON 
AM Service — Servicio matutino								
G12	7:00	7:05	7:13	7:19	7:24	7:30	7:37	7:42
G12	8:00	8:05	8:13	8:19	8:24	8:30	8:37	8:42
G12	9:00	9:05	9:13	9:19	9:24	9:30	9:37	9:42
G12	10:00	10:05	10:13	10:19	10:24	10:30	10:37	10:42
G12	11:00	11:05	11:13	11:19	11:24	11:30	11:37	11:42
PM Service — Servicio vespertino								
G12	12:00	12:05	12:14	12:21	12:26	12:33	12:41	12:46
G12	1:00	1:05	1:14	1:21	1:26	1:33	1:41	1:46
G12	2:00	2:05	2:14	2:21	2:26	2:33	2:41	2:46
G12	3:00	3:05	3:14	3:21	3:26	3:33	3:41	3:46
G12	4:00	4:05	4:14	4:21	4:26	4:33	4:41	4:46
G12	5:00	5:05	5:14	5:21	5:26	5:33	5:41	5:46
G12	6:00	6:05	6:14	6:21	6:26	6:33	6:41	6:46
G12	7:00	7:05	7:13	7:19	7:24	7:31	7:37	7:42
G12	8:00	8:05	8:13	8:19	8:24	8:31	8:37	8:42
G12	9:00	9:05	9:13	9:19	9:24	9:31	9:37	9:42
G12	10:00	10:05	10:13	10:19	10:24	10:31	10:37	10:42

► Westbound to Greenbelt station

Sunday — Domingos

Route Number	New Carrollton 	Annapolis Rd. & 85th Ave.	Doctors Community Hospital	Ora Glen Dr. & Mandan Rd.	Greenbelt Rd. opposite Greenway Shopping Center	Crescent Rd. & Gardenway (Roosevelt Center)	Ivy La. & Ridge Rd	GREENBELT 
AM Service — Servicio matutino								
G12	6:30	6:35	6:42	6:50	6:56	7:05	7:09	7:13
G12	7:30	7:35	7:42	7:50	7:56	8:05	8:09	8:13
G12	8:30	8:35	8:42	8:50	8:56	9:05	9:09	9:13
G12	9:30	9:35	9:42	9:50	9:56	10:05	10:09	10:13
G12	10:30	10:35	10:42	10:50	10:56	11:05	11:09	11:13
G12	11:30	11:36	11:44	11:52	11:58	12:07	12:11	12:16
PM Service — Servicio vespertino								
G12	12:30	12:36	12:44	12:52	12:58	1:07	1:11	1:16
G12	1:30	1:36	1:44	1:52	1:58	2:07	2:11	2:16
G12	2:30	2:36	2:44	2:52	2:58	3:07	3:11	3:16
G12	3:30	3:36	3:44	3:52	3:58	4:07	4:11	4:16
G12	4:30	4:36	4:44	4:52	4:58	5:07	5:11	5:16
G12	5:30	5:36	5:44	5:52	5:58	6:07	6:11	6:16
G12	6:30	6:36	6:44	6:52	6:58	7:07	7:11	7:16
G12	7:30	7:35	7:43	7:50	7:55	8:04	8:08	8:12
G12	8:30	8:35	8:43	8:50	8:55	9:04	9:08	9:12
G12	9:30	9:35	9:43	9:50	9:55	10:04	10:08	10:12

How to use this timetable

- Use the map to find the stops closest to where you will get on and off the bus.
- Select the schedule (Weekday, Saturday, Sunday) for when you will travel. Along the top of the schedule, find the stop at or nearest the point where you will get on the bus. Follow that column down to the time you want to leave.
- Use the same method to find the times the bus is scheduled to arrive at the stop where you will get off the bus.
- If the bus stop is not listed, use the time shown for the bus stop before it as the time to wait at the stop.
- The end-of-the-line or last stop is listed in ALL CAPS on the schedule.

Cómo Usar este Horario

- Use este mapa para localizar las paradas más cercanas a donde se subirá y bajará del autobús.
- Seleccione el horario (Entre semana, sábado, domingo) de cuando viajará. A lo largo de la parte superior del horario, localice la parada o el punto más cercano a la parada en la que se subirá al autobús. Siga esa columna hacia abajo hasta la hora en la que desee salir.
- Utilice el mismo método para localizar las horas en que el autobús está programado para llegar a la parada en donde desea bajarse del autobús.
- Si la parada del autobús no está listada use la hora que se muestra en la parada anterior como la hora de espera en la parada.
- El final de la ruta o la última parada del autobús aparece en letras MAYÚSCULAS en el horario.

English-Español

Effective 6-25-23

G14

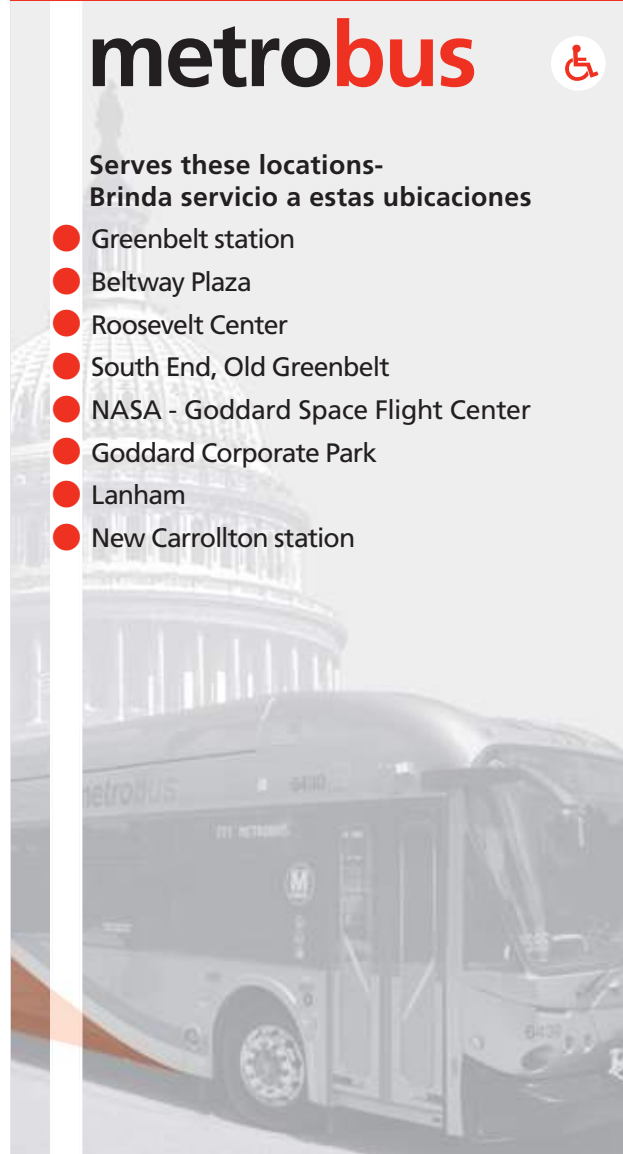
Greenbelt Rd.-Good Luck Rd. Line

metrobus



Serves these locations-
Brinda servicio a estas ubicaciones

- Greenbelt station
- Beltway Plaza
- Roosevelt Center
- South End, Old Greenbelt
- NASA - Goddard Space Flight Center
- Goddard Corporate Park
- Lanham
- New Carrollton station



www.wmata.com

Information Anytime 202-637-7000 TTY 202-962-2033



**Washington
Metropolitan Area
Transit Authority**

*A District of Columbia,
Maryland and Virginia
Transit Partnership*

G14

Greenbelt Rd.-Good Luck Rd. Line

Effective Sunday, June 25, 2023

A partir del domingo, 25 de junio de 2023

G14

Greenbelt Rd.-Goodluck Rd. Line

For route and schedule information

Call 202-637-7000

www.wmata.com



Legend

-  — Metrorail Station
-  — Terminal Stands

Guaranteed Ride Home

When you take Metrobus or Metrorail to work, you are eligible to participate in the free Commuter Connection Guaranteed Ride Home Program.

The program will get you home in the event of a personal emergency or unscheduled overtime.

To register and to receive program details, call Commuter Connection at 1-800-745-RIDE.

(2/97)

► Eastbound to New Carrollton station

Monday thru Friday — Lunes a viernes

Route Number	Greenbelt M	Beltway Plaza ■	Crescent Rd. & Gardenway (Roosevelt Center)	Greenbelt Rd. at Greenway Shopping Center	Greenbelt Rd. opposite NASA Main Gate	Goddard Corporate Park	Cipriano Rd. & Tuckerman St.	Annapolis Rd. & 85th Ave.	NEW CARROLLTON M
AM Service — Servicio matutino									
G14	-	-	5:15	5:20	5:24	5:30	5:38	5:44	5:48
G14	-	-	5:45	5:50	5:54	6:00	6:08	6:14	6:18
G14	6:15	●	6:27	6:31	6:35	6:41	6:49	6:55	6:59
G14	6:45	●	6:57	7:01	7:05	7:11	7:19	7:25	7:29
G14	7:15	●	7:27	7:31	7:35	7:41	7:49	7:55	7:59
G14	7:45	7:54	8:04	8:09	8:14	8:20	8:28	8:34	8:39
G14	8:15	8:24	8:34	8:39	8:44	8:50	8:58	9:04	9:09
G14	8:45	8:54	9:04	9:09	9:14	9:20	9:28	9:34	9:39
G14	9:15	9:24	9:34	9:39	9:44	9:50	9:58	10:04	10:09
G14	9:45	9:54	10:02	10:07	10:10	10:17	10:25	10:31	10:37
G14	10:45	10:54	11:02	11:07	11:10	11:17	11:25	11:31	11:37
G14	11:45	11:54	12:02	12:07	12:10	12:17	12:25	12:31	12:37
PM Service — Servicio vespertino									
G14	12:45	12:54	1:02	1:07	1:10	1:17	1:25	1:31	1:37
G14	1:45	1:54	2:02	2:07	2:10	2:17	2:25	2:31	2:37
G14	2:45	2:56	3:04	3:09	3:13	3:19	3:27	3:34	3:40
G14	3:00	3:11	3:19	3:24	3:28	3:34	3:42	3:49	3:55
G14	3:15	3:26	3:34	3:39	3:43	3:49	3:57	4:04	4:10
G14	3:45	3:56	4:04	4:09	4:13	4:19	4:27	4:34	4:40
G14	4:15	4:26	4:34	4:39	4:43	4:49	4:57	5:04	5:10
G14	4:45	4:56	5:04	5:09	5:13	5:19	5:27	5:34	5:40
G14	5:15	5:26	5:34	5:39	5:43	5:49	5:57	6:04	6:10
G14	5:45	5:56	6:04	6:09	6:13	6:19	6:27	6:34	6:40
G14	6:15	6:26	6:34	6:39	6:43	6:49	6:57	7:04	7:10
G14	6:45	6:55	7:02	7:07	7:11	7:17	7:25	7:30	7:35
G14	7:15	7:25	7:32	7:37	7:41	7:47	7:55	8:00	8:05
G14	8:15	8:25	8:32	8:37	8:41	8:47	8:55	9:00	9:05
G14	9:15	9:25	9:32	9:37	9:41	9:47	9:55	10:00	10:05
G14	10:15	10:25	10:29	10:34	10:38	10:44	10:52	10:57	11:02

- — Buses are timed at stop under parking garage.
Estas horas son las de la parada del autobús que está debajo del garaje del estacionamiento.
- — These trips do not loop through shopping center. Use stops on Greenbelt Road.
Estos viajes no darán vuelta a través del centro comercial. Use las paradas de la calle Greenbelt Road.

► Westbound to Greenbelt station

Monday thru Friday — Lunes a viernes

Route Number	New Carrollton M	Annapolis Rd. & 85th Ave.	Cipriano Rd. & Tuckerman St.	Goddard Corporate Park	Greenbelt Rd. at NASA Main Gate	Greenbelt Rd. opposite Greenway Shopping Center	Crescent Rd. & Gardenway (Roosevelt Center)	Beltway Plaza	GREEN-BELT M
AM Service — Servicio matutino									
G14	5:00	5:04	5:09	5:17	5:24	5:28	5:33	●	5:44
G14	5:30	5:34	5:39	5:47	5:54	5:58	6:03	●	6:14
G14	6:00	6:04	6:09	6:17	6:24	6:28	6:33	●	6:44
G14	6:30	6:35	6:40	6:49	6:57	7:01	7:08	●	7:20
G14	7:00	7:05	7:10	7:19	7:27	7:31	7:38	7:47	7:53
G14	7:30	7:35	7:40	7:49	7:57	8:01	8:08	8:17	8:23
G14	8:00	8:06	8:11	8:20	8:27	8:33	8:39	8:48	8:54
G14	8:30	8:36	8:41	8:50	8:57	9:03	9:09	9:18	9:24
G14	9:15	9:21	9:26	9:35	9:42	9:48	9:54	10:03	10:09
G14	10:15	10:21	10:26	10:35	10:42	10:48	10:54	11:03	11:09
G14	11:15	11:21	11:26	11:35	11:42	11:48	11:54	12:03	12:09
PM Service — Servicio vespertino									
G14	12:15	12:21	12:26	12:35	12:42	12:48	12:54	1:03	1:09
G14	1:15	1:21	1:26	1:35	1:42	1:48	1:54	2:03	2:09
G14	2:00	2:06	2:11	2:20	2:27	2:33	2:39	2:48	2:54
G14	2:30	2:36	2:41	2:50	2:58	3:02	3:11	3:20	3:25
G14	3:00	3:06	3:11	3:20	3:28	3:32	3:41	3:50	3:55
G14	3:30	3:37	3:43	3:53	4:01	4:06	4:13	4:22	4:29
G14	4:00	4:07	4:13	4:23	4:31	4:36	4:43	4:52	4:59
G14	4:30	4:37	4:43	4:53	5:01	5:06	5:13	5:22	5:29
G14	5:00	5:07	5:13	5:23	5:31	5:36	5:43	5:52	5:59
G14	5:30	5:37	5:43	5:53	6:01	6:06	6:13	6:22	6:29
G14	6:00	6:07	6:13	6:23	6:31	6:36	6:43	6:52	6:59
G14	6:45	6:51	6:56	7:06	7:14	7:18	7:24	7:31	7:38
G14	7:45	7:51	7:56	8:06	8:14	8:18	8:24	8:31	8:38
G14	8:45	8:51	8:56	9:06	9:14	9:18	9:24	9:31	9:38
▲ G14	-	-	-	-	-	-	11:20	-	11:30

- — Buses are timed at stop under parking garage.
Estas horas son las de la parada del autobús que está debajo del garaje del estacionamiento.
- — These trips do not loop through shopping center. Use stops on Greenbelt Road.
Estos viajes no darán vuelta a través del centro comercial. Use las paradas de la calle Greenbelt Road.
- ▲ — This trip serves the stop at Crescent Rd. & Gardenway normally served by trips going to New Carrollton station.
It does not serve regular G14 stops east of Southway.
Este viaje provee servicio a la parada de Crescent Rd. & Gardenway, la cual normalmente ofrece viajes a usuarios que van a la estación de Metrorail de New Carrollton. Este viaje no proveerá servicio a las paradas regulares de la ruta G14 al este de Southway.

► Eastbound to New Carrollton station

Saturday — Sábados

Route Number	Greenbelt M	Beltway Plaza ■	Crescent Rd. & Gardenway (Roosevelt Center)	Greenbelt Rd. at Greenway Shopping Center	Greenbelt Rd. opposite NASA Main Gate	Goddard Corporate Park	Cipriano Rd. & Tuckerman St.	Annapolis Rd. & 85th Ave.	NEW CARROLLTON M
AM Service — Servicio matutino									
G14	6:30	6:39	6:48	6:52	6:55	7:01	7:09	7:14	7:19
G14	7:30	7:39	7:48	7:52	7:55	8:01	8:09	8:14	8:19
G14	8:30	8:39	8:48	8:52	8:55	9:01	9:09	9:14	9:19
G14	9:30	9:39	9:48	9:52	9:55	10:01	10:09	10:14	10:19
G14	10:30	10:39	10:48	10:52	10:55	11:01	11:09	11:14	11:19
PM Service — Servicio vespertino									
G14	11:30	11:40	11:50	11:54	11:58	12:05	12:12	12:18	12:23
G14	12:30	12:40	12:50	12:54	12:58	1:05	1:12	1:18	1:23
G14	1:30	1:40	1:50	1:54	1:58	2:05	2:12	2:18	2:23
G14	2:30	2:40	2:50	2:54	2:58	3:05	3:12	3:18	3:23
G14	3:30	3:40	3:50	3:54	3:58	4:05	4:12	4:18	4:23
G14	4:30	4:40	4:50	4:54	4:58	5:05	5:12	5:18	5:23
G14	5:30	5:40	5:50	5:54	5:58	6:05	6:12	6:18	6:23
G14	6:30	6:40	6:50	6:54	6:58	7:05	7:12	7:18	7:23
G14	7:30	7:39	7:45	7:49	7:53	7:59	8:07	8:13	8:18
G14	8:30	8:39	8:45	8:49	8:53	8:59	9:07	9:13	9:18
G14	9:30	9:39	9:45	9:49	9:53	9:59	10:07	10:13	10:18

■ — Buses are timed at stop under parking garage.

Estas horas son las de la parada del autobús que está debajo del garaje del estacionamiento.

On five Federal holidays, Juneteenth, Columbus Day, Veterans' Day, Martin Luther King, Jr. Day, and Presidents' Day, the Saturday schedule will be in effect.

Metrobus proveerá servicio con horario de sábado durante los cinco días festivos de Juneteenth, Columbus Day, Veterans Day, Martin Luther King Jr. Day, y Presidents' Day.

► Westbound to Greenbelt station

Saturday — Sábados

Route Number	New Carrollton M	Annapolis Rd. & 85th Ave.	Cipriano Rd. & Tuckerman St.	Goddard Corporate Park	Greenbelt Rd. at NASA Main Gate	Greenbelt Rd. opposite Greenway Shopping Center	Crescent Rd. & Gardenway (Roosevelt Center)	Beltway Plaza ■	GREENBELT M
AM Service — Servicio matutino									
G14	7:00	7:05	7:10	7:18	7:24	7:29	7:35	7:43	7:48
G14	8:00	8:05	8:10	8:18	8:24	8:29	8:35	8:43	8:48
G14	9:00	9:05	9:10	9:18	9:24	9:29	9:35	9:43	9:48
G14	10:00	10:05	10:10	10:18	10:24	10:29	10:35	10:43	10:48
G14	11:00	11:06	11:11	11:19	11:26	11:30	11:36	11:45	11:51
PM Service — Servicio vespertino									
G14	12:00	12:06	12:11	12:19	12:26	12:30	12:36	12:45	12:51
G14	1:00	1:06	1:11	1:19	1:26	1:30	1:36	1:45	1:51
G14	2:00	2:06	2:11	2:19	2:26	2:30	2:36	2:45	2:51
G14	3:00	3:06	3:11	3:19	3:26	3:30	3:36	3:45	3:51
G14	4:00	4:06	4:11	4:19	4:26	4:30	4:36	4:45	4:51
G14	5:00	5:06	5:11	5:19	5:26	5:30	5:36	5:45	5:51
G14	6:00	6:06	6:11	6:19	6:26	6:30	6:36	6:45	6:51
G14	7:00	7:05	7:09	7:17	7:23	7:27	7:33	7:41	7:47
G14	8:00	8:05	8:09	8:17	8:23	8:27	8:33	8:41	8:47
G14	9:00	9:05	9:09	9:17	9:23	9:27	9:33	9:41	9:47
G14	10:00	10:05	10:09	10:17	10:23	10:27	10:33	10:41	10:47

■ — Buses are timed at stop under parking garage.

Estas horas son las de la parada del autobús que está debajo del garaje del estacionamiento.

On five Federal holidays, Juneteenth, Columbus Day, Veterans' Day, Martin Luther King, Jr. Day, and Presidents' Day, the Saturday schedule will be in effect.

Metrobus proveerá servicio con horario de sábado durante los cinco días festivos de Juneteenth, Columbus Day, Veterans Day, Martin Luther King Jr. Day, y Presidents' Day.

▶ Eastbound to New Carrollton station

Sunday — Domingos

Route Number	Greenbelt Plaza	Beltway Plaza ■	Crescent Rd. & Gardenway (Roosevelt Center)	Greenbelt Rd. at Greenway Shopping Center	Greenbelt Rd. opposite NASA Main Gate	Goddard Corporate Park	Cipriano Rd. & Tuckerman St.	Annapolis Rd. & 85th Ave.	NEW CARROLLTON
AM Service — Servicio matutino									
G14	6:30	6:39	6:48	6:52	6:55	7:01	7:09	7:14	7:19
G14	7:30	7:39	7:48	7:52	7:55	8:01	8:09	8:14	8:19
G14	8:30	8:39	8:48	8:52	8:55	9:01	9:09	9:14	9:19
G14	9:30	9:39	9:48	9:52	9:55	10:01	10:09	10:14	10:19
G14	10:30	10:39	10:48	10:52	10:55	11:01	11:09	11:14	11:19
G14	11:30	11:40	11:50	11:54	11:58	12:05	12:12	12:18	12:23
PM Service — Servicio vespertino									
G14	12:30	12:40	12:50	12:54	12:58	1:05	1:12	1:18	1:23
G14	1:30	1:40	1:50	1:54	1:58	2:05	2:12	2:18	2:23
G14	2:30	2:40	2:50	2:54	2:58	3:05	3:12	3:18	3:23
G14	3:30	3:40	3:50	3:54	3:58	4:05	4:12	4:18	4:23
G14	4:30	4:40	4:50	4:54	4:58	5:05	5:12	5:18	5:23
G14	5:30	5:40	5:50	5:54	5:58	6:05	6:12	6:18	6:23
G14	6:30	6:40	6:50	6:54	6:58	7:05	7:12	7:18	7:23
G14	7:30	7:39	7:45	7:49	7:53	7:59	8:07	8:13	8:18
G14	8:30	8:39	8:45	8:49	8:53	8:59	9:07	9:13	9:18
G14	9:30	9:39	9:45	9:49	9:53	9:59	10:07	10:13	10:18

■ — Buses are timed at stop under parking garage.
 Estas horas son las de la parada del autobús que está debajo del garaje del estacionamiento.

▶ Westbound to Greenbelt station

Sunday — Domingos

Route Number	New Carrollton	Annapolis Rd. & 85th Ave.	Cipriano Rd. & Tuckerman St.	Goddard Corporate Park	Greenbelt Rd. at NASA Main Gate	Greenbelt Rd. opposite Greenway Shopping Center	Crescent Rd. & Gardenway (Roosevelt Center)	Beltway Plaza ■	GREENBELT
AM Service — Servicio matutino									
G14	7:00	7:05	7:10	7:18	7:24	7:29	7:35	7:43	7:48
G14	8:00	8:05	8:10	8:18	8:24	8:29	8:35	8:43	8:48
G14	9:00	9:05	9:10	9:18	9:24	9:29	9:35	9:43	9:48
G14	10:00	10:05	10:10	10:18	10:24	10:29	10:35	10:43	10:48
G14	11:00	11:05	11:10	11:18	11:24	11:29	11:35	11:43	11:48
PM Service — Servicio vespertino									
G14	12:00	12:06	12:11	12:19	12:26	12:30	12:36	12:45	12:51
G14	1:00	1:06	1:11	1:19	1:26	1:30	1:36	1:45	1:51
G14	2:00	2:06	2:11	2:19	2:26	2:30	2:36	2:45	2:51
G14	3:00	3:06	3:11	3:19	3:26	3:30	3:36	3:45	3:51
G14	4:00	4:06	4:11	4:19	4:26	4:30	4:36	4:45	4:51
G14	5:00	5:06	5:11	5:19	5:26	5:30	5:36	5:45	5:51
G14	6:00	6:06	6:11	6:19	6:26	6:30	6:36	6:45	6:51
G14	7:00	7:06	7:11	7:19	7:26	7:30	7:36	7:45	7:51
G14	8:00	8:05	8:09	8:17	8:23	8:27	8:33	8:41	8:47
G14	9:00	9:05	9:09	9:17	9:23	9:27	9:33	9:41	9:47
G14	10:00	10:05	10:09	10:17	10:23	10:27	10:33	10:41	10:47

■ — Buses are timed at stop under parking garage.
 Estas horas son las de la parada del autobús que está debajo del garaje del estacionamiento.

DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION
OFFICE OF TRANSPORTATION

ROUTE 11

GREENBELT METRORAIL STATION/IVY LANE/FEDERAL COURTHOUSE/GREENWAY CENTER/MANDAN ROAD

TIMETABLE

GREENBELT METRORAIL STATION	GREENBELT FEDERAL COURT HOUSE	CRESCENT ROAD@ GARDEN WAY	HANOVER PARKWAY@ GREENWAY CENTER	MANDAN ROAD@ HANOVER PARKWAY	CRESCENT ROAD@ GARDEN WAY	GREENBELT FEDERAL COURT HOUSE	GREENBELT METRORAIL STATION
6:15 AM	6:18 AM	6:25 AM	6:31 AM	6:38 AM	6:44 AM	6:51 AM	6:53 AM
6:45 AM	6:48 AM	6:55 AM	7:01 AM	7:08 AM	7:14 AM	7:21 AM	7:23 AM
7:15 AM	7:18 AM	7:25 AM	7:31 AM	7:38 AM	7:44 AM	7:51 AM	7:53 AM
7:45 AM	7:48 AM	7:55 AM	8:01 AM	8:08 AM	8:14 AM	8:21 AM	8:23 AM
8:15 AM	8:18 AM	8:25 AM	8:31 AM	8:38 AM	8:44 AM	8:51 AM	8:53 AM
8:45 AM	8:48 AM	8:55 AM	9:01 AM	9:08 AM	9:14 AM	9:21 AM	9:23 AM
9:15 AM	9:18 AM	9:25 AM	9:31 AM	9:38 AM	9:44 AM	9:51 AM	9:53 AM
9:45 AM	9:48 AM	9:55 AM	10:01 AM	10:08 AM	10:14 AM	10:21 AM	10:23 AM
10:15 AM	10:18 AM	10:25 AM	10:31 AM	10:38 AM	10:44 AM	10:51 AM	10:53 AM
10:45 AM	10:48 AM	10:55 AM	11:01 AM	11:08 AM	11:14 AM	11:21 AM	11:23 AM
11:15 AM	11:18 AM	11:25 AM	11:31 AM	11:38 AM	11:44 AM	11:51 AM	11:53 AM
11:45 AM	11:48 AM	11:55 AM	12:01 PM	12:08 PM	12:14 PM	12:21 PM	12:23 PM
12:15 PM	12:18 PM	12:25 PM	12:31 PM	12:38 PM	12:44 PM	12:51 PM	12:53 PM
12:45 PM	12:48 PM	12:55 PM	1:01 PM	1:08 PM	1:14 PM	1:21 PM	1:23 PM
1:15 PM	1:18 PM	1:25 PM	1:31 PM	1:38 PM	1:44 PM	1:51 PM	1:53 PM
1:45 PM	1:48 PM	1:55 PM	2:01 PM	2:08 PM	2:14 PM	2:21 PM	2:23 PM
2:15 PM	2:18 PM	2:25 PM	2:31 PM	2:38 PM	2:44 PM	2:51 PM	2:53 PM
2:45 PM	2:48 PM	2:55 PM	3:01 PM	3:08 PM	3:14 PM	3:21 PM	3:23 PM
3:15 PM	3:18 PM	3:25 PM	3:31 PM	3:38 PM	3:44 PM	3:51 PM	3:53 PM
3:45 PM	3:48 PM	3:55 PM	4:01 PM	4:08 PM	4:14 PM	4:21 PM	4:23 PM
4:15 PM	4:18 PM	4:25 PM	4:31 PM	4:38 PM	4:44 PM	4:51 PM	4:53 PM
4:45 PM	4:48 PM	4:55 PM	5:01 PM	5:08 PM	5:14 PM	5:21 PM	5:23 PM
5:15 PM	5:18 PM	5:25 PM	5:31 PM	5:38 PM	5:44 PM	5:51 PM	5:53 PM
5:45 PM	5:48 PM	5:55 PM	6:01 PM	6:08 PM	6:14 PM	6:21 PM	6:23 PM
6:15 PM	6:18 PM	6:25 PM	6:31 PM	6:38 PM	6:44 PM	6:51 PM	6:53 PM



Angela D. Alsobrooks
County Executive



Michael D. Johnson, P.E.
Director

Press Ctrl+F keys to find a street name

Greenbelt Elementary

MORNING ROUTES:

BUS 157

8:58 AM 7710 HANOVER PKWY @ THE PLAYGROUND
9:06 AM MANDAN RD AND 2ND ENTRANCE GREENWOOD VILLAGE

BUS 273

8:31 AM MANDAN RD & GREENBRIAR CONDOS

BUS 459

9:05 AM LAKESIDE DR & WESTWAY

BUS 460

9:04 AM LAKECREST DR & LAKESIDE DR

BUS 469

8:59 AM S CHANNEL DR & STREAM BANK LN

BUS 470

8:53 AM HANOVER PKWY & MANDAN RD

BUS 477

8:43 AM 7600 HANOVER PKWY - GREENBELT CHILDREN'S CENTER DAYCARE
9:04 AM 6420 CHERRYWOOD LN @ ENT TO MOTIVA APT

BUS 626

8:39 AM MANDAN RD & MANDAN TER

Press Ctrl+F keys to find a street name

Greenbelt Elementary

AFTERNOON ROUTES:

BUS 157

3:58 PM 7600 HANOVER PKWY - GREENBELT CHILDREN'S CENTER DAYCARE

BUS 169

3:52 PM S CHANNEL DR & STREAM BANK LN

BUS 177

3:48 PM MANDAN RD & MANDAN TER

BUS 180

3:49 PM MANDAN RD & GREENBRIAR CONDOS

BUS 276

3:47 PM 6420 CHERRYWOOD LN @ ENT TO MOTIVA APT

3:55 PM LAKESIDE DR & WESTWAY

BUS 277

3:46 PM LAKECREST DR & LAKESIDE DR

BUS 465

3:47 PM HANOVER PKWY & MANDAN RD

BUS 488

3:49 PM 7710 HANOVER PKWY @ THE PLAYGROUND

3:57 PM MANDAN RD AND 2ND ENTRANCE GREENWOOD VILLAGE

APPENDIX C – CITY OF GREENBELT COLLISION DATA

Incident Number:	Incident Type:	Date:	Day of Week:	Address:
Ridge Road - Lastner Lane to Research Road				
2019-00023255	Accident Personal Injury	11/24/19 9:54 AM	Sun	71 RIDGE RD
GRP21010900000045	HIT AND RUN	1/9/21 5:00 PM	Sat	71 Ridge Rd Apt M / GREENBELT, MD 20770
GRP21031100000044	VEHICLE ACCIDENT COMBINED	3/11/21 4:22 PM	Thu	101 Ridge Rd / GREENBELT, MD 20770
Ridge Road - Research Road to Northway Road				
GRP20072300000023	HIT AND RUN	7/23/20 1:35 PM	Thu	65 Ridge Rd / GREENBELT, MD 20770
2019-00002739	HIT AND RUN	2/10/19 8:43 AM	Sun	65 RIDGE RD
2019-00022057	HIT AND RUN	11/6/19 9:19 PM	Wed	58 RIDGE RD
GRP20082300000065	HIT AND RUN	8/23/20 7:16 PM	Sun	53 Ridge Rd Apt Q / GREENBELT, MD 20770
GRP21061100000017	VEHICLE ACCIDENT	6/11/21 10:42 AM	Fri	66 Ridge Rd / GREENBELT, MD 20770
GRP21071200000030	VEHICLE ACCIDENT COMBINED	7/12/21 10:14 AM	Mon	55 Ridge Ct / GREENBELT, MD 20770
GRP22070500000020	ACCIDENT	7/5/22 6:11 AM	Tue	62 Ridge Rd / GREENBELT, MD 20770
GRP23033100000053	ACCIDENT	3/31/23 3:33 PM	Fri	66 Ridge Rd / GREENBELT, MD 20770
GRP23033100000055	HIT AND RUN	3/31/23 3:56 PM	Fri	66 Ridge Rd / GREENBELT, MD 20770
GRP23060800000054	ACCIDENT	6/8/23 2:35 PM	Thu	51 Ridge Ct / GREENBELT, MD 20770
Ridge Road - Northway Road to Hamilton Place				
GRP20071000000044	HIT AND RUN	7/10/20 12:47 PM	Fri	43 Ridge Rd Apt B / GREENBELT, MD 20770
GRP21111700000019	VEHICLE ACCIDENT	11/17/21 8:43 AM	Wed	42 Ridge Rd / GREENBELT, MD 20770
GRP22021400000012	ACCIDENT	2/14/22 8:52 AM	Mon	47 Ridge Rd / GREENBELT, MD 20770
GRP22081100000055	ACCIDENT	8/11/22 1:45 PM	Thu	36 Ridge Rd / GREENBELT, MD 20770
GRP22081600000026	VEHICLE ACCIDENT COMBINED	8/16/22 9:18 AM	Tue	38 Ridge Rd / GREENBELT, MD 20770
GRP22082400000068	ACCIDENT	8/24/22 5:47 PM	Wed	44 Ridge Rd / GREENBELT, MD 20770
GRP22092800000059	ACCIDENT	9/28/22 3:57 PM	Wed	43 Ridge Rd / GREENBELT, MD 20770
GRP23021000000051	VEHICLE ACCIDENT	2/10/23 12:54 PM	Fri	38 Ridge Rd / GREENBELT, MD 20770
Ridge Road - Hamilton Place to Southway				
2019-00018357	Accident Personal Injury	9/14/19 10:09 PM	Sat	32 RIDGE RD
2019-00016604	HIT AND RUN	8/24/19 6:35 PM	Sat	20 RIDGE CT
2019-00018491	Accident Property Damage	9/16/19 8:56 PM	Mon	26 RIDGE RD
2019-00023762	Accident Property Damage	12/1/19 10:30 AM	Sun	22 RIDGE RD
2020-00003672	Accident Property Damage	3/3/20 2:16 PM	Tue	33 RIDGE RD
GRP20121400000020	ACCIDENT	12/14/20 1:36 PM	Mon	28 Ridge Rd / GREENBELT, MD 20770
GRP21092100000045	HIT AND RUN	9/21/21 5:39 PM	Tue	33 Ridge Rd / GREENBELT, MD 20770
GRP21110800000020	VEHICLE ACCIDENT	11/8/21 9:24 AM	Mon	28 Ridge Rd / GREENBELT, MD 20770
GRP22031300000039	HIT AND RUN	3/13/22 4:25 PM	Sun	33 Ridge Rd Apt Unit V / GREENBELT, MD 20770
GRP22081000000048	ACCIDENT	8/10/22 1:43 PM	Wed	19 Ridge Rd / GREENBELT, MD 20770
GRP23010300000065	HIT AND RUN	1/3/23 10:35 AM	Tue	28 Ridge Rd Apt A / GREENBELT, MD 20770
GRP23011200000037	PEDESTRIAN STRUCK	1/12/23 11:59 AM	Thu	19 Ridge Rd / GREENBELT, MD 20770
GRP23033000000028	ACCIDENT	3/30/23 9:08 AM	Thu	Ridge Rd / Eastway / GREENBELT, MD 20770
GRP23081100000073	HIT AND RUN	8/11/23 6:17 PM	Fri	22 Ridge Rd / GREENBELT, MD 20770
GRP23111200000044	ACCIDENT	11/12/23 12:36 PM	Sun	25 Ridge Rd / GREENBELT, MD 20770
Ridge Road / Southway intersection				
GRP20122700000059	HIT AND RUN	12/27/20 6:50 PM	Sun	Westway / Ridge Rd / GREENBELT, MD 20770
GRP21082900000036	VEHICLE ACCIDENT COMBINED	8/29/21 4:42 PM	Sun	Westway / Ridge Rd / GREENBELT, MD 20770
GRP22041300000071	ACCIDENT	4/13/22 2:21 PM	Wed	Ridge Rd / Southway / GREENBELT, MD 20770
GRP22053000000030	VEHICLE ACCIDENT	5/30/22 12:51 PM	Mon	Ridge Rd / Southway / GREENBELT, MD 20770
GRP22082100000036	VEHICLE ACCIDENT	8/21/22 1:12 PM	Sun	Ridge Rd / Westway / GREENBELT, MD 20770
GRP22122000000054	VEHICLE ACCIDENT	12/20/22 12:09 PM	Tue	Southway / Ridge Rd / GREENBELT, MD 20770
GRP23050600000002	ACCIDENT	5/6/23 12:02 AM	Sat	Ridge Rd / Southway / GREENBELT, MD 20770
Ridge Road - Southway to Westway				
2020-00011615	Accident Property Damage	5/28/20 10:59 AM	Thu	11 RIDGE RD
GRP22012800000046	HIT AND RUN	1/28/22 3:49 PM	Fri	15 Ridge Rd Apt D / GREENBELT, MD 20770
GRP22041800000022	HIT AND RUN	4/18/22 8:17 AM	Mon	14 Ridge Rd Apt N / GREENBELT, MD 20770
GRP22100300000021	ACCIDENT	10/3/22 7:15 AM	Mon	10 Ridge Rd / GREENBELT, MD 20770
GRP23041700000060	ACCIDENT	4/17/23 1:23 PM	Mon	14 Ridge Rd / GREENBELT, MD 20770

Incident Number:	Incident Type:	Date:	Day of Week:	Address:
Lakeside Drive / Crescent Road intersection				
2019-00003539	HIT AND RUN	2/21/19 12:25 PM	Thu	NULL LAKESIDE DR
GRP21082700000040	ACCIDENT	8/27/21 2:31 PM	Fri	1 Lakeside Dr / GREENBELT, MD 20770
GRP23103100000088	ACCIDENT	10/31/23 4:18 PM	Tue	2 Lakeside Dr / GREENBELT, MD 20770
Lakeside Drive - Olivewood Court to Westway				
2019-00009360	Accident Property Damage	5/14/19 7:39 AM	Tue	125 LAKESIDE DR
2019-00010240	Accident Property Damage	5/26/19 12:54 PM	Sun	127 LAKESIDE DR
2019-00013350	HIT AND RUN	7/9/19 9:07 PM	Tue	123 LAKESIDE DR
2020-00000957	Accident Personal Injury	1/16/20 3:48 PM	Thu	153 LAKESIDE DR
2020-00011822	Accident Property Damage	5/30/20 2:20 PM	Sat	117 LAKESIDE DR
GRP20081800000033	ACCIDENT	8/18/20 12:33 PM	Tue	121 Lakeside Dr / GREENBELT, MD 20770
GRP23073000000040	VEHICLE ACCIDENT COMBINED	7/30/23 11:25 AM	Sun	132 Lakeside Dr / GREENBELT, MD 20770
Lakeside Drive / Westway intersection				
GRP20100900000055	ACCIDENT	10/9/20 7:21 PM	Fri	Lakeside Dr / Westway / GREENBELT, MD 20770
GRP21092200000049	VEHICLE ACCIDENT	9/22/21 7:05 PM	Wed	Westway / Lakeside Dr / GREENBELT, MD 20770
GRP23020200000100	VEHICLE ACCIDENT	2/2/23 8:41 PM	Thu	Lakeside Dr / Westway / GREENBELT, MD 20770
GRP23042600000087	ACCIDENT	4/26/23 7:38 PM	Wed	Westway / Lakeside Dr / GREENBELT, MD 20770
Lakeside Drive - Westway to Lakecrest Drive				
2019-00020544	HIT AND RUN	10/15/19 7:53 AM	Tue	225 LAKESIDE DR
GRP20103000000076	HIT AND RUN	10/30/20 5:39 PM	Fri	201 Lakeside Dr Apt 203 / GREENBELT, MD 20770
GRP20103000000084	HIT AND RUN	10/30/20 6:39 PM	Fri	201 Lakeside Dr / GREENBELT, MD 20770
GRP21010400000007	HIT AND RUN	1/4/21 7:40 AM	Mon	217 Lakeside Dr Apt 101 / GREENBELT, MD 20770
GRP21020500000025	HIT AND RUN	2/5/21 11:10 AM	Fri	Lakecrest Dr / Lakeside Dr / GREENBELT, MD 20770
GRP21021800000031	HIT AND RUN	2/18/21 1:05 PM	Thu	225 Lakeside Dr Apt 104 / GREENBELT, MD 20770
GRP21050600000051	HIT AND RUN	5/6/21 2:39 PM	Thu	205 Lakeside Dr Apt 203 / GREENBELT, MD 20770
GRP21072200000007	ACCIDENT	7/22/21 7:01 AM	Thu	225 Lakeside Dr / GREENBELT, MD 20770
GRP21082900000041	ACCIDENT	8/29/21 6:39 PM	Sun	201 Lakeside Dr / GREENBELT, MD 20770
GRP21111700000045	HIT AND RUN	11/17/21 3:44 PM	Wed	217 Lakeside Dr Apt T2 / GREENBELT, MD 20770
GRP22012200000061	VEHICLE ACCIDENT COMBINED	1/22/22 10:10 PM	Sat	205 Lakeside Dr / GREENBELT, MD 20770
GRP22031600000053	HIT AND RUN	3/16/22 4:08 PM	Wed	200 Lakeside Dr / GREENBELT, MD 20770
GRP22072300000020	HIT AND RUN	7/23/22 9:50 AM	Sat	200 Lakeside Dr / GREENBELT, MD 20770
GRP23070900000031	VEHICLE ACCIDENT COMBINED	7/9/23 3:14 AM	Sun	223 Lakeside Dr / GREENBELT, MD 20770
GRP23072400000029	HIT AND RUN	7/24/23 6:56 AM	Mon	217 Lakeside Dr / GREENBELT, MD 20770

Incident Number:	Incident Type:	Date:	Day of Week:	Address:
Lastner Lane - Ridge Road to Hedgewood Drive				
2019-00007708	HIT AND RUN	4/17/19 4:41 PM	Wed	208 LASTNER LN
2019-00011129	Accident Property Damage	6/8/19 11:02 AM	Sat	220 LASTNER LN
2019-00005751	HIT AND RUN	3/21/19 4:10 PM	Thu	235 LASTNER LN
GRP23030800000075	HIT AND RUN	3/8/23 8:36 PM	Wed	231 Lastner Ln / GREENBELT, MD 20770
GRP23090600000061	VEHICLE ACCIDENT	9/6/23 4:07 PM	Wed	237 Lastner Ln / GREENBELT, MD 20770
GRP23102500000053	HIT AND RUN	10/25/23 1:15 PM	Wed	Julian Ct / Lastner Ln / GREENBELT, MD 20770
Lastner Lane - Hedgewood Drive to Crescent Road				
2019-00025349	Accident Property Damage	12/24/19 11:02 AM	Tue	108 LASTNER LN
2020-00000668	HIT AND RUN	1/12/20 12:50 AM	Sun	118 LASTNER LN
GRP22010400000011	ACCIDENT	1/4/22 9:42 AM	Tue	Ivy Ln / Lastner Ln / GREENBELT, MD 20770
Lastner Lane / Crescent Road intersection				
GRP21071600000031	ACCIDENT	7/16/21 1:22 PM	Fri	Crescent Rd / Lastner Ln / GREENBELT, MD 20770
GRP21092100000048	VEHICLE ACCIDENT COMBINED	9/21/21 6:21 PM	Tue	Lastner Ln / Crescent Rd / GREENBELT, MD 20770
GRP21092100000049	VEHICLE ACCIDENT COMBINED	9/21/21 6:21 PM	Tue	Crescent Rd / Lastner Ln / GREENBELT, MD 20770
GRP22040900000030	MOTORCYCLE ACCIDENT COMBINED	4/9/22 12:11 PM	Sat	Crescent Rd / Lastner Ln / GREENBELT, MD 20770

Incident Number:	Incident Type:	Date:	Day of Week:	Address:
Greenhill Road - Crescent Road to Research Road				
2019-00006499	Accident Property Damage	3/31/19 6:29 PM	Sun	122 GREENHILL RD
GRP22051700000055	ACCIDENT	5/17/22 12:35 PM	Tue	120 Greenhill Rd / GREENBELT, MD 20770
Greenhill Road / Research Road / Hillside Road				
GRP20121800000042	HIT AND RUN	12/18/20 2:17 PM	Fri	Greenhill Rd / Hillside Rd / GREENBELT, MD 20770
GRP21031600000052	PEDESTRIAN STRUCK	3/16/21 7:44 PM	Tue	Greenhill Rd / Research Rd / GREENBELT, MD 20770
GRP22081100000078	VEHICLE ACCIDENT COMBINED	8/11/22 5:40 PM	Thu	Greenway Pl / Greenhill Rd / GREENBELT, MD 20770
GRP21090400000040	ACCIDENT	9/4/21 5:34 PM	Sat	Hillside Rd / Research Rd / GREENBELT, MD 20770
GRP21090400000041	VEHICLE ACCIDENT COMBINED	9/4/21 5:34 PM	Sat	Greenhill Rd / Hillside Rd / GREENBELT, MD 20770
Hillside Road - Research Road to Crescent Road				
2019-00004760	Accident Property Damage	3/8/19 5:01 PM	Fri	NULL HILLSIDE RD
GRP21071500000015	ACCIDENT	7/15/21 11:46 AM	Thu	Crescent Rd / Hillside Rd / GREENBELT, MD 20770
GRP21090400000039	ACCIDENT	9/4/21 5:33 PM	Sat	19 Hillside Rd Apt F / GREENBELT, MD 20770
GRP23052100000051	VEHICLE ACCIDENT	5/21/23 2:45 PM	Sun	Northway / Hillside Rd / GREENBELT, MD 20770

APPENDIX D – TRAFFIC CALMING RANKING SYSTEM

TRAFFIC CALMING RANKING SYSTEM

85th PERCENTILE TRAVEL SPEED

<u>POINT</u>	<u>85TH % SPEED</u>
0.5	<26
1.0	27-28
1.5	29-30
2.0	31-32
2.5	33-35

Average Daily Traffic (ADT) over 24-hours on average weekday

<u>POINT</u>	<u>ADT Volume (veh per day)</u>
0.5	100 to 250
1.0	251-500
1.5	501-1,000
2.0	>1,001

Collision History over a four-year period.

<u>POINT</u>	<u>Avg. Annual Crashes</u>
0.5	0.51 to 1.00
1.0	1.01 to 1.25
1.5	1.26 to 1.50
2.0	1.51 to 2.00
2.5	2.01 to 2.50
3.0	>2.51

**Table D-1
Traffic Calming Priority Ranking**

LOCATION	ADT Volume (veh per day) ^a	POINT	85 th Percentile Speed ^b	POINT	Avg. Annual Crashes ^c	POINT	SCORE	PRIORITY ^d
Ridge Road between Laurel Hill Road and Plateau Place	998	1.5	33	2.5	2.50	2.5	6.5	High
Ridge Road at Greenbelt Elementary School / 65 Ridge Road								High
Ridge Road / Plateau Place								High
Ridge Road / Northway								High
Ridge Road between Westway and Southway	986	1.5	30	1.5	1.25	1.0	4.0	Medium
Ridge Road between Eastway and Hamilton Place	1,427	2.0	31	2.0	1.00	0.5	4.5	High
Ridge Road / Gardenway								High
33 Ridge Road								High
Ridge Road / 21-24 Court Ridge Road								High
Ridge Road between Lastner Lane and Research Road	539	1.5	31	2.0	0.75	0.5	4.0	Medium
Ridge Road / Lastner Lane								Medium
Ridge Road at 7 Court Research Road								Medium
Lakeside Drive between Olivewood Court and Pinecrest Court	287	1.0	28	1.0	1.75	2.0	4.0	Medium
Lakeside Drive north of Crescent Road	245	0.5	25	0.5	0.00	0.0	1.0	Low
Lastner Lane south of Ridge Road	536	1.5	29	1.5	1.50	1.5	4.5	High
Lastner Lane from Julian Court to Hedgewood Drive								High
Lastner Lane between Rosewood Drive and Ivy Lane	726	1.5	30	1.5	0.75	0.5	3.5	Medium
Lastner Lane / Rosewood Drive								Medium
Lastner Lane / Ivy Lane								Medium
Greenhill Road west of Research Road	608	1.5	27	1.0	0.50	0.5	3.0	Medium
Greenhill Road / Hillside Road / Research Road								Medium
Greenhill Road north of Greenway Place	809	1.5	24	0.5	0.50	0.5	2.5	Medium
Greenhill Road / Orange Court / Greenway Place								Medium
Hillside Road north of Northway Road	710	1.5	27	1.0	0.75	0.5	3.0	Medium
Hillside Road / from Northway to Woodland Way								Medium
Hillside Road south of Woodland Way	860	1.5	32	2.0	0.75	0.5	4.0	Medium
Hillside Road / Crescent Road								Medium

^a ADT from Table 1.

^b 85th Percentile Speed from Table 2.

^c Average Annual Crash rate from Table 3.

^d low priority = 0≤X<2.0, medium priority = 2.0≤X<4.0, high priority = ≥4.0.

APPENDIX E – CONCEPTUAL COST ESTIMATES

Ridge Road / Lastner Lane Improvements

Figure 4A

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	255	LF	\$ 1.30	\$ 331.50
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	115	LF	\$ 1.60	\$ 184.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	30	LF	\$ 5.00	\$ 150.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	255	LF	\$ 65.00	\$ 16,575.00
	Sub Cost				\$ 17,240.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	160	LF	\$ 1.30	\$ 208.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 5,000.00	\$ 5,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	260	LF	\$ 15.00	\$ 3,900.00
210025	REMOVAL OF EXISTING PAVEMENT	100	CY	\$ 115.00	\$ 11,500.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	115	LF	\$ 1.60	\$ 184.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	30	LF	\$ 5.00	\$ 150.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	100	LF	\$ 8.40	\$ 840.00
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
613110	ASPHALT DRIVEWAYS AND ENTRANCES	4	TON	\$ 105.00	\$ 380.63
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	260	LF	\$ 75.00	\$ 19,500.00
655105	5 INCH CONCRETE SIDEWALK	290	SF	\$ 15.00	\$ 4,350.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	2	SF	\$ 50.00	\$ 83.33
705500	TURFGRASS ESTABLISHMENT	300	SY	\$ 2.10	\$ 630.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	7	EA	\$ 25.00	\$ 175.00
	Sub Cost				\$ 47,400.00

Ridge Road at 7 Court Research Road Improvements

Figure 4B

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
110100	CLEARING & GRUBBING	1	LS	\$ 7,000.00	\$ 7,000.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	135	LF	\$ 65.00	\$ 8,775.00
	Sub Cost				\$ 15,780.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
110100	CLEARING & GRUBBING	1	LS	\$ 7,000.00	\$ 7,000.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	120	LF	\$ 15.00	\$ 1,800.00
210025	REMOVAL OF EXISTING PAVEMENT	40	CY	\$ 115.00	\$ 4,600.00
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
613110	ASPHALT DRIVEWAYS AND ENTRANCES	5	TON	\$ 105.00	\$ 494.81
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	120	LF	\$ 75.00	\$ 9,000.00
655105	5 INCH CONCRETE SIDEWALK	580	SF	\$ 15.00	\$ 8,700.00
705500	TURFGRASS ESTABLISHMENT	120	SY	\$ 2.10	\$ 252.00
	Sub Cost				\$ 34,350.00

Ridge Road at Greenbelt Elementary School / 65 Ridge Road Improvements

Figure 4C

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
-	QUIK CURB w/ POST MOUNTED DELINEATOR	140	LF	\$ 65.00	\$ 9,100.00
	Sub Cost				\$ 9,100.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	140	LF	\$ 15.00	\$ 2,100.00
210025	REMOVAL OF EXISTING PAVEMENT	18	CY	\$ 115.00	\$ 2,108.33
504500	SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE, PG 64S-22, LEVEL 2	7	TON	\$ 170.00	\$ 1,232.50
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	140	LF	\$ 75.00	\$ 10,500.00
655105	5 INCH CONCRETE SIDEWALK	40	SF	\$ 15.00	\$ 600.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	1	SF	\$ 50.00	\$ 41.67
705500	TURFGRASS ESTABLISHMENT	55	SY	\$ 2.10	\$ 115.50
	Sub Cost				\$ 19,200.00

Ridge Road / Plateau Place Improvements

Figure 4D

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	50	LF	\$ 1.30	\$ 65.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	160	LF	\$ 8.40	\$ 1,344.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	176	LF	\$ 65.00	\$ 11,440.00
	Sub Cost				\$ 12,850.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	50	LF	\$ 1.30	\$ 65.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 3,000.00	\$ 3,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	135	LF	\$ 15.00	\$ 2,025.00
210025	REMOVAL OF EXISTING PAVEMENT	150	CY	\$ 115.00	\$ 17,250.00
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	160	LF	\$ 8.40	\$ 1,344.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	200	LF	\$ 75.00	\$ 15,000.00
655105	5 INCH CONCRETE SIDEWALK	730	SF	\$ 15.00	\$ 10,950.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	3	SF	\$ 50.00	\$ 166.67
705500	TURFGRASS ESTABLISHMENT	17	SY	\$ 2.10	\$ 35.00
801605	SHEET ALUMINUM SIGNS	4	EA	\$ 40.00	\$ 160.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	1	EA	\$ 25.00	\$ 25.00
	Sub Cost				\$ 50,520.00

Ridge Road / Northway Improvements

Figure 4E

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	85	LF	\$ 1.30	\$ 110.50
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	130	LF	\$ 1.60	\$ 208.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	35	LF	\$ 5.00	\$ 175.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	300	LF	\$ 8.40	\$ 2,520.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	310	LF	\$ 65.00	\$ 20,150.00
	Sub Cost				\$ 23,160.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	85	LF	\$ 1.30	\$ 110.50
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 5,000.00	\$ 5,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	290	LF	\$ 15.00	\$ 4,350.00
210025	REMOVAL OF EXISTING PAVEMENT	42	CY	\$ 115.00	\$ 4,791.67
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	130	LF	\$ 1.60	\$ 208.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	35	LF	\$ 5.00	\$ 175.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	300	LF	\$ 8.40	\$ 2,520.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	290	LF	\$ 75.00	\$ 21,750.00
655105	5 INCH CONCRETE SIDEWALK	950	SF	\$ 15.00	\$ 14,250.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	7	SF	\$ 50.00	\$ 333.33
705500	TURFGRASS ESTABLISHMENT	125	SY	\$ 2.10	\$ 262.50
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	3	EA	\$ 25.00	\$ 75.00
	Sub Cost				\$ 54,330.00

Ridge Road from 46 Court Ridge Road to Eastway / 41 Court Ridge Road Improvements

Figure 4F

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	20	LF	\$ 1.30	\$ 26.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	40	LF	\$ 1.60	\$ 64.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	10	LF	\$ 5.00	\$ 50.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	80	LF	\$ 8.40	\$ 672.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	290	LF	\$ 65.00	\$ 18,850.00
801605	SHEET ALUMINUM SIGNS	2	EA	\$ 40.00	\$ 80.00
	Sub Cost				\$ 19,740.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	50	LF	\$ 1.30	\$ 65.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 5,000.00	\$ 5,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	360	LF	\$ 15.00	\$ 5,400.00
504500	SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE, PG 64S-22, LEVEL 2	7	TON	\$ 170.00	\$ 1,232.50
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	40	LF	\$ 1.60	\$ 64.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	10	LF	\$ 5.00	\$ 50.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	200	LF	\$ 8.40	\$ 1,680.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	360	LF	\$ 75.00	\$ 27,000.00
655105	5 INCH CONCRETE SIDEWALK	560	SF	\$ 15.00	\$ 8,400.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	8	SF	\$ 50.00	\$ 416.67
705500	TURFGRASS ESTABLISHMENT	130	SY	\$ 2.10	\$ 273.00
801605	SHEET ALUMINUM SIGNS	2	EA	\$ 40.00	\$ 80.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	3	EA	\$ 25.00	\$ 75.00
	Sub Cost				\$ 55,220.00

33 Ridge Road Improvements

Figure 4G

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	110	LF	\$ 1.30	\$ 143.00
	Sub Cost				\$ 143.00

Ridge Road / Gardenway Improvements

Figure 4H

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	355	LF	\$ 1.30	\$ 461.50
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	40	LF	\$ 1.60	\$ 64.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	25	LF	\$ 5.00	\$ 125.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	80	LF	\$ 8.40	\$ 672.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	310	LF	\$ 65.00	\$ 20,150.00
	Sub Cost				\$ 21,470.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	355	LF	\$ 1.30	\$ 461.50
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 5,000.00	\$ 5,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	150	LF	\$ 15.00	\$ 2,250.00
210025	REMOVAL OF EXISTING PAVEMENT	10	CY	\$ 115.00	\$ 1,150.00
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	40	LF	\$ 1.60	\$ 64.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	25	LF	\$ 5.00	\$ 125.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	80	LF	\$ 8.40	\$ 672.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	150	LF	\$ 75.00	\$ 11,250.00
655105	5 INCH CONCRETE SIDEWALK	450	SF	\$ 15.00	\$ 6,750.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	3	SF	\$ 50.00	\$ 125.00
705500	TURFGRASS ESTABLISHMENT	30	SY	\$ 2.10	\$ 63.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	1	EA	\$ 25.00	\$ 25.00
	Sub Cost				\$ 28,440.00

21-24 Court Ridge Road Improvements

Figure 4I

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
-	QUIK CURB w/ POST MOUNTED DELINEATOR	170	LF	\$ 65.00	\$ 11,050.00
	Sub Cost				\$ 11,050.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	170	LF	\$ 15.00	\$ 2,550.00
210025	REMOVAL OF EXISTING PAVEMENT	42	CY	\$ 115.00	\$ 4,791.67
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	60	LF	\$ 8.40	\$ 504.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	170	LF	\$ 75.00	\$ 12,750.00
655105	5 INCH CONCRETE SIDEWALK	160	SF	\$ 15.00	\$ 2,400.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	3	SF	\$ 50.00	\$ 166.67
705500	TURFGRASS ESTABLISHMENT	125	SY	\$ 2.10	\$ 262.50
801605	SHEET ALUMINUM SIGNS	4	EA	\$ 40.00	\$ 160.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	4	EA	\$ 25.00	\$ 100.00
873002	REMOVE AND DISPOSE OF EXISTING SIGNAL EQUIPMENT	1	LS	\$ 8,300.00	\$ 8,300.00
-	RECTANGULAR RAPID FLASHING BEACONS	2	EA	\$ 5,000.00	\$ 10,000.00
	Sub Cost				\$ 44,480.00

Ridge Road / Southway Improvements

Figure 4J

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
-	QUIK CURB w/ POST MOUNTED DELINEATOR	495	LF	\$ 65.00	\$ 32,175.00
	Sub Cost				\$ 32,180.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	2,600	LF	\$ 1.30	\$ 3,380.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 10,000.00	\$ 10,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	495	LF	\$ 15.00	\$ 7,425.00
210025	REMOVAL OF EXISTING PAVEMENT	122	CY	\$ 115.00	\$ 13,991.67
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	260	LF	\$ 1.60	\$ 416.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	70	LF	\$ 5.00	\$ 350.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	600	LF	\$ 8.40	\$ 5,040.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	495	LF	\$ 75.00	\$ 37,125.00
655105	5 INCH CONCRETE SIDEWALK	810	SF	\$ 15.00	\$ 12,150.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	8	SF	\$ 50.00	\$ 416.67
705500	TURFGRASS ESTABLISHMENT	365	SY	\$ 2.10	\$ 766.50
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	4	EA	\$ 25.00	\$ 100.00
	Sub Cost				\$ 91,660.00

Ridge Road between Southway and Westway Improvements

Figure 4K

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	240	LF	\$ 1.30	\$ 312.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	300	LF	\$ 8.40	\$ 2,520.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	210	LF	\$ 65.00	\$ 13,650.00
	Sub Cost				\$ 16,480.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	260	LF	\$ 1.30	\$ 338.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	345	LF	\$ 15.00	\$ 5,175.00
210025	REMOVAL OF EXISTING PAVEMENT	78	CY	\$ 115.00	\$ 9,008.33
504500	SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE, PG 64S-22, LEVEL 2	15	TON	\$ 170.00	\$ 2,465.00
549010	THERMOPLASTIC PAVEMENT MARKING LEGENDS AND SYMBOLS	20	SF	\$ 25.00	\$ 500.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	40	LF	\$ 1.60	\$ 64.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	25	LF	\$ 5.00	\$ 125.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	330	LF	\$ 8.40	\$ 2,772.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	345	LF	\$ 75.00	\$ 25,875.00
655105	5 INCH CONCRETE SIDEWALK	640	SF	\$ 15.00	\$ 9,600.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	3	SF	\$ 50.00	\$ 166.67
705500	TURFGRASS ESTABLISHMENT	235	SY	\$ 2.10	\$ 493.50
801605	SHEET ALUMINUM SIGNS	2	EA	\$ 40.00	\$ 80.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	8	EA	\$ 25.00	\$ 200.00
	Sub Cost				\$ 58,860.00

Lakeside Drive from Lakeview Circle to Olivewood Court Improvements

Figure 5A

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	35	LF	\$ 5.00	\$ 175.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	100	LF	\$ 8.40	\$ 840.00
801605	SHEET ALUMINUM SIGNS	3	EA	\$ 40.00	\$ 120.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	300	LF	\$ 8.40	\$ 2,520.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	110	LF	\$ 65.00	\$ 7,150.00
	Sub Cost				\$ 10,810.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	160	LF	\$ 1.30	\$ 208.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	110	LF	\$ 15.00	\$ 1,650.00
210025	REMOVAL OF EXISTING PAVEMENT	25	CY	\$ 115.00	\$ 2,875.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	35	LF	\$ 5.00	\$ 175.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	100	LF	\$ 8.40	\$ 840.00
613110	ASPHALT DRIVEWAYS AND ENTRANCES	6	TON	\$ 105.00	\$ 647.06
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	625	LF	\$ 75.00	\$ 46,875.00
655105	5 INCH CONCRETE SIDEWALK	2,690	SF	\$ 15.00	\$ 40,350.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	2	SF	\$ 50.00	\$ 83.33
705500	TURFGRASS ESTABLISHMENT	75	SY	\$ 2.10	\$ 157.50
801605	SHEET ALUMINUM SIGNS	3	EA	\$ 40.00	\$ 120.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	4	EA	\$ 25.00	\$ 100.00
	Sub Cost				\$ 96,080.00

Lakeside Drive from Westway to Lakecrest Drive Improvements

Figure 5B

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	405	LF	\$ 1.30	\$ 526.50
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	1,400	LF	\$ 1.60	\$ 2,240.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	600	LF	\$ 65.00	\$ 39,000.00
	Sub Cost				\$ 41,770.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	405	LF	\$ 1.30	\$ 526.50
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210025	REMOVAL OF EXISTING PAVEMENT	88	CY	\$ 115.00	\$ 10,158.33
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	1,400	LF	\$ 1.60	\$ 2,240.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	820	LF	\$ 75.00	\$ 61,500.00
705500	TURFGRASS ESTABLISHMENT	265	SY	\$ 2.10	\$ 556.50
	Sub Cost				\$ 76,980.00

Lastner Lane from Julian Court to Hedgewood Drive Improvements

Figure 6A

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	280	LF	\$ 8.40	\$ 2,352.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	330	LF	\$ 65.00	\$ 21,450.00
	Sub Cost				\$ 23,800.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	60	LF	\$ 1.30	\$ 78.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	320	LF	\$ 15.00	\$ 4,800.00
210025	REMOVAL OF EXISTING PAVEMENT	60	CY	\$ 115.00	\$ 6,900.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	35	LF	\$ 5.00	\$ 175.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	280	LF	\$ 8.40	\$ 2,352.00
613110	ASPHALT DRIVEWAYS AND ENTRANCES	6	TON	\$ 105.00	\$ 647.06
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	330	LF	\$ 75.00	\$ 24,750.00
655105	5 INCH CONCRETE SIDEWALK	510	SF	\$ 15.00	\$ 7,650.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	7	SF	\$ 50.00	\$ 333.33
705500	TURFGRASS ESTABLISHMENT	180	SY	\$ 2.10	\$ 378.00
801605	SHEET ALUMINUM SIGNS	8	EA	\$ 40.00	\$ 320.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	2	EA	\$ 25.00	\$ 50.00
	Sub Cost				\$ 50,430.00

Lastner Lane / Rosewood Drive Improvements

Figure 6B

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	100	LF	\$ 8.40	\$ 840.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	230	LF	\$ 65.00	\$ 14,950.00
	Sub Cost				\$ 15,790.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	30	LF	\$ 1.30	\$ 39.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	160	LF	\$ 15.00	\$ 2,400.00
210025	REMOVAL OF EXISTING PAVEMENT	37	CY	\$ 115.00	\$ 4,216.67
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	50	LF	\$ 5.00	\$ 250.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	100	LF	\$ 8.40	\$ 840.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	230	LF	\$ 75.00	\$ 17,250.00
655105	5 INCH CONCRETE SIDEWALK	200	SF	\$ 15.00	\$ 3,000.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	5	SF	\$ 50.00	\$ 250.00
705500	TURFGRASS ESTABLISHMENT	110	SY	\$ 2.10	\$ 231.00
801605	SHEET ALUMINUM SIGNS	8	EA	\$ 40.00	\$ 320.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	1	EA	\$ 25.00	\$ 25.00
	Sub Cost				\$ 30,820.00

Lastner Lane / Ivy Lane Improvements

Figure 6C

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	120	LF	\$ 8.40	\$ 1,008.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	320	LF	\$ 65.00	\$ 20,800.00
	Sub Cost				\$ 21,810.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	40	LF	\$ 1.30	\$ 52.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	200	LF	\$ 15.00	\$ 3,000.00
210025	REMOVAL OF EXISTING PAVEMENT	45	CY	\$ 115.00	\$ 5,175.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	10	LF	\$ 1.60	\$ 16.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	50	LF	\$ 5.00	\$ 250.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	250	LF	\$ 8.40	\$ 2,100.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	320	LF	\$ 75.00	\$ 24,000.00
655105	5 INCH CONCRETE SIDEWALK	300	SF	\$ 15.00	\$ 4,500.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	3	SF	\$ 50.00	\$ 166.67
705500	TURFGRASS ESTABLISHMENT	135	SY	\$ 2.10	\$ 283.50
801605	SHEET ALUMINUM SIGNS	4	EA	\$ 40.00	\$ 160.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	3	EA	\$ 25.00	\$ 75.00
	Sub Cost				\$ 41,780.00

Lastner Lane / Crescent Road Improvements

Figure 6D

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
549401	5 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	130	LF	\$ 1.35	\$ 175.50
-	GREEN PAVEMENT MARKINGS	375	SF	\$ 20.00	\$ 7,500.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	250	LF	\$ 65.00	\$ 16,250.00
	Sub Cost				\$ 23,930.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	250	LF	\$ 1.30	\$ 325.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	110	LF	\$ 15.00	\$ 1,650.00
210025	REMOVAL OF EXISTING PAVEMENT	27	CY	\$ 115.00	\$ 3,066.67
549401	5 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	130	LF	\$ 1.35	\$ 175.50
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	100	LF	\$ 1.60	\$ 160.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	10	LF	\$ 5.00	\$ 50.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	140	LF	\$ 8.40	\$ 1,176.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	250	LF	\$ 75.00	\$ 18,750.00
655105	5 INCH CONCRETE SIDEWALK	590	SF	\$ 15.00	\$ 8,850.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	3	SF	\$ 50.00	\$ 166.67
705500	TURFGRASS ESTABLISHMENT	80	SY	\$ 2.10	\$ 168.00
801605	SHEET ALUMINUM SIGNS	4	EA	\$ 40.00	\$ 160.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	2	EA	\$ 25.00	\$ 50.00
-	GREEN PAVEMENT MARKINGS	375	SF	\$ 20.00	\$ 7,500.00
	Sub Cost				\$ 44,250.00

Greenhill Road / Crescent Road Improvements

Figure 7A

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
-	GREEN PAVEMENT MARKINGS	200	SF	\$ 20.00	\$ 4,000.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	205	LF	\$ 65.00	\$ 13,325.00
	Sub Cost				\$ 17,330.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	50	LF	\$ 15.00	\$ 750.00
210025	REMOVAL OF EXISTING PAVEMENT	18	CY	\$ 115.00	\$ 2,108.33
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	12	LF	\$ 5.00	\$ 60.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	80	LF	\$ 8.40	\$ 672.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	205	LF	\$ 75.00	\$ 15,375.00
655105	5 INCH CONCRETE SIDEWALK	480	SF	\$ 15.00	\$ 7,200.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	7	SF	\$ 50.00	\$ 333.33
705500	TURFGRASS ESTABLISHMENT	55	SY	\$ 2.10	\$ 115.50
801605	SHEET ALUMINUM SIGNS	4	EA	\$ 40.00	\$ 160.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	2	EA	\$ 25.00	\$ 50.00
-	GREEN PAVEMENT MARKINGS	200	SF	\$ 20.00	\$ 4,000.00
	Sub Cost				\$ 32,820.00

Greenhill Road / Orange Court / Greenway Place Improvements

Figure 7B

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	160	LF	\$ 8.40	\$ 1,344.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	180	LF	\$ 65.00	\$ 11,700.00
	Sub Cost				\$ 13,040.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 2,000.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	140	LF	\$ 15.00	\$ 2,100.00
210025	REMOVAL OF EXISTING PAVEMENT	18	CY	\$ 115.00	\$ 2,108.33
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	48	LF	\$ 5.00	\$ 240.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	320	LF	\$ 8.40	\$ 2,688.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	180	LF	\$ 75.00	\$ 13,500.00
655105	5 INCH CONCRETE SIDEWALK	450	SF	\$ 15.00	\$ 6,750.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	3	SF	\$ 50.00	\$ 166.67
705500	TURFGRASS ESTABLISHMENT	55	SY	\$ 2.10	\$ 115.50
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	5	EA	\$ 25.00	\$ 125.00
	Sub Cost				\$ 29,790.00

Greenhill Road / Research Road Improvements

Figure 7C

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
-	QUIK CURB w/ POST MOUNTED DELINEATOR	205	LF	\$ 65.00	\$ 13,325.00
	Sub Cost				\$ 13,330.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
120500	MAINTENANCE OF TRAFFIC	2,000	LS	\$ 1.00	\$ 2,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	50	LF	\$ 15.00	\$ 750.00
210025	REMOVAL OF EXISTING PAVEMENT	18	CY	\$ 115.00	\$ 2,108.33
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	12	LF	\$ 5.00	\$ 60.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	80	LF	\$ 8.40	\$ 672.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	205	LF	\$ 75.00	\$ 15,375.00
655105	5 INCH CONCRETE SIDEWALK	480	SF	\$ 15.00	\$ 7,200.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	7	SF	\$ 50.00	\$ 333.33
705500	TURFGRASS ESTABLISHMENT	55	SY	\$ 2.10	\$ 115.50
801605	SHEET ALUMINUM SIGNS	4	EA	\$ 40.00	\$ 160.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	2	EA	\$ 25.00	\$ 50.00
	Sub Cost				\$ 28,820.00

Hillside Road from Northway to Woodland Way Improvements

Figure 8A

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
801605	SHEET ALUMINUM SIGNS	10	EA	\$ 40.00	\$ 400.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	410	LF	\$ 65.00	\$ 26,650.00
	Sub Cost				\$ 27,050.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 5,000.00	\$ 5,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	240	LF	\$ 15.00	\$ 3,600.00
210025	REMOVAL OF EXISTING PAVEMENT	97	CY	\$ 115.00	\$ 11,116.67
504500	SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE, PG 64S-22, LEVEL 2	15	TON	\$ 170.00	\$ 2,588.25
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	40	LF	\$ 1.60	\$ 64.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	60	LF	\$ 5.00	\$ 300.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	250	LF	\$ 8.40	\$ 2,100.00
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	530	LF	\$ 75.00	\$ 39,750.00
655105	5 INCH CONCRETE SIDEWALK	1,180	SF	\$ 15.00	\$ 17,700.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	8	SF	\$ 50.00	\$ 416.67
705500	TURFGRASS ESTABLISHMENT	290	SY	\$ 2.10	\$ 609.00
801605	SHEET ALUMINUM SIGNS	12	EA	\$ 40.00	\$ 480.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	5	EA	\$ 25.00	\$ 125.00
	Sub Cost				\$ 83,850.00

Hillside Road / Crescent Road Improvements

Figure 8B

Short-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	150	LF	\$ 8.40	\$ 1,260.00
801605	SHEET ALUMINUM SIGNS	12	EA	\$ 40.00	\$ 480.00
-	QUIK CURB w/ POST MOUNTED DELINEATOR	285	LF	\$ 65.00	\$ 18,525.00
	Sub Cost				\$ 20,270.00

Long-Term Improvements

<u>ITEM NO.</u>	<u>ITEM</u>	<u>QUANTITY</u>	<u>UNIT</u>	<u>UNIT COST</u>	<u>TOTAL COST</u>
114280	REMOVAL OF EXISTING PAVEMENT LINE MARKINGS, ANY WIDTH	50	LF	\$ 1.30	\$ 65.00
120500	MAINTENANCE OF TRAFFIC	1	LS	\$ 5,000.00	\$ 5,000.00
210011	REMOVAL OF EXISTING COMBINATION CURB & GUTTER	120	LF	\$ 15.00	\$ 1,800.00
210025	REMOVAL OF EXISTING PAVEMENT	37	CY	\$ 115.00	\$ 4,216.67
504500	SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE, PG 64S-22, LEVEL 2	15	TON	\$ 170.00	\$ 2,465.00
549403	5 INCH YELLOW THERMOPLASTIC PAVEMENT MARKINGS	40	LF	\$ 1.60	\$ 64.00
549409	12 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	12	LF	\$ 5.00	\$ 60.00
549419	24 INCH WHITE THERMOPLASTIC PAVEMENT MARKINGS	270	LF	\$ 8.40	\$ 2,268.00
613110	ASPHALT DRIVEWAYS AND ENTRANCES	9	TON	\$ 105.00	\$ 970.59
634301	STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN 8 INCH MINIMUM DEPTH	285	LF	\$ 75.00	\$ 21,375.00
655105	5 INCH CONCRETE SIDEWALK	2,540	SF	\$ 15.00	\$ 38,100.00
655120	DETECTABLE WARNING SURFACE FOR CURB RAMPS	7	SF	\$ 50.00	\$ 333.33
705500	TURFGRASS ESTABLISHMENT	110	SY	\$ 2.10	\$ 231.00
801605	SHEET ALUMINUM SIGNS	12	EA	\$ 40.00	\$ 480.00
813023	RELOCATE EXISTING GROUND MOUNTED SIGNS	1	EA	\$ 25.00	\$ 25.00
	Sub Cost				\$ 77,450.00